LOS ALAMOS COUNTY AIRPORT

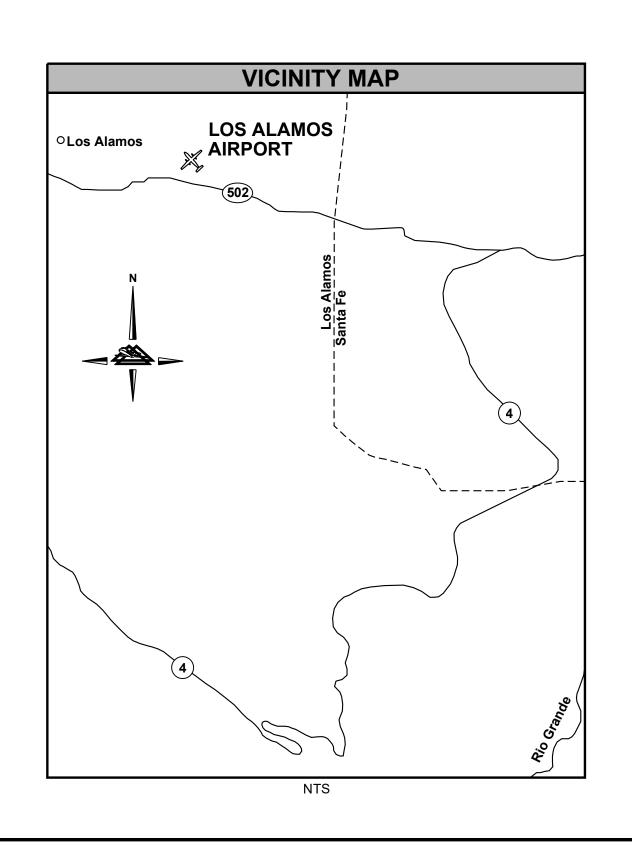
LOS ALAMOS, NEW MEXICO



CONSTRUCT T-HANGARS

NMDOT-AD PROJECT NO. LAM-24-02 COUNTY OF LOS ALAMOS BID NO. IFB25-15 DELTA PROJECT NO. 23063

FOR THE INCORPORATED COUNTY OF LOS ALAMOS SEPTEMBER 2024



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AS-BID

FOR BIDDING PURPOSES
DO NOT USE FOR CONSTRUCTION
CONFORMED DOCUMENTS WILL BE
ISSUED SEPARATELY FOR CONSTRUCTION

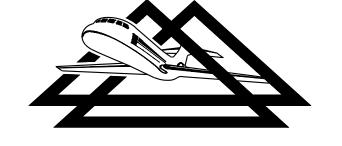
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7804 Pan American East Freeway NE, Suite 4
Albuquerque, New Mexico 87109

phone: (505) 797-4921 • fax: (505) 797-1725

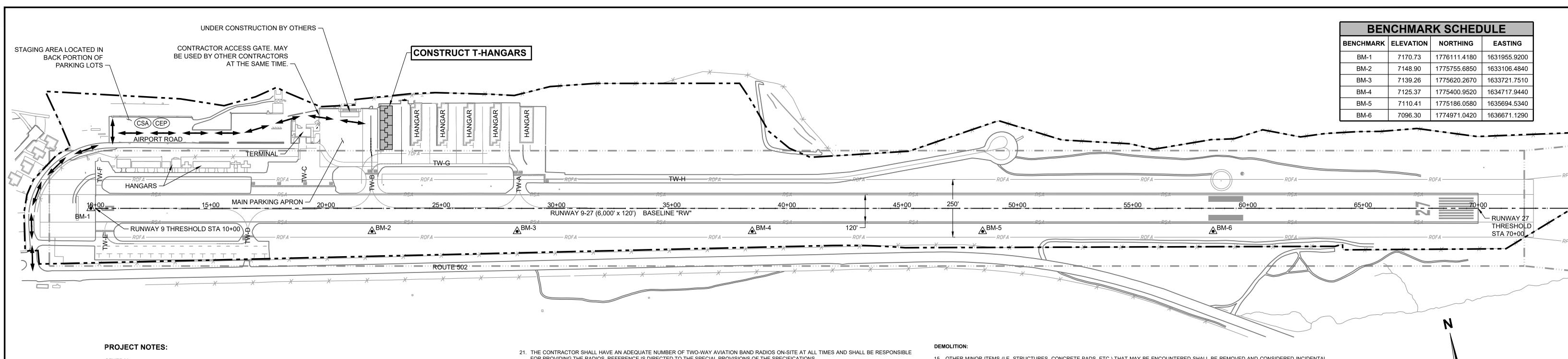
www.deltaairport.com







CONSTRUCT T-HANGARS	NMDOT-AD F	JOB NO . 23063	
COVER SHEET	DRAWN BY:	MJH	SHEET
COVER SHEET	DESIGNED BY: CAR/ABH		OF
LOS ALAMOS COUNTY AIRPORT	SCALE: NONE	DATE: SEPTEMBER 2024	14



- 1. THE FULL PROJECT NOTICE-TO-PROCEED WILL NOT BE ISSUED UNTIL ALL PROJECT FUNDING IS IN-PLACE, THE CONSTRUCTION CONTRACT HAS BEEN APPROVED AND FULLY EXECUTED BY ALL PARTIES, AND ALL MATERIALS FOR THE ERECTION OF THE HANGAR BUILDING ARE ON-SITE. A PARTIAL NOTICE-TO-PROCEED WILL BE ISSUED FOR THE ACQUISITION OF THE BUILDING MATERIALS.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING (INCLUDING PAYMENT OF FEES) ALL BONDS, PERMITS, LICENSES, ETC. REQUIRED BY LOCAL, STATE, AND FEDERAL AGENCIES. THE PERMITS REQUIRED SHALL BE IDENTIFIED AND PROCURED BY THE CONTRACTOR PRIOR TO THE START OF CONSTRUCTION. THE CONTRACTOR SHALL COORDINATE WHICH PERMITS ARE REQUIRED WITH THE COUNTY OF LOS ALAMOS. NEW MEXICO ENVIRONMENT DEPARTMENT, NEW MEXICO DEPARTMENT OF TRANSPORTATION, ETC. (NO SEPARATE PAY ITEM). BUILDING PERMIT(S) WILL BE ISSUED BY THE NEW MEXICO REGULATION & LICENSING DEPARTMENT - CONSTRUCTION INDUSTRIES DIVISION
- 3. ANY LISTED MANUFACTURER'S EQUIPMENT WILL MEAN THAT LISTED ITEM OR AN APPROVED EQUIVALENT.
- 4. BORING LOCATIONS SHOWN ON THE PLANS ARE APPROXIMATE. BORING INFORMATION WAS PROVIDED BY:

8528 CALLE ALAMEDA NE ALBUQUERQUE, NM 87113 (505) 857-0933

5. FIELD SURVEY INFORMATION WAS PROVIDED BY:

BOHANNAN HUSTON, INC. 7500 JEFFERSON STREET NE ALBUQUERQUE, NM 87109 (505) 823-1000

- 6. THE OWNER RESERVES THE RIGHT TO CONTRACT AND PERFORM OTHER OR ADDITIONAL WORK ADJACENT TO AND WITHIN THE WORK AREA COVERED BY THIS CONTRACT. WHEN SEPARATE CONTRACTS ARE LET WITHIN THE LIMITS OF ANY ONE PROJECT, EACH CONTRACTOR SHALL OTHER CONTRACTORS. THE CONTRACTORS WORKING ON THE SAME PROJECT SHALL COOPERATE WITH EACH OTHER AS ORDERED BY THE ENGINEER. ANY ADDITIONAL EFFORT OR WORK REQUIRED FOR SUCH COORDINATION WITH OTHER CONTRACTORS SHALL BE INCIDENTAL TO THE PROJECT AND NO SEPARATE MEASUREMENT OR PAYMENT WILL BE MADE. REFERENCE IS DIRECTED TO SECTION 50-06 OF THE SPECIFICATIONS. IT IS ANTICIPATED THAT OTHER WORK MAY BE COMPLETED BY AN INDEPENDENT CONTRACTOR(S) DURING THE COURSE OF THIS PROJECT.
- 7. THE APPROXIMATE LOCATION OF THE CONTRACTOR'S STAGING AREA(S) HAVE BEEN SHOWN ON THE PLANS.
- 8. CONTRACTOR EMPLOYEE(S) ARE PROHIBITED FROM RESIDING AT THE PROJECT SITE, ON AIRPORT PROPERTY, IN THE STAGING AREA, OR ANY OTHER TEMPORARY FACILITY
- 9. THIS PROJECT HAS BEEN ENVIRONMENTALLY APPROVED BY THE FAA UNDER A CATEX DATED JULY 20, 2016.
- 10. THERE ARE PROVISIONS IN THE CONTRACT DOCUMENTS FOR ADJUSTMENT IN CONTRACT TIME DUE TO UNUSUALLY ADVERSE WEATHER CONDITIONS. HOWEVER, THE CONTRACT TIME HAS BEEN ESTABLISHED BASED UPON THE RECEIPT OF NORMAL PRECIPITATION FOR THE PROJECT AREA. REFER TO "CONTRACT TIME EXTENSIONS" OF THE SPECIAL PROVISIONS FOR ADDITIONAL INFORMATION.
- 11. IN ACCORDANCE WITH SECTION 50-07. THE CONTRACTOR IS RESPONSIBLE FOR CONSTRUCTION LAYOUT AND STAKING AND MUST FURNISH STAKES FOR THE LAYOUT AND CONSTRUCTION OF THE WORK AND SHALL PRESERVE ALL STAKES AND BENCHMARKS THROUGH THE DURATION OF THE

SECURITY:

- 12. THE CONTRACTOR SHALL SUBMIT A SAFETY PLAN COMPLIANCE DOCUMENT (SPCD) IN ACCORDANCE WITH SECTION 80-04.1 "OPERATIONAL SAFETY ON AIRPORT DURING CONSTRUCTION" AND THE SPECIAL PROVISIONS OF THE SPECIFICATIONS.
- 13. THE CONTRACTOR IS RESPONSIBLE FOR ENSURING THAT ALL OF ITS EMPLOYEES AND SUBCONTRACTOR EMPLOYEES ONLY USE AUTHORIZED ACCESS POINT(S). EMPLOYEES SHALL VERIFY THAT THE ACCESS POINT(S) IS SECURE IMMEDIATELY AFTER USE. GATES THAT FAIL TO SECURE MUST BE IMMEDIATELY REPORTED TO THE ENGINEER OR OWNER.
- 14. THE CONTRACTOR SHALL ONLY USE THE HAUL ROUTE(S) INDICATED ON THE PLANS UNLESS OTHERWISE APPROVED BY THE ENGINEER AND
- 15. THE CONTRACTOR SHALL ALWAYS MAINTAIN SEPARATION BETWEEN THE SECURE SIDE AND NON-SECURE SIDE OF THE AIRPORT. THE PERIMETER FENCE SHALL BE MAINTAINED ON A CONTINUOUS BASIS WITH ANY TEMPORARY OPENING BEING CONTINUOUSLY OBSERVED BY THE CONTRACTOR'S DESIGNATED EMPLOYEE. IF ORDERED BY THE ENGINEER OR OWNER. THE CONTRACTOR MAY BE REQUIRED TO PROVIDE GATE GUARDS. GATE GUARDS ARE INCIDENTAL TO THE COST OF THE PROJECT. ALL TEMPORARY OPENINGS AND CONSTRUCTION GATES SHALL BE SECURED AND LOCKED AT THE COMPLETION OF WORK EACH DAY.
- 16. ALL CONTRACTOR VEHICLES OPERATING INSIDE THE SECURITY FENCE SHALL BE CLEARLY LABELED WITH THE CONTRACTOR'S COMPANY NAME VISIBLE FROM 200 FEET. THE COMPANY NAME SHALL BE LABELED ON BOTH SIDES OF THE VEHICLE.

OPERATIONS AND PHASING:

- 17. A 25-FOOT MAXIMUM EQUIPMENT HEIGHT HAS BEEN COORDINATED WITH THE FAA FOR AIRSPACE CLEARANCE. THE CONTRACTOR SHALL SUBMIT AN UPDATED FAA FORM 7460 "NOTICE OF PROPOSED CONSTRUCTION" IF THE CONTRACTOR INTENDS TO USE ANY EQUIPMENT WITH A HEIGHT
- 18. THE CONTRACTOR SHALL MARK AND LIGHT AREAS UNDER CONSTRUCTION IN ACCORDANCE WITH THE PLANS AND SPECIFICATIONS. AIRCRAFT AND PUBLIC SAFETY SHALL NOT BE COMPROMISED, AND PROPER SEPARATION SHALL BE MAINTAINED AT ALL TIMES. REFERENCE IS DIRECTED TO SECTION 80-04.1 "OPERATIONAL SAFETY ON AIRPORT DURING CONSTRUCTION" AND THE SPECIAL PROVISIONS OF THE SPECIFICATIONS. EQUIPMENT AND VEHICLES SHALL BE MARKED WITH YELLOW FLASHING DOME TYPE LIGHTS OR 3' X 3' ORANGE AND WHITE CHECKER FLAGS.
- 19. ALL CONTRACTOR PERSONNEL SHALL ARRIVE AT THE PROJECT IN COMPANY VEHICLES. ONLY AUTHORIZED VEHICLES WILL BE ALLOWED INSIDE THE SECURITY FENCE. AT THE END OF EACH WORK DAY, THE CONTRACTOR SHALL POSITION ALL EQUIPMENT, TOOLS, MATERIAL, ETC. IN THE APPROVED CONTRACTOR'S STAGING AREA UNLESS OTHERWISE REQUESTED BY THE CONTRACTOR AND APPROVED BY THE ENGINEER.
- 20. THE CONTRACTOR SHALL RELOCATE THE CLOSED MARKERS AND AVIATION BARRICADES DURING THE PROJECT. MULTIPLE RELOCATION OF THE CLOSED MARKERS WILL BE CONSIDERED INCIDENTAL TO THE PROJECT. THE LOCATION OF THE CLOSED MARKERS AND BARRICADES, AS SHOWN ON THE PLANS, REPRESENTS THE MOST TYPICAL LOCATION.

FOR PROVIDING THE RADIOS. REFERENCE IS DIRECTED TO THE SPECIAL PROVISIONS OF THE SPECIFICATIONS

THE CONTRACTOR SHALL MONITOR THE COMMON TRAFFIC ADVISORY FREQUENCY (123.0 MHZ) AT ALL TIMES.

22. UPON NOTIFICATION FROM THE OWNER, ENGINEER, OR OFFICIAL DESIGNATED REPRESENTATIVE, THE CONTRACTOR SHALL IMMEDIATELY CLEAR THE RUNWAY AND/OR TAXIWAY SAFETY AREAS OF EQUIPMENT AND PERSONNEL IN THE EVENT OF A DECLARED AIRCRAFT EMERGENCY.

HAUL ROUTE:

- 23. ALL CONTRACTOR AND SUBCONTRACTOR PERSONNEL AND EQUIPMENT SHALL USE ONLY THE DESIGNATED HAUL ROUTES FOR INGRESS AND EGRESS TO THE CONSTRUCTION AREA AND FOR HAULING MATERIALS TO THE APPROVED WASTE AREA (AS APPLICABLE).
- 24. NO CONTRACTOR TRAFFIC SHALL USE ANY PORTION OF RUNWAY(S), TAXIWAY(S) OR APRON(S) THAT ARE NOT ON THE HAUL ROUTE OR WITHIN THE
- 25. ANY CONSTRUCTION OR MAINTENANCE NECESSARY, INCLUDING ASSOCIATED LABOR AND MATERIALS, TO PRESERVE THE HAUL ROUTE FOR THE CONTRACTOR'S USE WILL BE CONSIDERED INCIDENTAL PROJECT COSTS AND WILL NOT BE A SEPARATE PAY ITEM.
- 26. UPON COMPLETION OF THE PROJECT, THE CONTRACTOR SHALL REPAIR ANY PAVEMENTS DAMAGED BY THE CONTRACTOR'S OPERATIONS. NO SEPARATE PAYMENT WILL BE MADE FOR PAVEMENT REPAIR NECESSARY DUE TO CONTRACTOR'S HAULING OPERATIONS.
- 27. THE CONTRACTOR'S ATTENTION IS ALSO DIRECTED TO THE GENERAL LAYOUT AND PHASING LAYOUT FOR ADDITIONAL INFORMATION.

CONSTRUCTION NOTES:

- 1. THE CONTRACTOR SHALL LOCATE AND PROTECT EXISTING UTILITIES AND FACILITIES (I.E., AIRPORT LIGHTING, NAVAIDS, ETC.) FROM DAMAGE BY EQUIPMENT OR PERSONNEL. THE CONTRACTOR SHALL CONTACT ALL UTILITY AND FACILITY AGENCIES FOR FIELD MARKING PRIOR TO BEGINNING INDEPENDENTLY VERIFIED BY THE OWNER OR ITS REPRESENTATIVE. THE CONTRACTOR SHALL DETERMINE THE EXACT LOCATION OF ALL EXISTING UTILITIES BEFORE COMMENCING WORK AND AGREES TO BE FULLY RESPONSIBLE FOR ANY AND ALL DAMAGES WHICH MIGHT BE OCCASIONED BY THE CONTRACTOR'S FAILURE TO EXACTLY LOCATE AND PRESERVE ANY AND ALL UNDERGROUND UTILITIES. ALL UTILITIES AND FACILITIES ARE NOT NECESSARILY SHOWN. THE CONTRACTOR SHALL ADVISE THE ENGINEER, IN WRITING, OF ANY EXISTING DAMAGED UTILITIES PRIOR TO BEGINNING
- ANY UTILITIES OR FACILITIES DAMAGED DURING THE PROJECT BY THE CONTRACTOR'S PERSONNEL OR EQUIPMENT SHALL BE PROMPTLY REPAIRED AT THE CONTRACTOR'S EXPENSE. THE CONTRACTOR SHALL HAND DIG WHEN WITHIN FIVE (5) FEET OF ANY KNOWN OR SUSPECTED UNDERGROUND UTILITY.
- 2. ALL STATIONS AND OFFSETS ARE APPROXIMATE AND SHALL BE CONFIRMED IN THE FIELD BY THE CONTRACTOR.
- 3. THE CONTRACTOR SHALL FIELD STAKE ALL PERMANENT ITEMS PRIOR TO INSTALLATION. ANY DISCREPANCIES IN THE ALIGNMENT LOCATION SHALL BE RESOLVED WITH THE ENGINEER PRIOR TO INSTALLATION.
- 4. THE CONTRACTOR SHALL PROTECT EXISTING AIRFIELD LIGHTING FIXTURES TO REMAIN FROM DAMAGE DURING CONSTRUCTION. AN INVENTORY OF EXISTING FIXTURES SHALL BE COMPLETED PRIOR TO THE START OF WORK WITH THE ENGINEER AND/OR OWNER. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE COST OF REPLACEMENT OF ANY FIXTURES DAMAGED BY THEIR OPERATIONS.
- 5. ALL BROKEN CONCRETE, ASPHALT, ETC. SHALL BE DISPOSED OF OFFSITE BY THE CONTRACTOR IN ACCORDANCE WITH LOCAL, STATE, AND FEDERAL GUIDELINES.
- 6. THE CONTRACTOR SHALL MAINTAIN POSITIVE DRAINAGE FOR THE COMPLETE DURATION OF THE PROJECT. THE DRAINAGE SHALL HAVE EQUAL OR GREATER CAPACITY AS THE EXISTING DRAINAGE CONVEYANCES. THIS MAY REQUIRE TEMPORARY DITCHES, STRUCTURES OR PIPES, WHICH WILL
- THE CONTRACTOR IS ADVISED THAT AIRCRAFT OPERATIONS ARE CONDUCTED ADJACENT TO THE PROJECT. SPECIAL ATTENTION TO DUST CONTROL WILL BE REQUIRED DURING THE DURATION OF THE PROJECT. THE USE OF WATER SHALL BE ANTICIPATED. THE ENGINEER RESERVES THE RIGHT TO HALT WORK OR HAULING IN NON-CONFORMING AREAS, IF DUST CONTROL ACTIONS ARE NOT PROMPTLY TAKEN BY THE
- 8. NO DEBRIS OF ANY NATURE IS ALLOWED IN ACTIVE AIRCRAFT OPERATIONS AREAS. ALL LOOSE MATERIALS (DIRT, STONE, PAVEMENT, FORMING, ETC.) MUST BE KEPT WITHIN THE LIMITS OF CONSTRUCTION. WHEN THE CONSTRUCTION BARRIERS ARE MOVED DURING CONSTRUCTION, THE CLEANUP OF THE AREAS OUTSIDE THE BARRIERS SHALL OCCUR IMMEDIATELY. IN ADDITION, NO LOOSE MATERIALS THAT COULD BLOW INTO AIRCRAFT OPERATIONS AREAS ARE ALLOWED IN THE CONSTRUCTION AREA.
- 9. CONTRACTORS ARE ENCOURAGED TO EXAMINE THE SITE TO VERIFY FIELD CONDITIONS BEFORE SUBMITTING BIDS. NO ADDITIONAL COMPENSATION WILL BE MADE FOR ADVERSE FIELD CONDITIONS SUCH AS EXCAVATION AND/OR TRENCHING THROUGH ROCK.
- 10. THE CONTRACTOR SHALL HAVE A FULLY OPERATIONAL VACUUM TRUCK AVAILABLE FOR CLEANING AREAS OF POTENTIAL FOD/DEBRIS. VACUUM TRUCKS SHALL BE IN GOOD WORKING ORDER AND ANY SWEEPING ELEMENTS SHALL BE MADE OF NON-WIRE BRISTLES (WIRED BRISTLES TEND TO BREAK AND BECOME FOD). SWEEPER VEHICLES SHALL BE AT ANY ACTIVE RUNWAY OR TAXIWAY CROSSING DURING WORK HOURS (NO SEPARATE PAY ITEM).
- 11. THE CONTRACTOR WILL BE RESPONSIBLE FOR THEIR OWN QUALITY CONTROL TESTING.
- 12. A STEEP DROP-OFF EXISTS ALONG THE NORTH SIDE OF THE PROJECT SITE. CONTRACTOR IS RESPONSIBLE FOR SITE SAFETY AND SHALL TAKE STEPS TO MARK AND PROTECT THIS AREA DURING CONSTRUCTION.
- 13. DUE TO THE PROXIMITY OF FUEL STORAGE AND FUELING OPERATIONS, SMOKING IS PROHIBITED WITHIN THE AIRCRAFT OPERATIONS AREA AND
- 14. CONTRACTOR SHALL MINIMIZE THE AMOUNT OF EXCAVATION FOR BUILDING FOOTINGS AND DRAINAGE INSTALLATION. POCKETS OF DEBRIS WERE LOCATED DURING GEOTECHNICAL EXPLORATION BELOW ANTICIPATED FOOTING DEPTHS (SEE THE GEOTECHNICAL REPORT INCLUDED IN THE SPECIFICATIONS). IF DEBRIS IS ENCOUNTERED DURING CONSTRUCTION, CONTRACTOR SHALL NOTIFY N3B TO VERIFY THAT IT IS NOT CLASSIFIED

- 15. OTHER MINOR ITEMS (I.E. STRUCTURES, CONCRETE PADS, ETC.) THAT MAY BE ENCOUNTERED SHALL BE REMOVED AND CONSIDERED INCIDENTAL
- 16. ALL PAVEMENT TO BE REMOVED SHALL BE NEATLY SAWCUT. (NO SEPARATE PAY ITEM).
- 17. THE CONTRACTOR SHALL LAYOUT THE LIMITS OF PAVEMENT REMOVAL IN THE FIELD PRIOR TO BEGINNING WORK

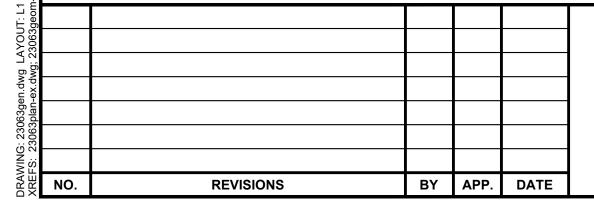
SITE CLEANUP

- 18. STAGING AREAS SHALL BE RESTORED TO THEIR PRE-CONSTRUCTION CONDITIONS. NO ADDITIONAL PAYMENT SHALL BE MADE FOR RESTORING THE CONTRACTOR'S HAUL ROUTES, STAGING AND STORAGE AREAS.
- THE CONTRACTOR SHALL INSTALL STANDARD STEPS IN ALL MANHOLES AND DROP INLETS. THE CONTRACTOR SHALL INSTALL INLET SHAPING IN ALL DRAINAGE AND SEWAGE MANHOLES AND DROP INLETS. NO SEPARATE MEASUREMENT WILL BE MADE FOR THE ABOVE REFERENCED ITEMS.
- 2. ALL MANHOLE AND DROP INLET LOCATIONS AND TOP ELEVATIONS ARE GIVEN TO THE CENTER OF THE STRUCTURE UNLESS OTHERWISE NOTED. CURB DROP INLETS SHALL BE ALIGNED TO MATCH CURB ALIGNMENT AND GRADE. SPOT ELEVATIONS ARE GIVEN FOR CURB OR CURB AND GUTTER AT THE TOP OF CURB UNLESS OTHERWISE NOTED. THE CONTRACTOR SHALL FIELD VERIFY THE INVERTS OF ALL CONNECTIONS TO EXISTING PIPES PRIOR TO FABRICATION OF PROPOSED STRUCTURES.
- LENGTHS OF DRAINAGE PIPES ON THE PLANS ARE GIVEN FROM CENTER OF MANHOLE TO CENTER OF MANHOLE OR FROM OUTSIDE EDGE OF ENDWALL TO OUTSIDE EDGE OF ENDWALL. PIPE WILL BE MEASURED FOR PAYMENT FROM THE INTERIOR FACE OF THE DOWNSTREAM STRUCTURE TO THE INTERIOR FACE OF THE UPSTREAM STRUCTURE IN ACCORDANCE WITH SECTION 90.
- 4. THE CONTRACTOR SHALL FIELD VERIFY EXISTING LOCATION, SIZE, INVERT AND TOP ELEVATION FOR EACH EXISTING PIPE AND/OR STRUCTURE WHICH IS BEING CONNECTED TO PROPOSED PIPE(S) AND/OR STRUCTURES. VERIFICATIONS SHALL BE COMPLETED AND SUBMITTED IN WRITING TO THE ENGINEER PRIOR TO STRUCTURE FABRICATION AND/OR DRAINAGE PIPE INSTALLATION (NO SEPARATE PAY ITEM).
- WELLPOINTING AND/OR OTHER DEWATERING METHODS REQUIRED FOR THE INSTALLATION OF MANHOLES, DROP INLETS, ETC. WILI CONSIDERED INCIDENTAL TO THE PROJECT.
- 6. THE CONTRACTOR SHALL CLEAN THE STORM SEWER SYSTEM OF ALL SEDIMENT UPON STABILIZATION OF THE UPSTREAM AREAS (NO SEPARATE
- 7. WHERE THE PLANS CALL FOR CONNECTIONS TO EXISTING OR PROPOSED STRUCTURES, THE CONNECTIONS SHALL BE WATERTIGHT AND MADE SO THAT A SMOOTH UNIFORM FLOW LINE WILL BE OBTAINED THROUGHOUT THE DRAINAGE SYSTEM (NO SEPARATE PAY ITEM).
- 8. AFTER THE PIPE BACKFILL IS COMPLETED, THE CONTRACTOR SHALL DISPOSE OF ALL SURPLUS MATERIAL, SOIL, AND RUBBISH FROM THE SITE. SURPLUS SOIL MAY BE DEPOSITED IN EMBANKMENT, SHOULDERS, OR AS ORDERED BY THE ENGINEER. THE CONTRACTOR SHALL RESTORE ALL DISTURBED AREAS OUTSIDE THE GRADING OR PAVING LIMITS OF THE PROJECT, TO THEIR ORIGINAL CONDITION (NO SEPARATE PAY ITEM).
- 9. AFTER WORK IS COMPLETED, THE CONTRACTOR SHALL REMOVE ALL TOOLS AND OTHER EQUIPMENT, LEAVING THE ENTIRE SITE FREE, CLEAR, AND IN GOOD CONDITION (NO SEPARATE PAY ITEM)

LEGEND BENCHMARK LOCATION (CEP) CONTRACTOR EMPLOYEE PARKING CONTRACTOR STAGING AREA HAUL ROUTE (TWO-WAY TRAFFIC) EXISTING PART 77 APPROACH SURFACE **EXISTING AIRPORT PROPERTY LINE** ------ROFA------ EXISTING RUNWAY OBJECT FREE AREA EXISTING RUNWAY SAFETY AREA PROPOSED CONSTRUCTION

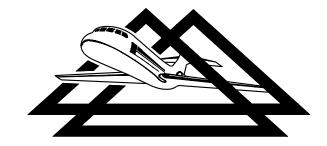
 REFER TO "PHASING LAYOUT" FOR CONSTRUCTION PHASING.

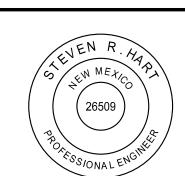




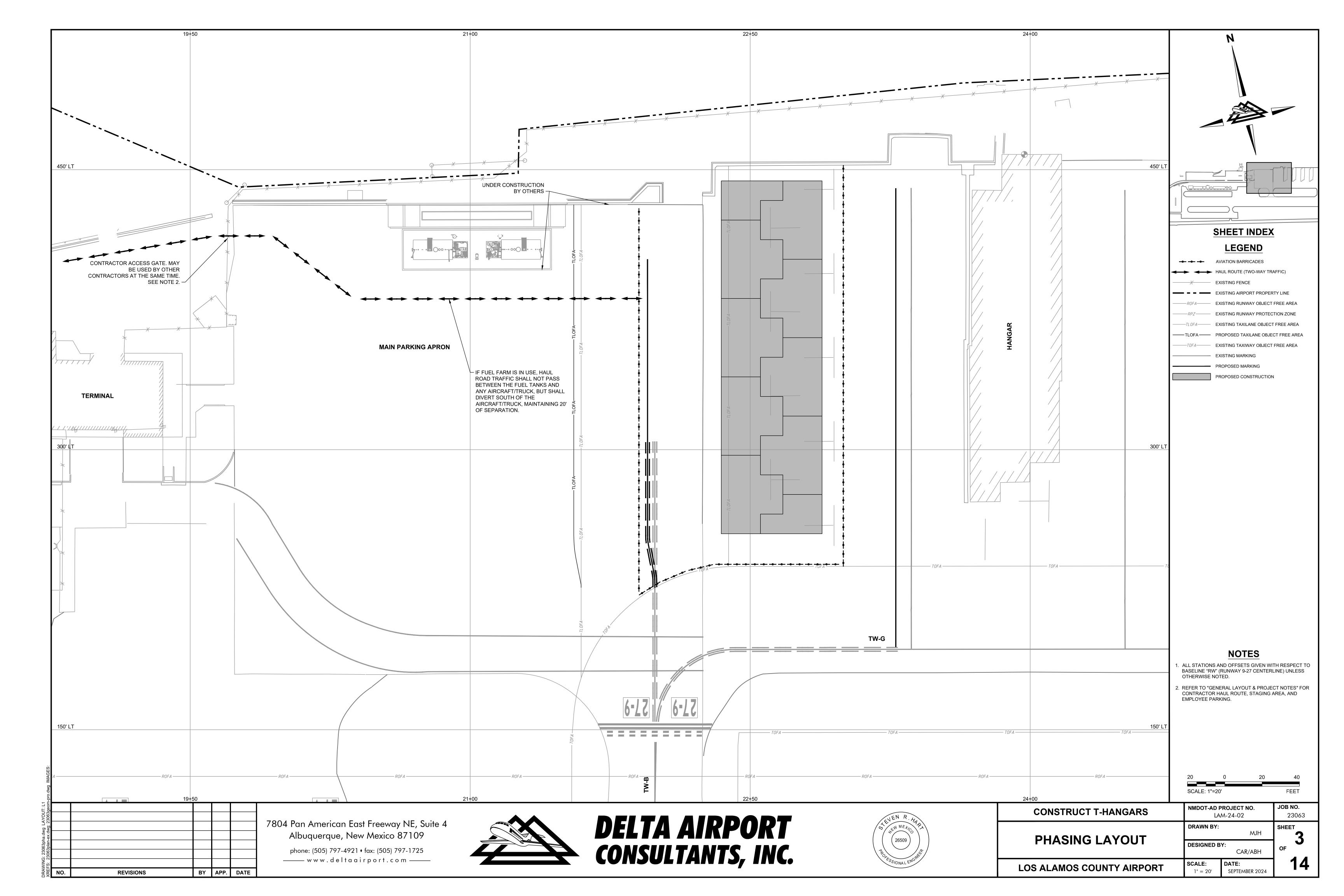
7804 Pan American East Freeway NE, Suite 4 Albuquerque, New Mexico 87109

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CONSTRUCT T-HANGARS	NMDOT-AD F LA	JOB NO . 23063	
GENERAL LAYOUT &	DRAWN BY:	MJH	SHEET 7
PROJECT NOTES	DESIGNED B	Y: CAR/ABH	OF
S ALAMOS COUNTY AIRPORT	SCALE: 1" = 200'	DATE: SEPTEMBER 2024	14



FLASHING OR STEADY BURNING RED BARRICADE LIGHT (TYPE A) SECURELY FASTENED. BI-DIRECTIONAL LIGHTS SHALL BE ORIENTED TO MAXIMIZE VISIBILITY TO APPROACHING TRAFFIC. IN AREAS WHERE TRAFFIC APPROACHES FROM MULTIPLE DIRECTIONS, TWO BI-DIRECTIONAL LIGHTS SHALL BE INSTALLED OR ONE OMNI-DIRECTIONAL LIGHT. REFLECTIVE MATERIAL, ORANGE FLUORESCENT AND WHITE FLUORESCENT STRIPES.

WATER FILLED AVIATION BARRICADE DETAIL

USE ON AIRPORTS.

BARRICADES SHALL BE COMMERCIALLY

MANUFACTURED FOR THE SPECIFIC

(LOW PROFILE) NOTES:

- 1. BARRICADES SHALL BE SPACED A MAXIMUM OF 4' APART, EXCEPT AT VEHICLE ACCESS/ARFF ACCESS POINTS WHERE A 15' GAP IS ALLOWED.
- 2. BARRICADES SHALL BE COMMERCIALLY MADE IN LENGTHS OF 6' OR 8'.
- 3. BARRICADES SHALL BE FILLED WITH WATER TO ADEQUATELY WITHSTAND HIGH WINDS AND/OR JET BLAST.
- 4. THE CONTRACTOR SHALL MAKE FREQUENT INSPECTION OF THE BARRICADES AND MAKE PROMPT REPAIRS AS NECESSARY.
- 5. LIGHTS SHALL BE INSPECTED AND OPERATING EVERY NIGHT PRIOR TO LEAVING THE SITE.
- 6. RELOCATIONS AND MULTIPLE SETUPS AND REMOVALS OF THE AVIATION BARRICADES SHALL BE INCIDENTAL TO THE UNIT COST WHEREIN NO SEPARATE PAYMENT WILL BE MADE.

PHASING NOTES:

- THE CONTRACTOR SHALL HAVE 100 CALENDAR DAYS TO COMPLETE THE PROJECT.
- 2. LIQUIDATED DAMAGES IN THE AMOUNT OF TWO THOUSAND DOLLARS (\$2,000.00) PER CALENDAR DAY WILL BE ASSESSED AGAINST THE CONTRACTOR FOR EACH CALENDAR DAY, OR PORTION THEREOF, THAT THE TOTAL CONTRACT TIME IS EXCEEDED.
- 3. THE INTENT OF THIS PHASING PLAN IS TO MINIMIZE THE IMPACT OF CONSTRUCTION ON THE OPERATION OF THE AIRPORT. ALL REQUIREMENTS CONTAINED IN THE CONTRACT DOCUMENTS WILL BE STRICTLY ENFORCED.
- 4. THE CONTRACTOR SHALL SCHEDULE THE WORK TO MINIMIZE CONSTRUCTION IMPACTS ON AIRPORT OPERATIONS. THE CONTRACTOR SHALL PROVIDE THE OWNER WITH A SEVEN (7) DAY ADVANCE NOTICE OF INTENT TO START WORK. THE OWNER WILL ISSUE THE APPROPRIATE NOTICE-TO-AIRMEN (NOTAMS).
- 5. THE CONTRACTOR SHALL NOT MAKE ANY CHANGES TO THE PHASING PLAN WITHOUT APPROVAL OF THE ENGINEER AND OWNER.
- 6. PRIOR TO SUBMITTAL OF THE NOTICE OF INTENT TO START WORK, SUBMITTAL AND ACCEPTANCE OF ALL SHOP DRAWINGS FOR ALL ITEMS INCLUDED IN THE WORK SHALL BE COMPLETED.
- 7. THE CONTRACTOR SHALL SUBMIT A DETAILED SCHEDULE TO THE ENGINEER A MINIMUM OF SEVEN (7) DAYS PRIOR TO THE PRE-CONSTRUCTION CONFERENCE. THE SCHEDULE SHALL CLEARLY DELINEATE THE PROPOSED WORK AREAS AND THE NUMBER OF DAYS ANTICIPATED FOR THE COMPLETION OF EACH WORK AREA. REFERENCE IS DIRECTED TO THE SPECIAL PROVISIONS OF THE SPECIFICATIONS.
- 8. THE CONTRACTOR SHALL MARK AND LIGHT AREAS UNDER CONSTRUCTION IN ACCORDANCE WITH THE PLANS AND SPECIFICATIONS. AIRCRAFT AND PUBLIC SAFETY SHALL NOT BE COMPROMISED, AND PROPER SEPARATION SHALL BE MAINTAINED AT ALL TIMES.
- 9. BARRICADES SHALL BE INSTALLED AS THE FIRST TASK AND REMOVED AS THE LAST TASK. AVIATION BARRICADES SHALL NOT BE REMOVED UNTIL THE PAVEMENT AND SAFETY AREAS ARE ESTABLISHED IN ACCORDANCE WITH THE PLANS AND SPECIFICATIONS AND ARE SUITABLE FOR USE BY AIRCRAFT.
- 10. THE CONTRACTOR SHALL BE REQUIRED TO RELOCATE THE AVIATION BARRICADES DURING THE PROJECT. MULTIPLE RELOCATION OF THE BARRICADES SHALL BE CONSIDERED INCIDENTAL TO THE PROJECT.
- 11. AIRCRAFT SHALL HAVE THE RIGHT-OF-WAY AT ALL TIMES. CONTRACTOR VEHICLES SHALL GIVE WAY TO AIRCRAFT AT ALL TIMES.
- 12. ANY REOPENING OF TEMPORARILY CLOSED AIRFIELD PAVEMENTS SHALL BE COORDINATED IN ADVANCE WITH THE OWNER. PRIOR TO REOPENING PAVEMENT TO AIRCRAFT OPERATIONS, THE CONTRACTOR SHALL CLEAN ALL DEBRIS (FOD) FROM THE PAVEMENT SURFACES, RESTORE MARKINGS, AND ESTABLISH THE SAFETY AREAS IN ACCORDANCE WITH THE SPECIFICATIONS. THE OWNER AND ENGINEER SHALL BE NOTIFIED SUFFICIENTLY IN ADVANCE OF THE OPENING TIME TO ALLOW FOR INSPECTION AND ANY REMEDIAL CLEANING.
- 13. REFER TO THE PHASING DETAILS SHEET FOR DETAILS OF AVIATION BARRICADES AND FAR PART 77 SURFACES.
- 14. REFER TO THE SPECIFICATIONS AND THE CONSTRUCTION SAFETY AND PHASING PLAN (CSPP) FOR THE CONTRACTOR'S RESPONSIBILITIES FOR OPERATIONAL SAFETY ON AIRPORTS DURING CONSTRUCTION.
- 15. THE CONTRACTOR IS RESPONSIBLE FOR COMPLIANCE WITH FAA ADVISORY CIRCULAR (AC) 150/5370-2 "OPERATIONAL SAFETY ON AIRPORTS DURING CONSTRUCTION" AND THE PROJECT CONSTRUCTION SAFETY AND PHASING PLAN (CSPP).
- 16. THE CONSTRUCTION SHALL SUBMIT A SAFETY PLAN COMPLIANCE DOCUMENT (SPCD) IN ACCORDANCE WITH THE SPECIAL PROVISIONS OF THE SPECIFICATIONS AND THE CURRENT VERSION OF FAA ADVISORY CIRCULAR (AC) 150/5370-2 "OPERATIONAL SAFETY ON AIRPORTS DURING CONSTRUCTION".
- 17. THE CONTRACTOR SHALL PROVIDE THE ENGINEER AND OWNER WITH A SEVEN (7) DAY WRITTEN ADVANCE NOTICE OF INTENT TO BEGIN EACH WORK AREA. THE OWNER WILL ISSUE THE APPROPRIATE NOTICE-TO-AIRMEN (NOTAMS).
- 18. THE CONTRACTOR SHALL SCHEDULE AND CONDUCT WEEKLY SAFETY/PROGRESS MEETINGS WITH THE ENGINEER AND OWNER.
- 19. ALL CONTRACTOR VEHICLES SHALL BE MARKED WITH YELLOW FLASHING STROBE LIGHTS OR 3' X 3' ORANGE AND WHITE CHECKER FLAGS WHEN OPERATING ON THE AIRFIELD.

WORK AREA NOTES:

GENERAL

- 20. PRIOR TO STARTING WORK, THE CONTRACTOR SHALL COORDINATE WITH THE OWNER AND CONFIRM THAT THE PROPER NOTAMS HAVE BEEN ISSUED AND ARE ON FILE. ALL NOTAMS SHALL BE ISSUED BY THE OWNER.
- 21. THE CONTRACTOR SHALL PROVIDE THE ENGINEER AND OWNER WITH A 7 DAY WRITTEN ADVANCE OF INTENT TO BEGIN WORK
- 22. ALL MATERIALS SHALL BE APPROVED AND ON-SITE PRIOR TO THE START OF WORK
- 23. THE CONTRACTOR'S PERSONNEL AND EQUIPMENT SHALL REMAIN WITHIN THE WORK AREA LIMITS AND DESIGNATED HAUL ROUTES AT ALL

WORK AREA: HANGAR CONSTRUCTION AND APRON MARKING

LIMITATIONS: NONE

MILESTONE TIME: 100 CALENDAR DAYS

MILESTONE

LIQUIDATED DAMAGES: \$2,000.00 PER CALENDAR DAY

ADVANCE NOTICE: 7 CALENDAR DAYS (WRITTEN) 48 HOURS

NOTICE / OTHER WORK REQUIREMENTS BEFORE STARTING

AOA CLOSURES: NONE

AOA CLOSURE PERIODS: NONE

• INSTALL ALL LOW-PROFILE BARRICADES PER THE PHASING PLAN FOR PHASE 1.

REQUIRED WORK TO BE COMPLETED

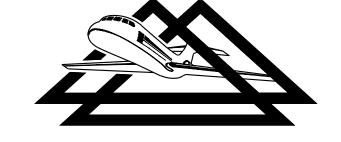
- MARKING REMOVAL ON HANGAR SITE AND ADJACENT TAXILANES
- INSTALLATION OF UTILITIES AND INFRASTRUCTURE CONSTRUCTION OF T-HANGAR FOUNDATION
- ERECTION OF T-HANGAR BUILDING
- INSTALLATION OF T-HANGAR ELECTRICAL AND LIGHTING MARKING OF ADJACENT TAXILANE PAVEMENTS

• AIRFIELD PAVEMENTS SHALL NOT BE PERMITTED TO REOPEN UNTIL THE SAFETY AREA REQUIREMENTS ARE MET AND THE PAVEMENT HAS BEEN CLEARED OF FOD.

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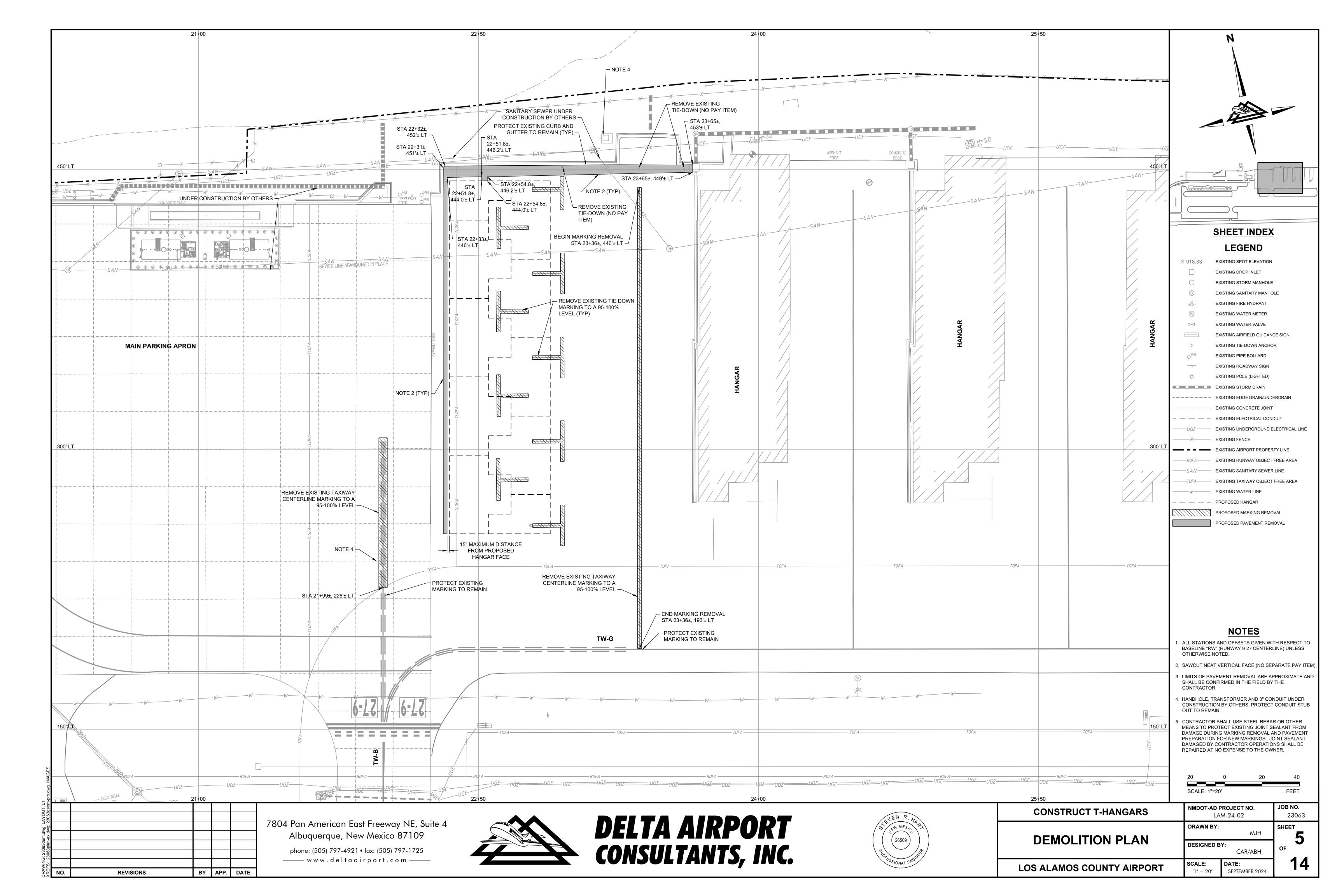
CONSTRUCT T-HANGARS	_	PROJECT NO. M-24-02
DUACING NOTES & DETAILS	DRAWN BY:	MJH
PHASING NOTES & DETAILS	DESIGNED B	Y: CAR/ABH
LOS ALAMOS COUNTY AIRPORT	SCALE:	DATE:

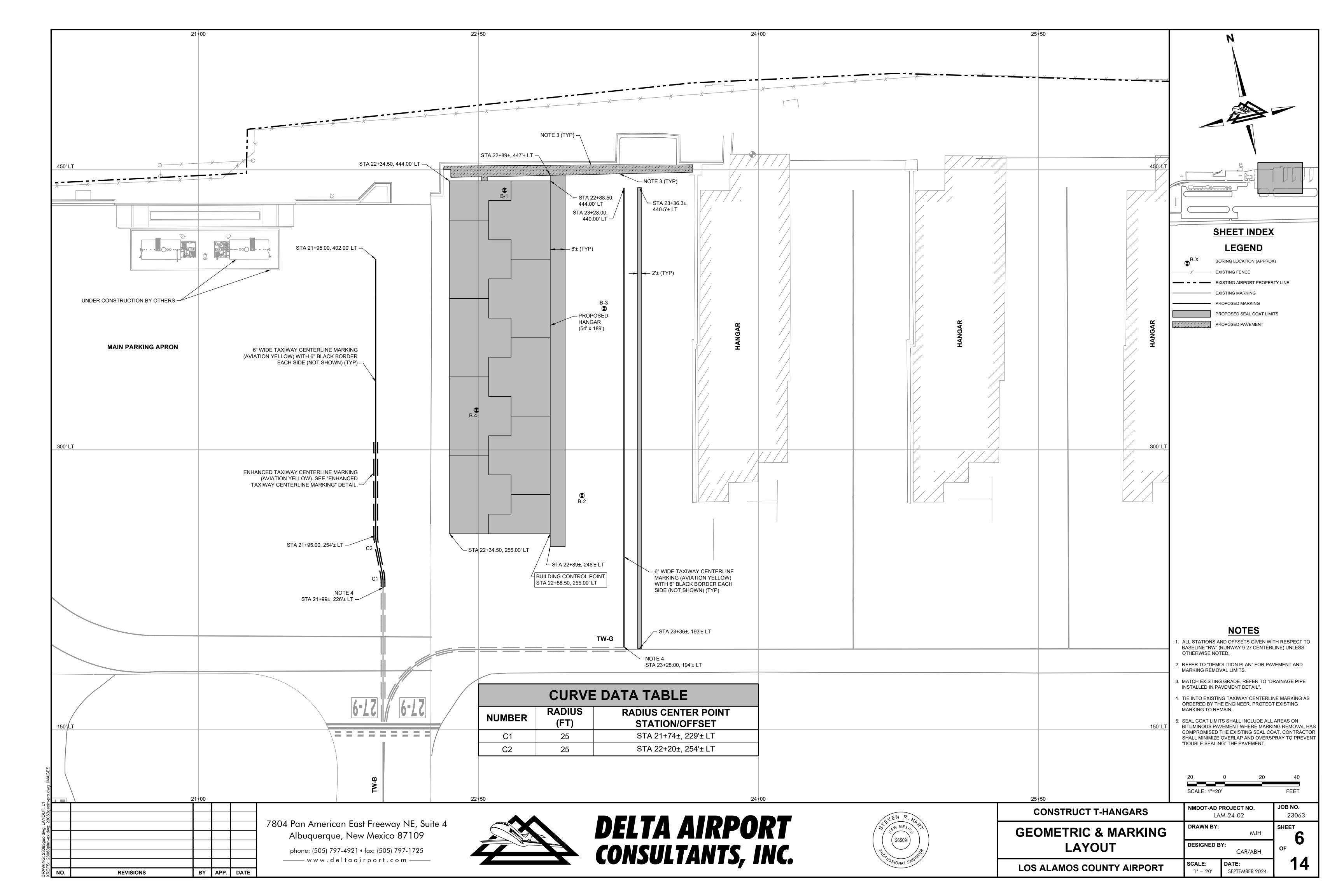
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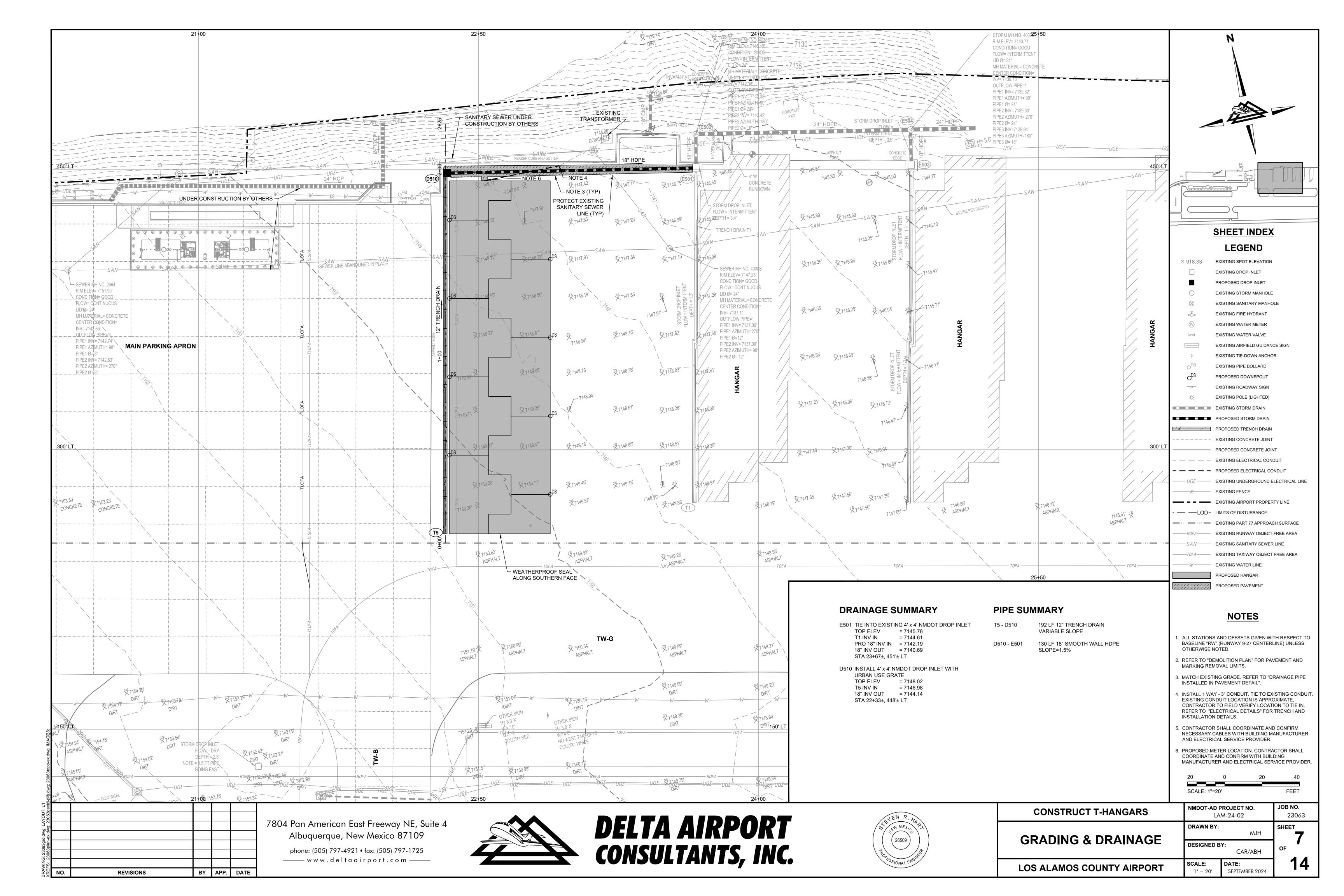
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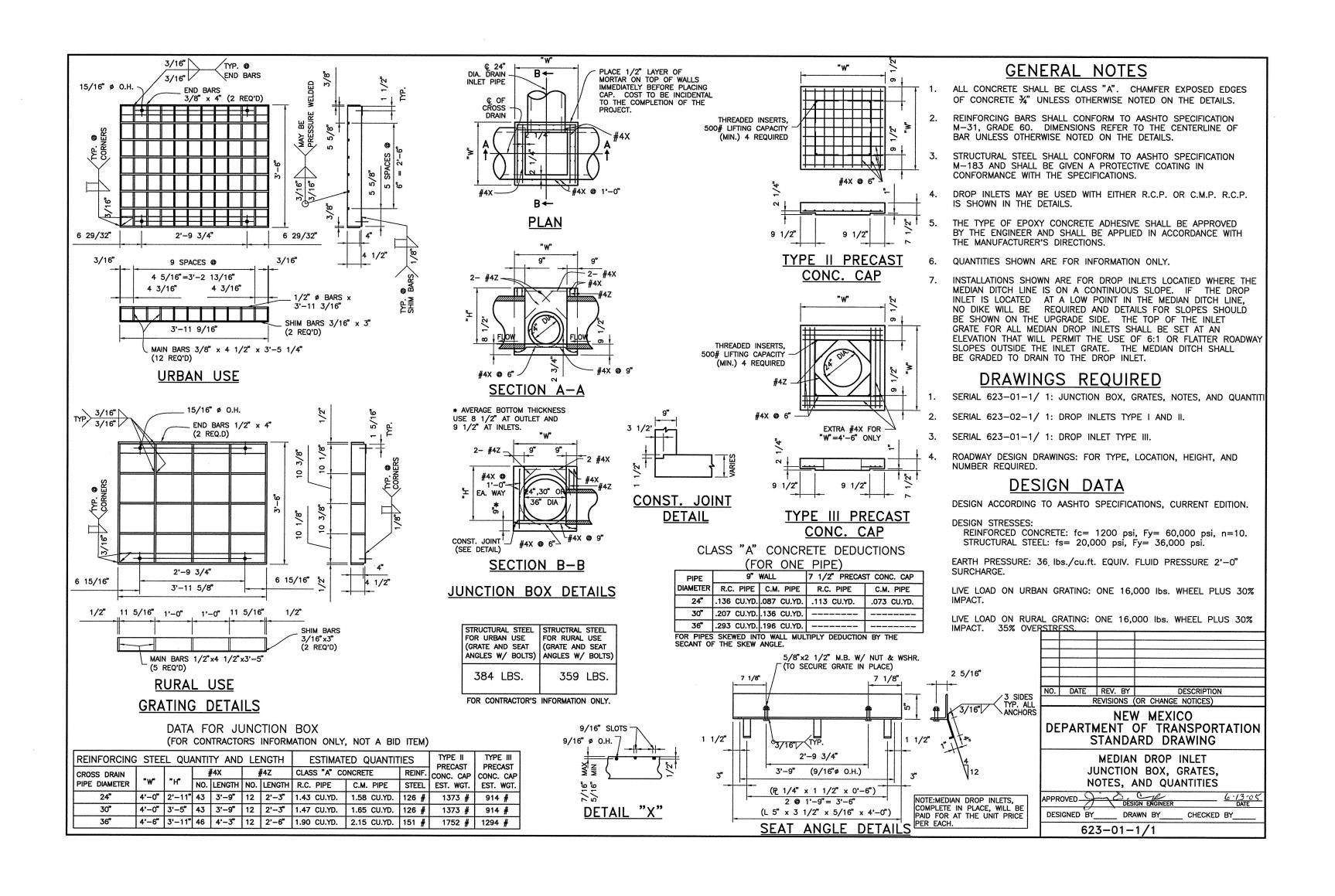
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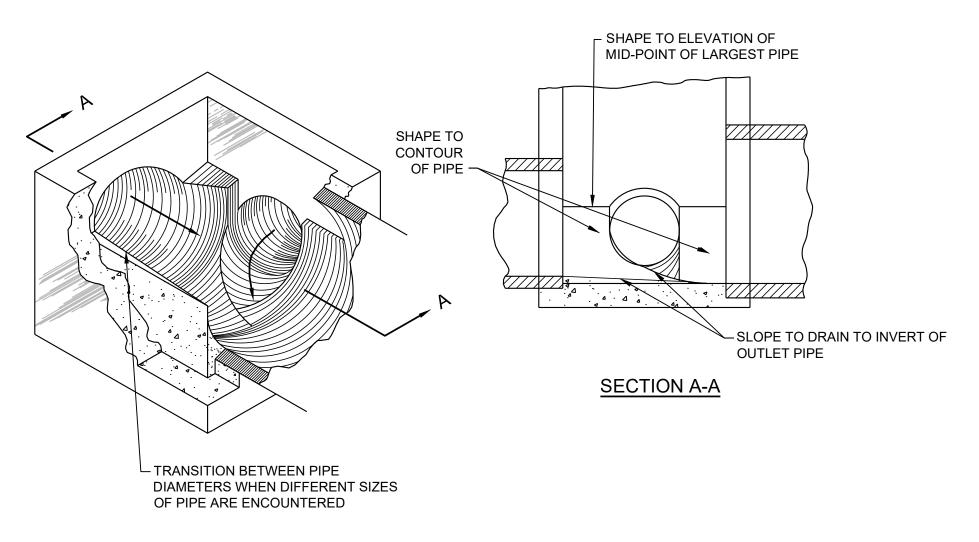
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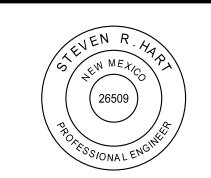




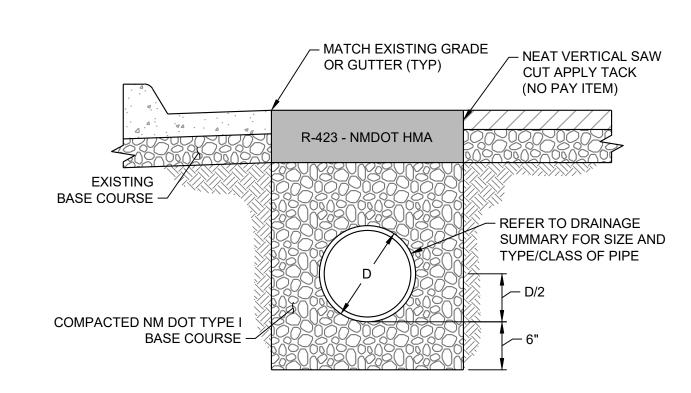


METHOD OF SHAPING INLET INVERTS

DELTA AIRPORT CONSULTANTS, INC.



CONSTRUCT T-HANGARS	NMDOT-AD PROJECT NO. LAM-24-02		JOB NO. 23063
DRAINAGE DETAILS	DRAWN BY:	MJH	SHEET
DRAINAGE DETAILS	DESIGNED BY: CAR/ABH		OF
OS ALAMOS COUNTY AIRPORT	SCALE: NONE	DATE: SEPTEMBER 2024	14

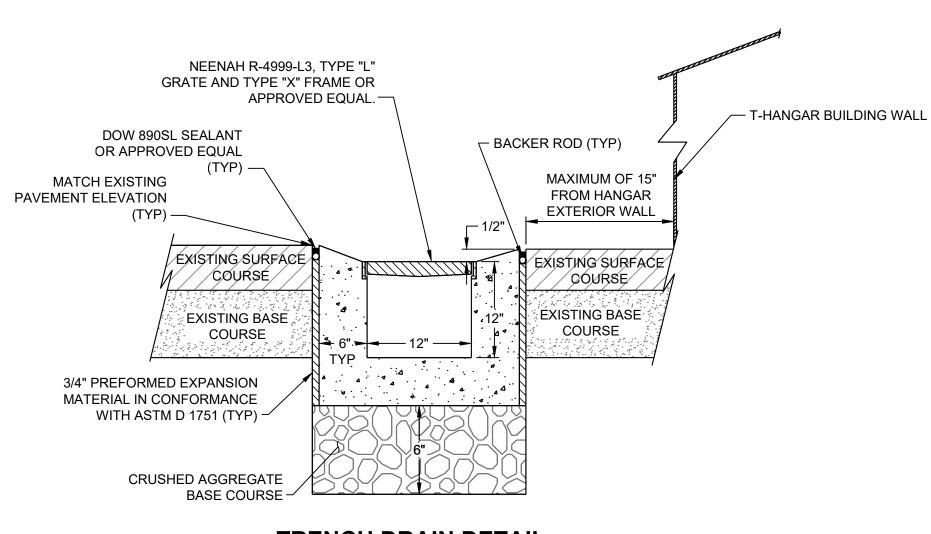


DRAINAGE PIPE INSTALLED IN PAVEMENT DETAIL

NTS

NOTES:

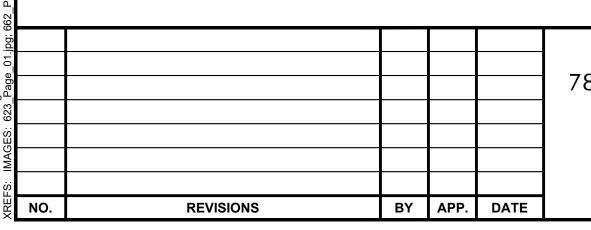
- 1. THE COST OF THE AGGREGATE BACKFILL SHALL BE CONSIDERED INCIDENTAL TO THE COST OF THE PIPE.
- 2. D = OUTSIDE PIPE DIAMETER.
- 3. OFF-SITE BACKFILL MATERIAL MAY BE NECESSARY TO OBTAIN COMPACTION. SELECT MATERIAL SHALL BE INCIDENTAL TO PRICE OF PIPE.
- 4. PROVIDE AND INSTALL TWO EACH 4" WEEP HOLES AT SPRING LINE OF DOWNSTREAM STRUCTURE ON EACH RUN OF PIPE. EXTEND 4" PERFORATED SCHEDULE 40 PVC PIPE FROM WEEP HOLE THROUGH STRUCTURE 12" INTO BEDDING. PERFORATIONS SHALL BE IN ACCORDANCE WITH ASTM F758. NO PAY ITEM.



TRENCH DRAIN DETAIL

NOTES:

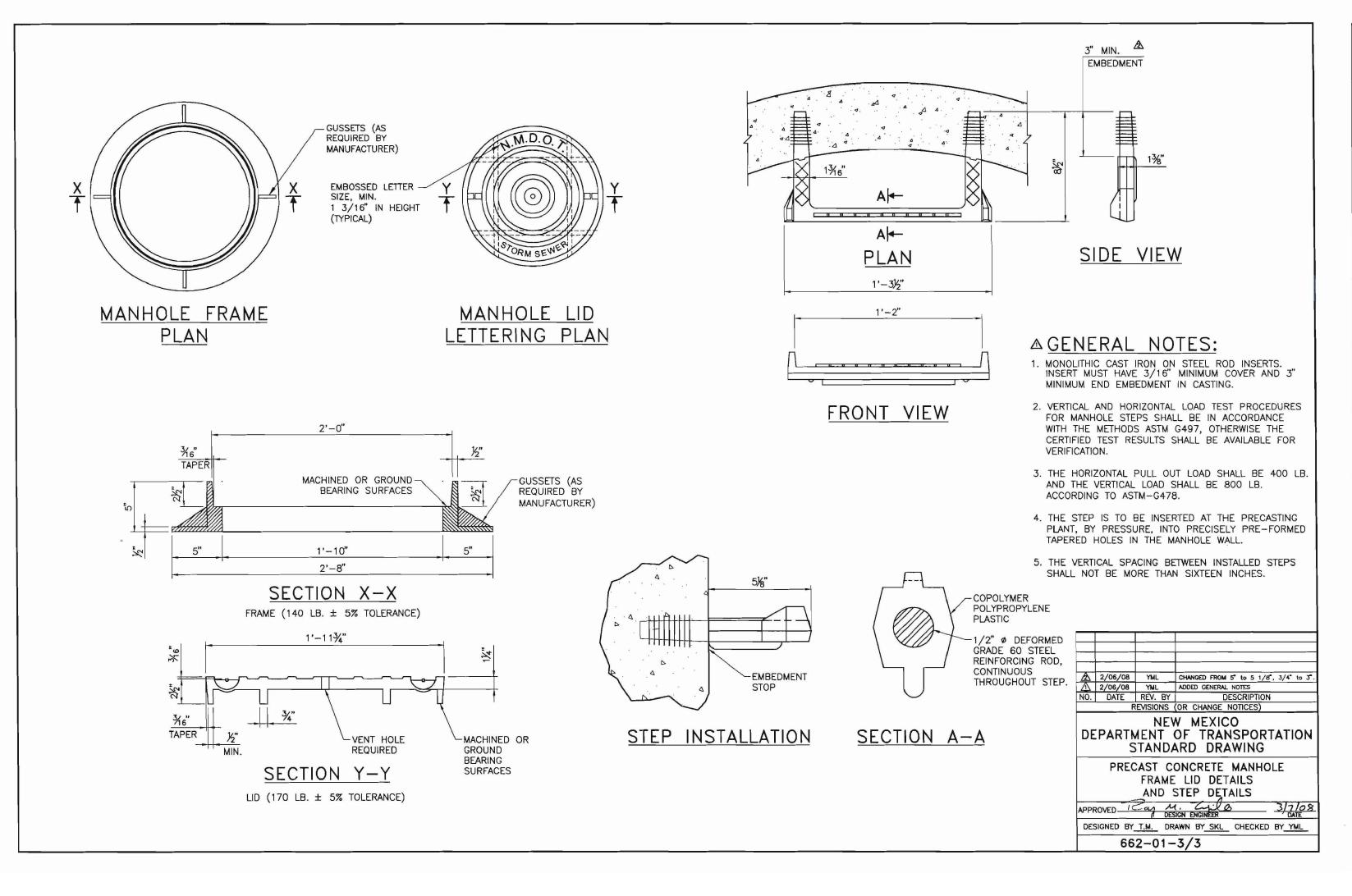
- 1. CONCRETE SHALL BE 4000 PSI, HIGH EARLY STRENGTH WITH FIBER REINFORCEMENT ADDED. CONTRACTOR SHALL SUBMIT CONCRETE MIX DESIGN TO ENGINEER FOR APPROVAL PRIOR TO CONSTRUCTION.
- 2. JOINT SEALANT SHALL REMAIN BELOW THE PAVEMENT SURFACE (1/4" MAX) INSTALL PER MANUFACTURER'S RECOMMENDATION.
- 3. NO SEPARATE MEASUREMENT FOR PAYMENT WILL BE MADE FOR EXPANSION JOINTS.

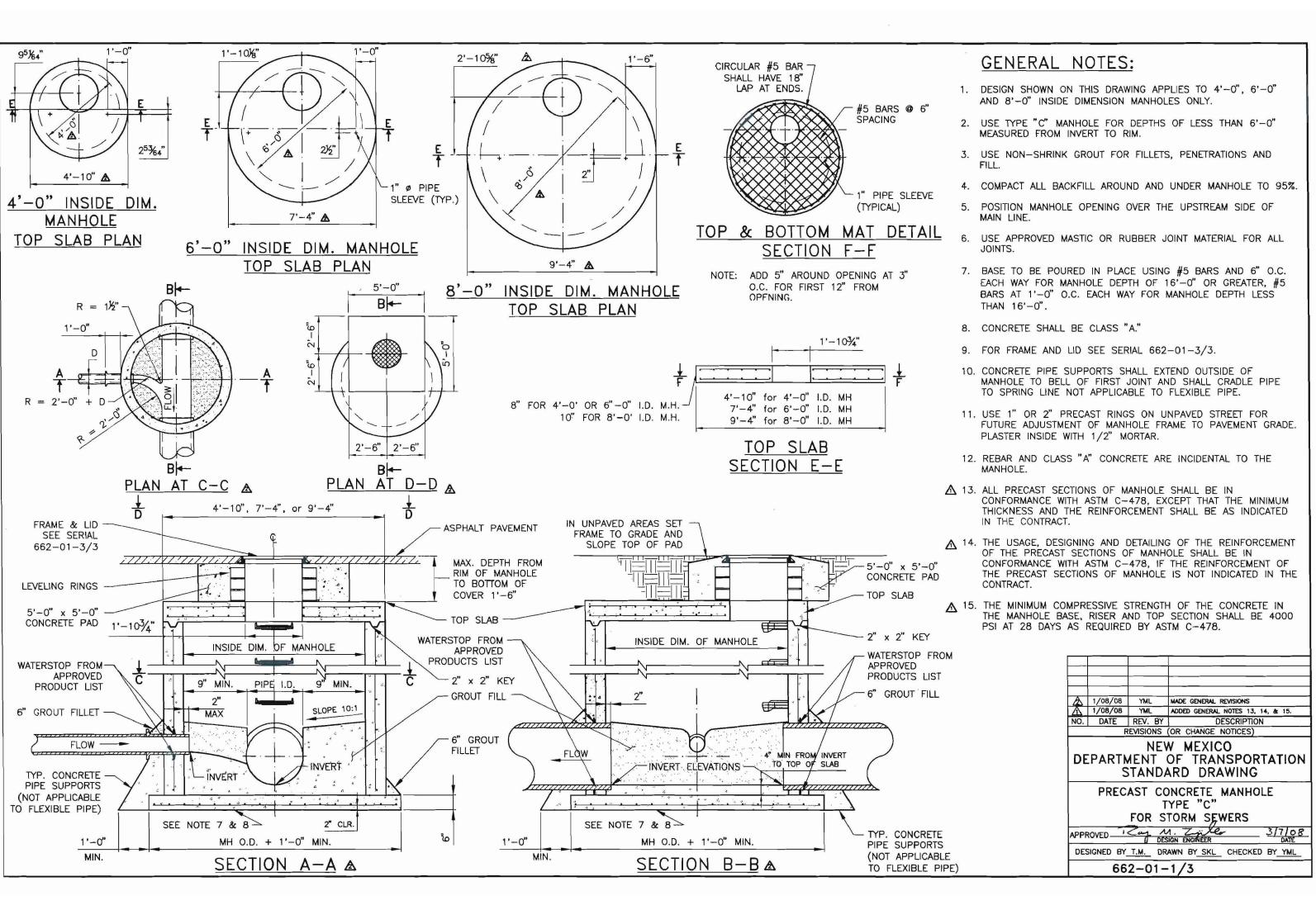


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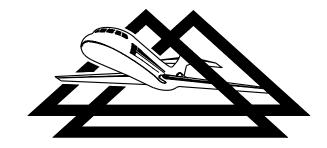




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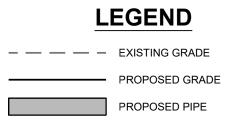
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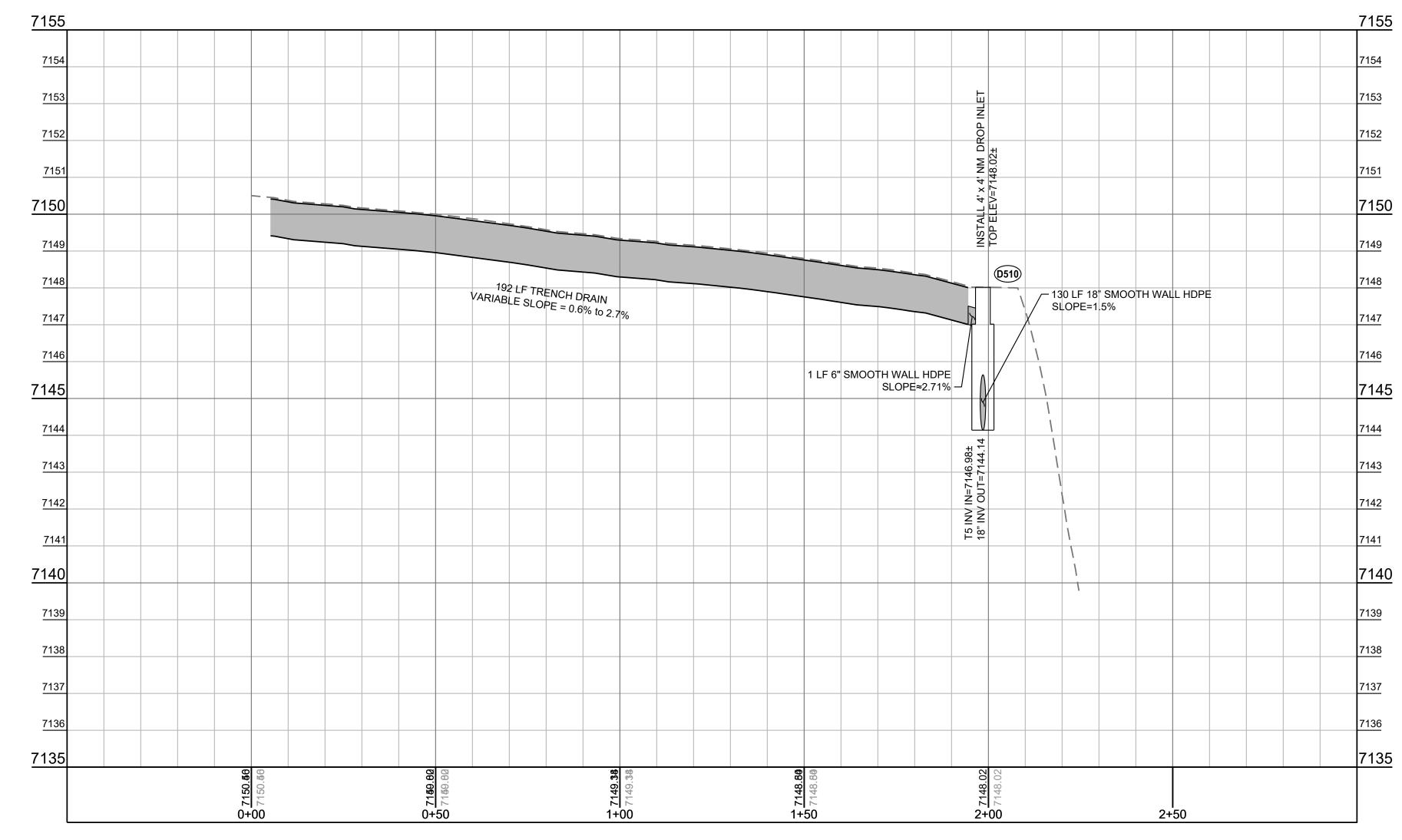
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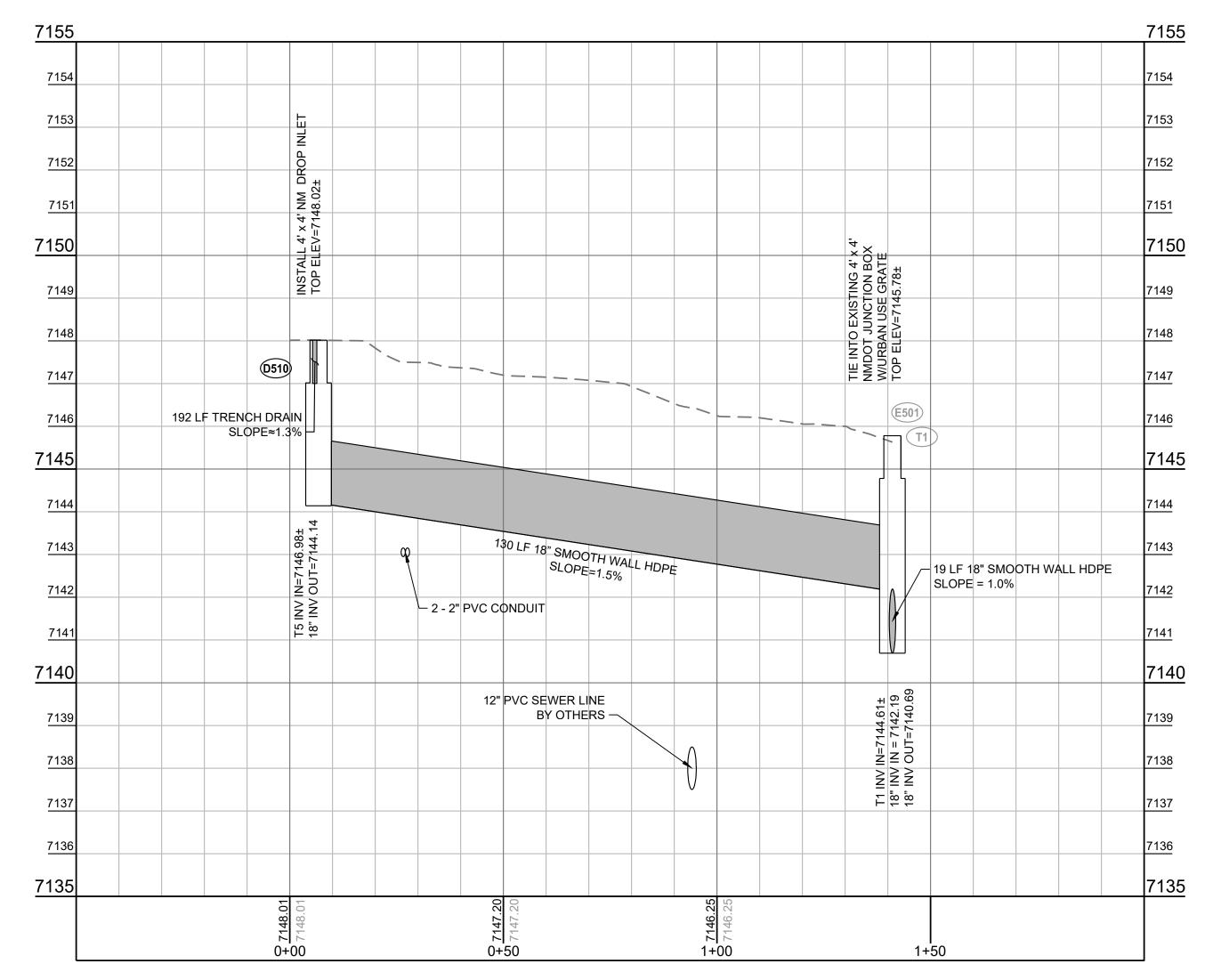




CONSTRUCT T-HANGARS		NMDOT-AD PROJECT NO. LAM-24-02	
	DRAWN BY:	MJH	SHEET
DRAINAGE DETAILS	DESIGNED E	DESIGNED BY: CAR/ABH	
OS ALAMOS COUNTY AIRPORT	SCALE: NONE	DATE: SEPTEMBER 2024	14







BASELINE "T5 - D510"

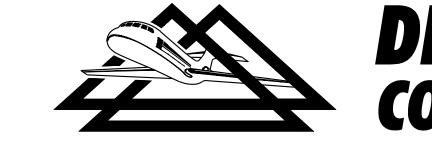
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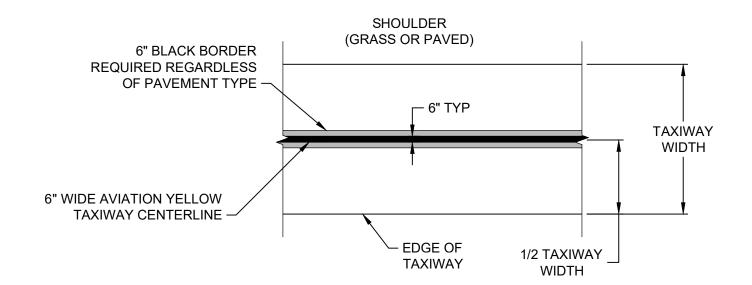
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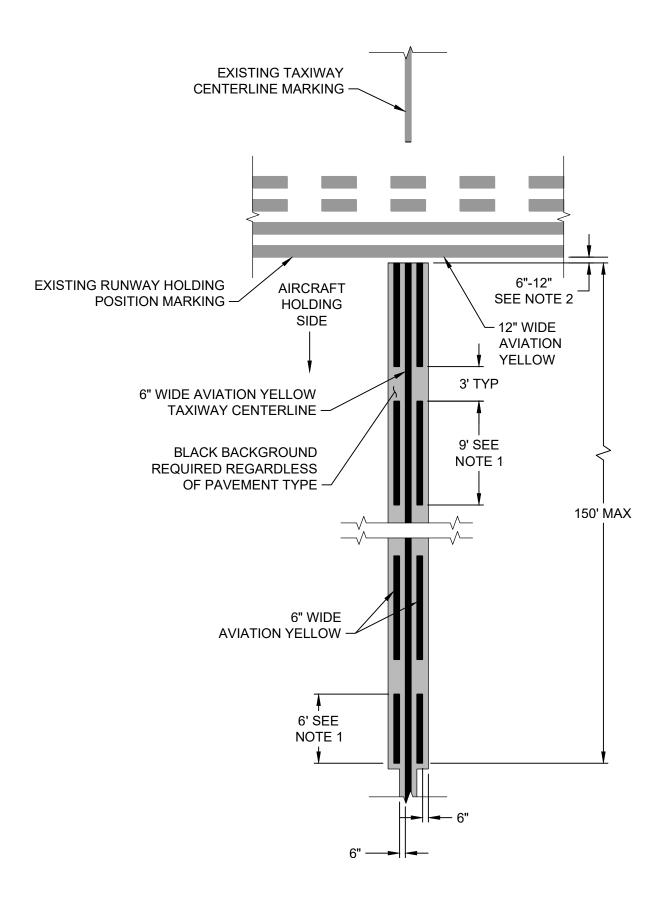


EN R. HAPPA
PORESSIONAL ENGINEER

CONSTRUCT T-HANGARS	NMDOT-AD F	JOB NO . 23063	
DRAINAGE PROFILES	DRAWN BY: MJH DESIGNED BY: CAR/ABH		SHEET 10 OF
LOS ALAMOS COUNTY AIRPORT	SCALE: HORIZ: 1" = 20' VERT: 1" = 2'	DATE: SEPTEMBER 2024	14



6" TAXIWAY CENTERLINE MARKING



ENHANCED TAXIWAY CENTERLINE MARKING

NOTES:

- 1. 9 FEET AND 6 FEET DIMENSION IS ALONG TAXIWAY CENTERLINE. IN SKEWED CONDITIONS, DIMENSION ON CENTER WILL BE THAT SHOWN. DIMENSION OF 6 INCHES WIDE DASHED ENHANCEMENT LINE WILL BE LONGER OR SHORTER THAN 9 FEET DEPENDING ON SIDE OF SKEW. DASHED LINE SHALL END AT SAME LOCATION RELATIVE TO THE CENTERLINE.
- 2. MATCH EXISTING FIELD CONDITIONS ELSEWHERE ON AIRPORT (6" SHOWN).

MARKING NOTES

- 1. THE CONTRACTOR SHALL SUBMIT DOCUMENTATION THAT THE PERSONNEL RESPONSIBLE FOR MARKING LAYOUT AND APPLICATION HAS A MINIMUM OF FIVE (5) YEARS OF SIMILAR EXPERIENCE WORKING ON AIRPORTS. THE CONTRACTOR SHALL PROVIDE REFERENCES TO SUBSTANTIATE THE REQUIRED EXPERIENCE.
- 2. ALL MARKINGS SHALL BE IN ACCORDANCE WITH FAA ADVISORY CIRCULAR (AC) 150/5340-1M, "STANDARDS FOR AIRPORT MARKINGS".
- 3. AFTER PROPER PAVEMENT CURE FOR BITUMINOUS PAVEMENT, ALL MARKINGS ON THE "MARKING LAYOUT" SHALL BE MARKED AT THE FULL RATE (115 SF/GAL) AND PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE FOOT FOR PERMANENT MARKING. REFERENCE IS DIRECTED TO ITEM P-620 OF THE SPECIFICATIONS.
- 4. REFLECTIVE MEDIA SHALL BE PROVIDED AS OUTLINED IN ITEM P-620. BLACK MARKINGS SHALL NOT BE REFLECTORIZED. MARKINGS WITHOUT BEADS MUST BE DRY PRIOR TO APPLYING MARKING WITH BEADS.
- 5. THE PAVEMENT SHALL BE THOROUGHLY CLEANED PRIOR TO THE APPLICATION OF THE MARKINGS. THE CLEANING SHALL BE ACCOMPLISHED WITH HIGH PRESSURE WATER; THE USE OF CHEMICALS WILL NOT BE PERMITTED.
- 6. ALL STATIONS AND OFFSETS ARE APPROXIMATE AND SHALL BE CONFIRMED IN THE FIELD BY THE CONTRACTOR.
- 7. ALL MARKINGS SHALL BE LAID OUT ON THE PAVEMENT BY THE CONTRACTOR FOR ACCEPTANCE BY THE ENGINEER PRIOR TO PLACEMENT.
- 8. PRIOR TO THE START OF WORK, A CONTROL STRIP SHALL BE APPLIED IN ACCORDANCE WITH ITEM P-620-3.7. ALL STRIPING EQUIPMENT SHALL BE CALIBRATED PRIOR TO THE TEST LINE APPLICATION. THE TEST SECTION SHALL INCLUDE THE APPLICATION OF A MINIMUM OF 5 GALLONS OF PAINT AND APPLICATION OF 50 POUNDS OF TYPE III GLASS BEADS. ALL TEST LINES WILL BE INSPECTED DURING DARKNESS PRIOR TO THE COMMENCEMENT OF
- 9. FINAL MARKINGS SHALL BE WITHIN THE TOLERANCES OUTLINED IN ITEM P-620-3.5.
- 10. EXCESS GLASS BEADS SHALL BE REMOVED BY VACUUM TRUCK, FOD BOSS, OR SIMILAR EQUIPMENT; BLOWING OF BEADS IS NOT ACCEPTABLE.

MARKING REMOVAL NOTES:

- 11. PAINT REMOVAL SHALL BE ACCOMPLISHED WITH EQUIPMENT AND IN A MANNER THAT WILL NOT DAMAGE THE PAVEMENT. APPROVED METHODS INCLUDE HIGH PRESSURE WATERBLASTING OR SCARIFICATION (GRINDING); THE USE OF CHEMICALS IS NOT PERMITTED. BLACKING OUT OF MARKINGS IS NOT PERMITTED. SCARIFICATION (GRINDING) SHALL BE LIMITED TO UNGROOVED PAVEMENTS OR TO REMOVING PAINT ABOVE THE SURFACE.
- 12. ANY DAMAGE CAUSED BY THE CONTRACTOR'S OPERATIONS SHALL BE CORRECTED AT THE CONTRACTOR'S EXPENSE AND IN A MANNER APPROVED BY THE ENGINEER.
- 13. THE CONTRACTOR SHALL PROVIDE CERTIFICATION THAT THE CONTRACTOR'S EQUIPMENT HAS BEEN USED IN THE PERFORMANCE OF A SIMILAR CONTRACT.
- 14. PRIOR TO THE START OF WORK, PAVEMENT MARKINGS SHALL BE REMOVED FROM A DESIGNATED TEST SECTION, NOT LESS THAN 50 SQUARE YARDS IN SIZE. THE METHOD AND EQUIPMENT USED FOR THE TEST SECTION SHALL BE THE SAME AS THAT INTENDED FOR THE REMAINDER OF THE WORK. THE TEST SECTION SHALL BE INSPECTED AND APPROVED BY THE ENGINEER PRIOR TO BEGINNING ANY FURTHER PAINT REMOVAL. REFERENCE IS DIRECTED TO ITEM P-619 OF THE SPECIFICATIONS.
- 15. DIFFERENT DEGREES OF PAINT REMOVAL SHALL BE REQUIRED AS SHOWN ON THE PLANS AND DEFINED BELOW:
- a. AT LEAST 95-100 PERCENT OF MARKINGS SHALL BE REMOVED WHEN THE MARKING IS OBSOLETE. b. THE REMOVAL LEVEL IS DEFINED SUCH THAT THE PAVEMENT IS CLEARLY EXPOSED TO THE DEGREE SPECIFIED. THE DEGREE WILL BE VERIFIED BY THE GRID METHOD AS OUTLINED IN ITEM P-619.
- 20. THE CONTRACTOR SHALL USE REBAR, METAL STRIPS, OR OTHER APPROVED METHODS TO PROTECT EXISTING JOINT SEAL DURING PAINT REMOVAL OPERATIONS.
- 21. WATER FOR THE CONTRACTOR'S USE IS AVAILABLE ON THE AIRFIELD. THE CONTRACTOR SHALL PROVIDE AN APPROVED METER AND METER ANY WATER USED. THE COST FOR THE WATER WILL BE CHARGED TO THE CONTRACTOR BY THE OWNER AND WILL BE BASED UPON THE RATES CHARGED TO THE OWNER.
- 22. USED OR WASTE WATER FROM PAINT REMOVAL OPERATIONS SHALL BE COLLECTED BY THE CONTRACTOR AND DISPOSED OF OFF-SITE IN ACCORDANCE WITH LOCAL, STATE, AND FEDERAL GUIDELINES.
- 23. ACCUMULATIONS OF WATER, DUST, OR OTHER RESIDUE RESULTING FROM THE PAINT REMOVAL SHALL BE REMOVED AS THE WORK PROGRESSES. PRIOR TO ANY PAINTING OPERATIONS, THE SURFACE SHALL BE FREE OF ANY DIRT, REMOVAL RESIDUE, OR OTHER CONTAMINANTS THAT WOULD PREVENT THE BOND OF THE NEW COATING TO THE PAVEMENT. QUALITY CONTROL MEASURES SHALL INCLUDE A SIMPLE "PULL TEST" WITH ADHESIVE MATERIAL; EVIDENCE OF EXCESSIVE DEBRIS ON THE ADHESIVE INDICATES THAT ADDITIONAL CLEANING SHALL BE REQUIRED.

BY APP. DATE **REVISIONS**

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CONSTRUCT T-HANGARS	NMDOT-AD PROJECT NO. LAM-24-02		
MARKING NOTES & DETAILS	DRAWN BY:	MJH	
MARKING NOTES & DETAILS	DESIGNED B	Y : Car/abh	
LOS ALAMOS COUNTY AIRPORT	SCALE:	DATE: SEPTEMBER 2024	

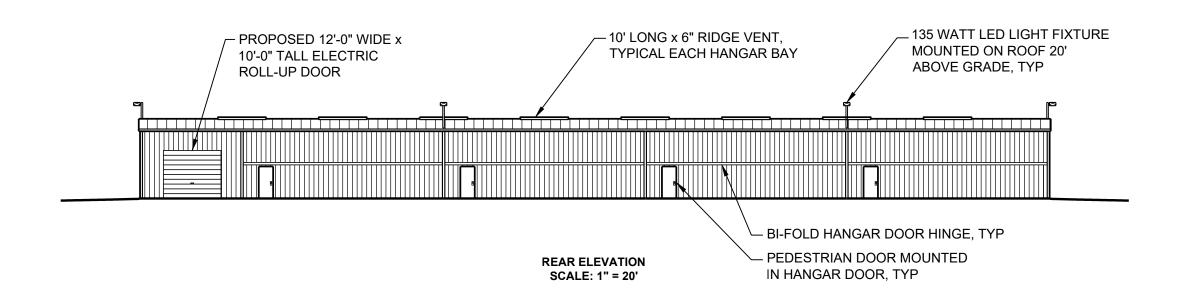
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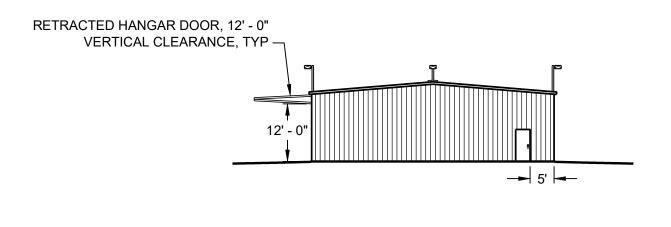
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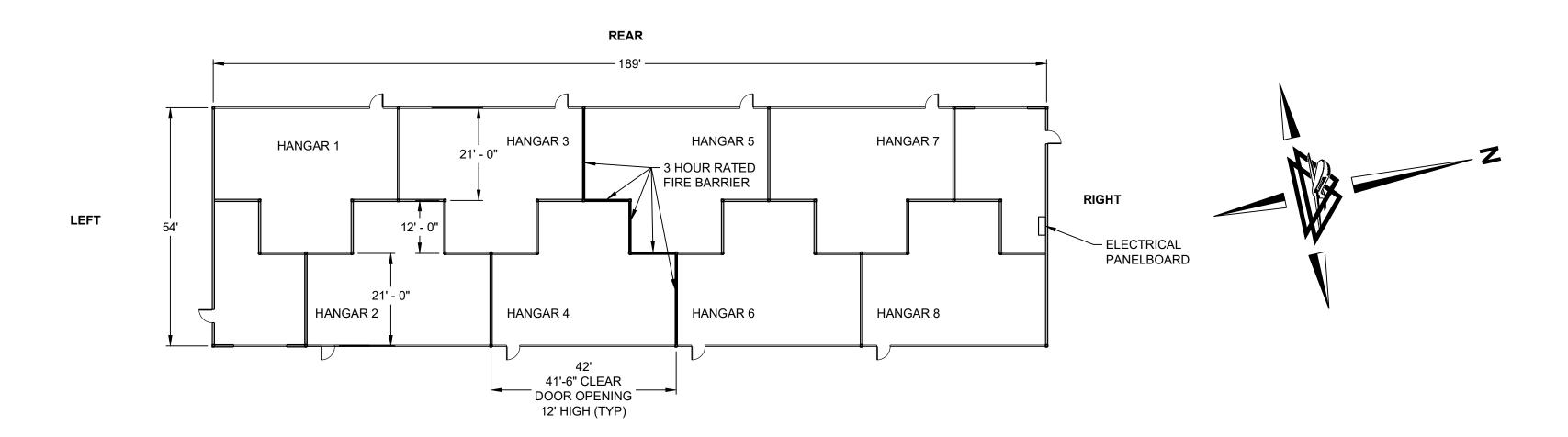
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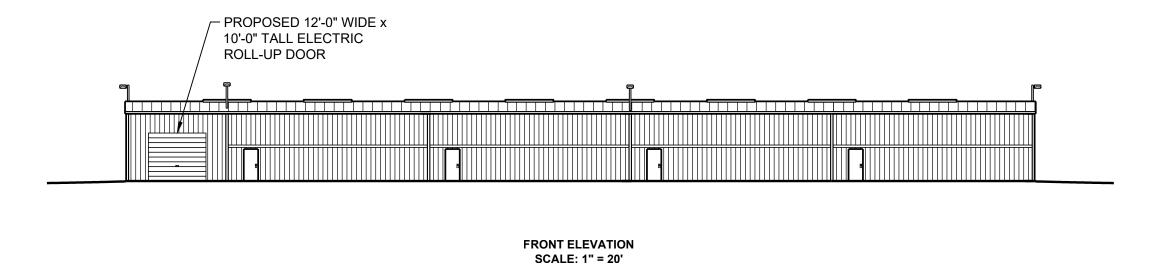
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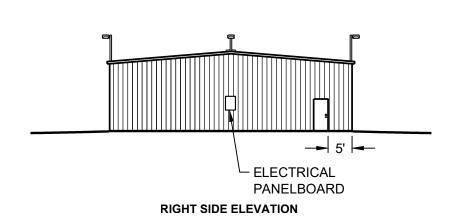
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NOTES:

- 1. SEE SPECIFICATION SECTION M-108 FOR HANGAR BUILDING SPECIFICATIONS.
- 2. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE FOLLOWING:
 - A. DESIGN AND CONSTRUCTION OF THE FOUNDATION FOR THE HANGAR BUILDING
 B. ERECTION OF THE HANGAR BUILDING IN ACCORDANCE WITH THE REQUIREMENTS OF IBC 2021
 C. DESIGN AND CONSTRUCTION OF ELECTRICAL SYSTEM FOR THE HANGAR BUILDING AS PER NFPA 409 (STANDARD ON AIRCRAFT HANGARS), THE NEC AND ALL LOCAL CODES.
- 3. DIMENSIONS OF HANGAR BUILDINGS ARE NOMINAL. FINAL DIMENSIONS SHALL BE BASED ON DESIGN DRAWINGS PROVIDED BY CONTRACTOR.
- 4. ALL INTERIOR PARTITION WALLS SHALL EXTEND FROM FLOOR TO ROOF.
- 5. THE CONTRACTOR SHALL CONSTRUCT A 3 HOUR RATED FIRE BARRIER IN THE LOCATION SHOWN ON THE BUILDING LAYOUT. THE FIRE BARRIER SHALL DIVIDE THE HANGAR BUILDING FROM FLOOR TO CEILING.
- 6. EXTERIOR LIGHTING SHALL BE BEACON ALPHA MODEL AL-X-60L-136-3K7-5X5-UNV OR EQUAL. FULL CUTOFFS SHALL BE PROVIDED FOR FIXTURES FACING RUNWAY.
- 7. THE CONTRACTOR SHALL BE RESPONSIBLE FOR ALL PERMITS AND FEES NECESSARY TO CONSTRUCT THE HANGAR BUILDING AND ASSOCIATED SITE WORK.
- 8. THE CONTRACTOR SHALL BE RESPONSIBLE FOR COORDINATING ALL UTILITY CONNECTIONS. ALL UTILITY CONNECTION FEES WILL BE PAID FOR BY THE OWNER.
- 9. HOLD ALL SHEATHING MATERIAL(FRP, GYPSUM BOARD) 1/2" ABOVE EXISTING PAVEMENT ELEVATION
- 10. FIBERGLASS GYPSUM BOARD USED IN THE 3-HOUR RATED FIRE BARRIER SHALL BE TAPED, FINISHED, AND PAINTED WITH TWO COATS OF EXTERIOR PAINT. ALL OTHER INTERIOR PARTITION WALLS WILL NOT USE TAPE, FINISH, OR PAINT.
- 11. PROVIDE AND INSTALL ONE (1) TEN (10) POUND ABC TYPE FIRE EXTINGUISHER WITH WALL HOOK IN EACH HANGAR. MOUNTING LOCATION SHALL BE WITHIN FIVE (5) FEET OF THE PEDESTRIAN DOOR.
- 12. HANGAR IS NOT BEING INSTALLED ON A LEVEL FOUNDATION. CONTRACTOR SHALL PROVIDE APPROPRIATE MEASURES AT EACH HANGAR DOOR TO PROVIDE A WATER-TIGHT SEAL ALONG THE BOTTOM OF EACH DOOR. 12'-0" VERTICAL CLEARANCE WHEN OPEN IS THE MINIMUM AT EACH HANGAR.
- 13. CONTRACTOR SHALL PROVIDE APPROPRIATE MEASURES AT THE SOUTH-FACING HANGAR WALL TO PROVIDE A WATER-TIGHT SEAL ALONG THE BOTTOM OF THE WALL.
- 14. CONTRACTOR SHALL BE RESPONSIBLE TO ENSURE DOORS MOVE FREELY AND DO NOT BIND.
- 15. THE DRAWINGS SHOWN ON THIS SHEET ARE FOR GUIDELINE PURPOSES ONLY. CONTRACTOR SHALL PROVIDE PERMIT DRAWINGS WHICH SHALL BE SEALED BY A LICENSED ARCHITECT AND/OR ENGINEER IN THE STATE OF NEW MEXICO AS REQUIRED BY ANY AUTHORITIES HAVING JURISDICTION. REFERENCE IS ALSO DIRECTED TO THE PROJECT SPECIFICATIONS FOR FURTHER HANGAR PERFORMANCE REQUIREMENTS.

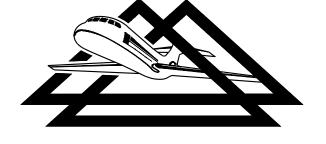
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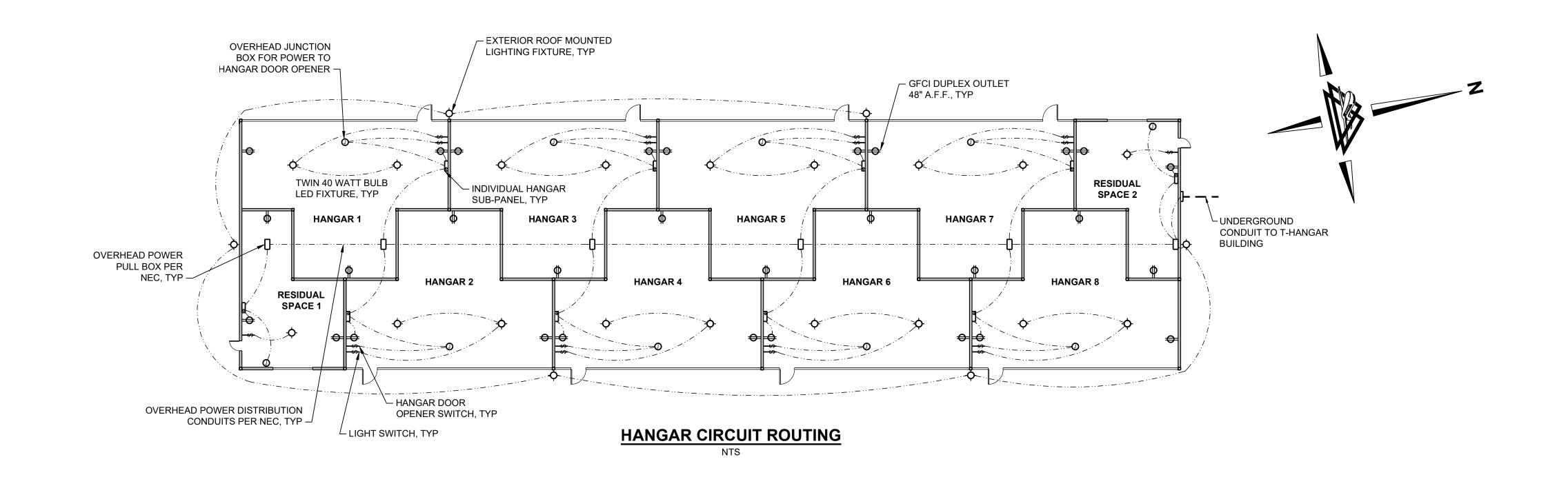


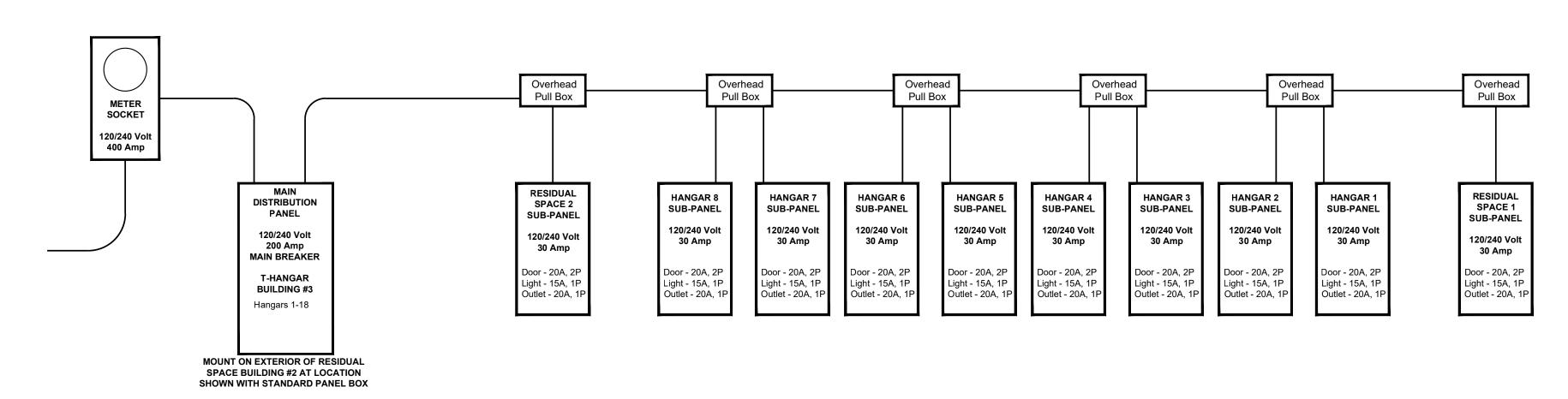
CONSTRUCT T-HANGARS	NMDOT-AD PROJECT NO. LAM-24-02	JOB NO 230
T-HANGAR LAYOUT	DRAWN BY:	SHEET
I-HANGAK LATOUT	DESIGNED BY: CAR/ABH	OF

SCALE:

AS SHOWN

SEPTEMBER 2024





POWER DISTRIBUTION RISER DIAGRAM

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NOTE:

THE ELECTRICAL DRAWINGS SHOWN ON THIS SHEET ARE FOR GUIDELINE PURPOSES ONLY. CONTRACTOR SHALL PROVIDE PERMIT ELECTRICAL DRAWINGS WHICH SHALL BE SEALED BY A LICENSED ENGINEER IN THE STATE OF NEW MEXICO AS REQUIRED BY ANY AUTHORITIES HAVING JURISDICTION. REFERENCE IS ALSO DIRECTED TO THE PROJECT SPECIFICATIONS FOR FURTHER PERFORMANCE REQUIREMENTS.

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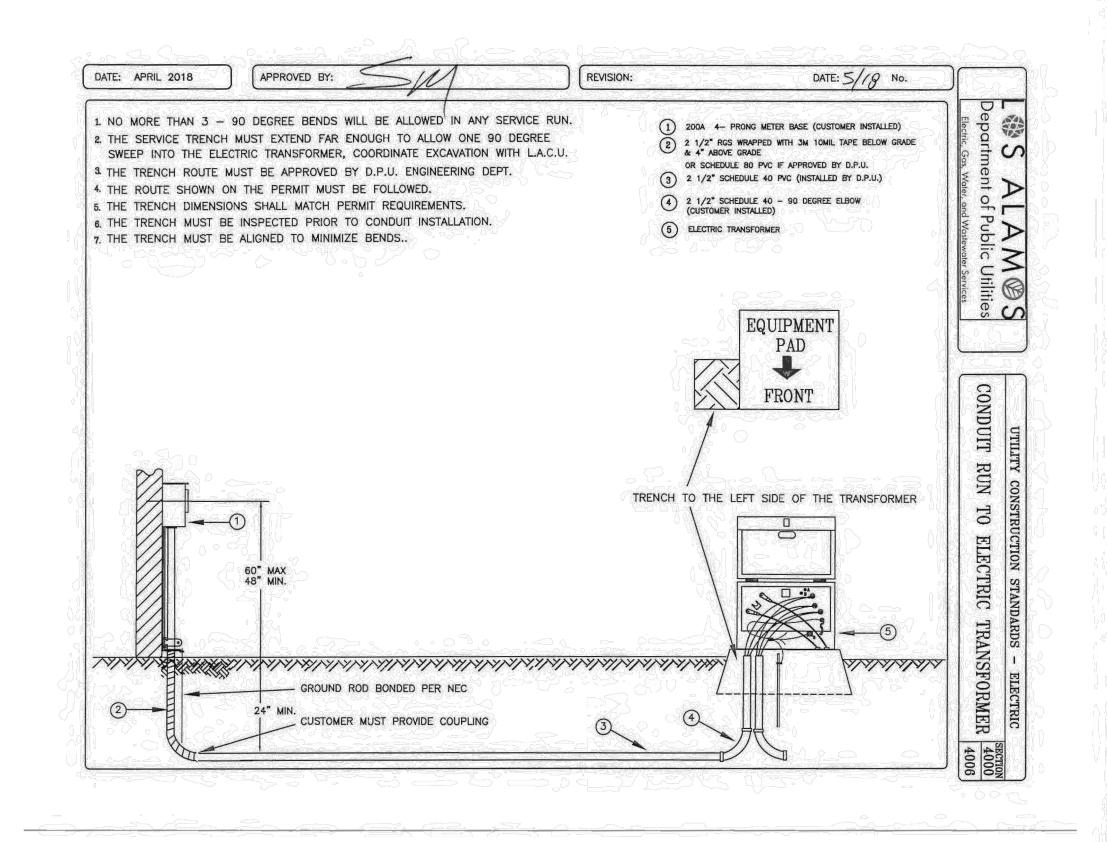
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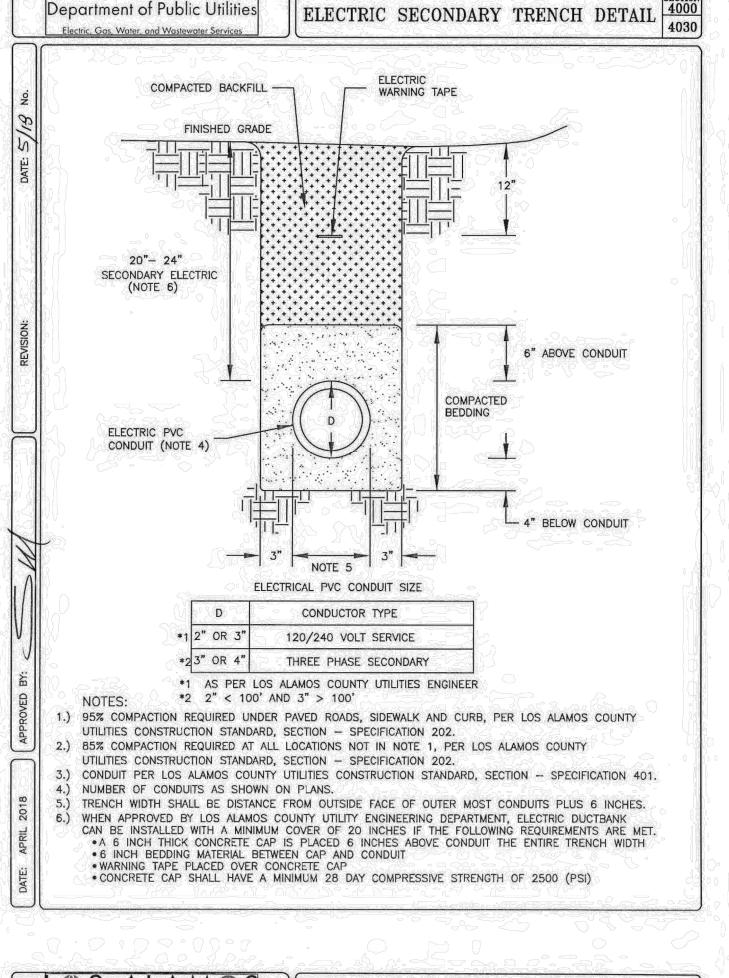


DELTA AIRPORT	
CONSULTANTS, INC.	

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CONSTRUCT T-HANGARS	NMDOT-AD PROJECT NO. LAM-24-02		JOB NO. 23063
T-HANGAR DETAILS	DRAWN BY:	MJH	SHEET 13
LOS ALAMOS COUNTY AIRPORT	SCALE: NONE	DATE: SEPTEMBER 2024	14

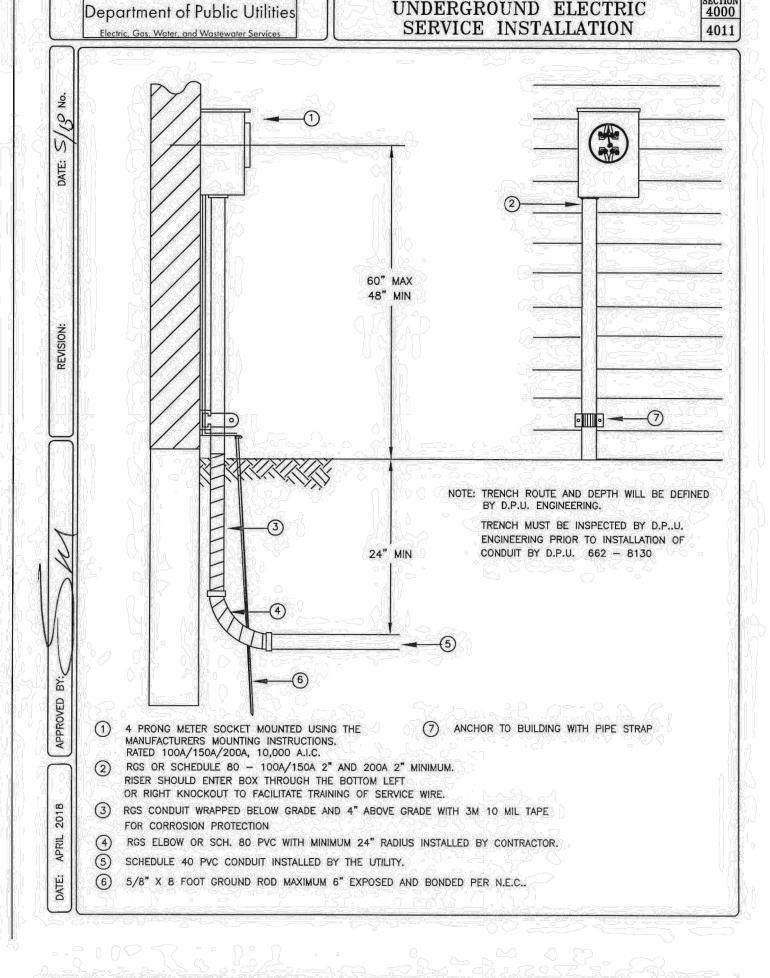




UTILITY CONSTRUCTION STANDARDS - ELECTRIC

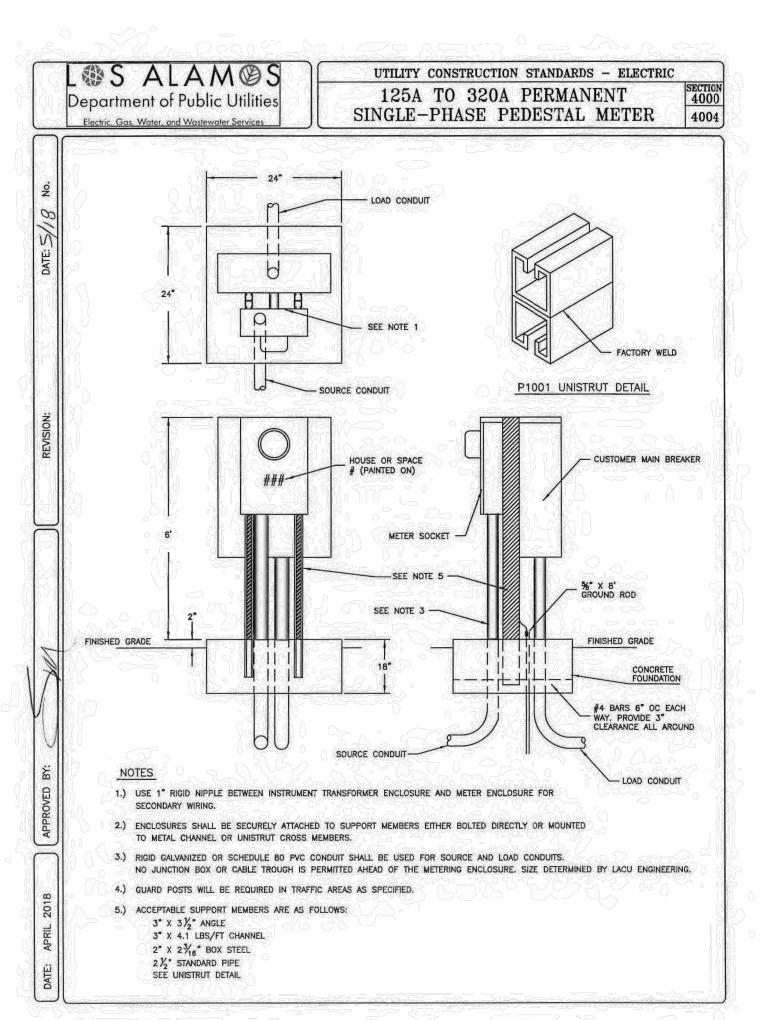
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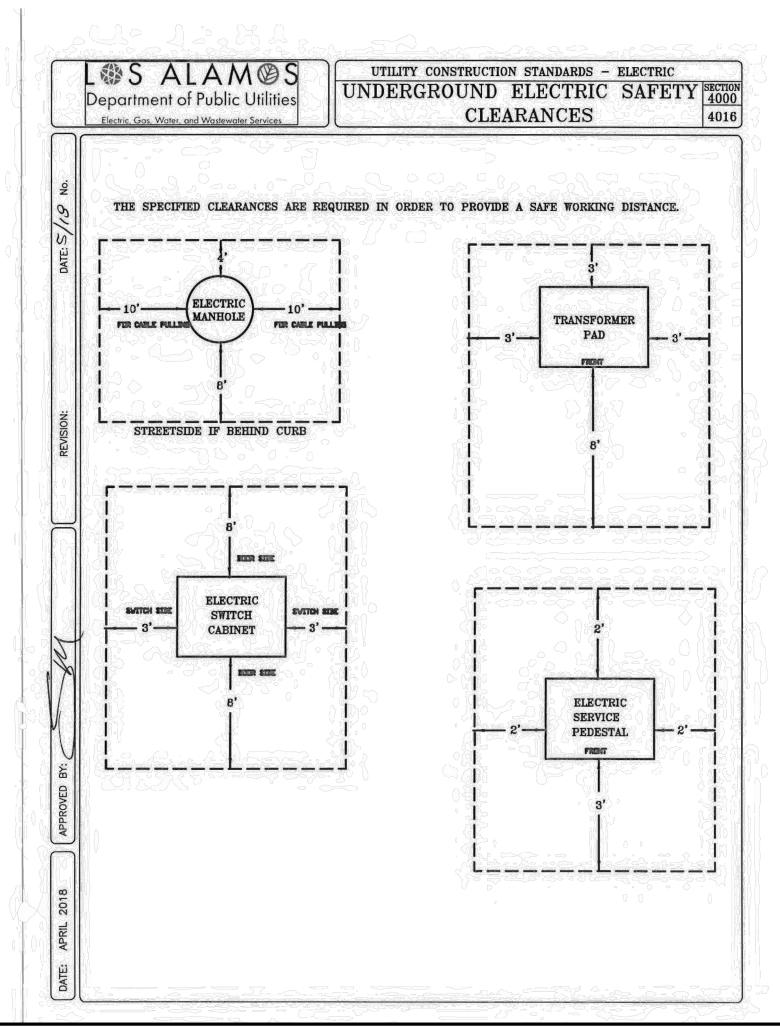
Department of Public Utilities

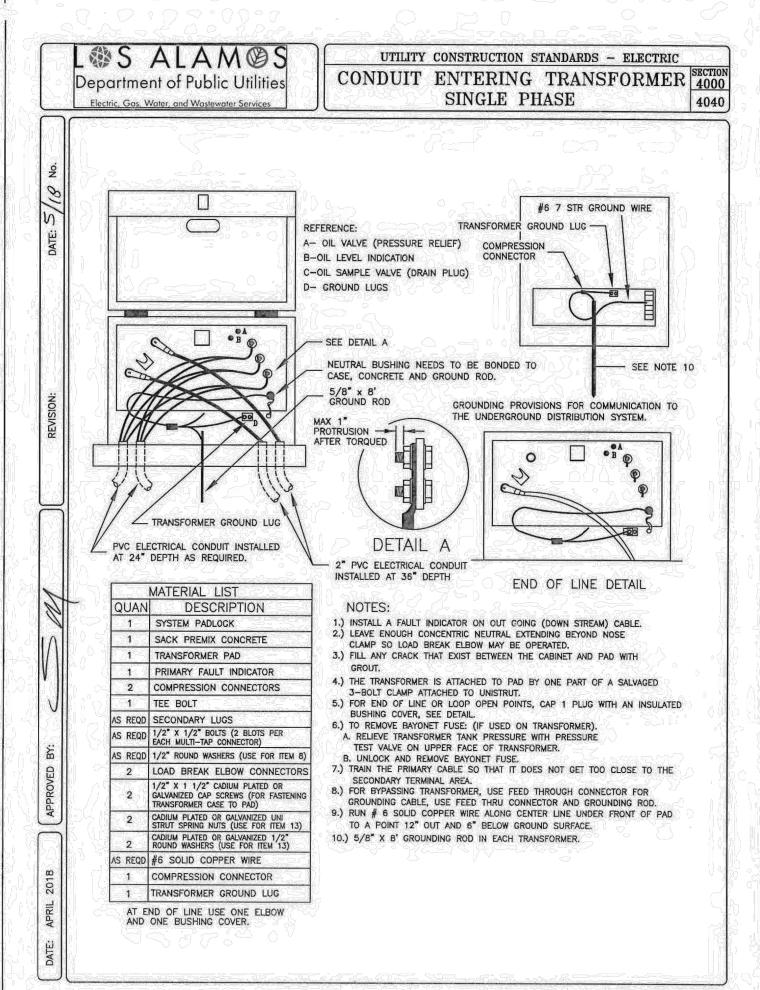


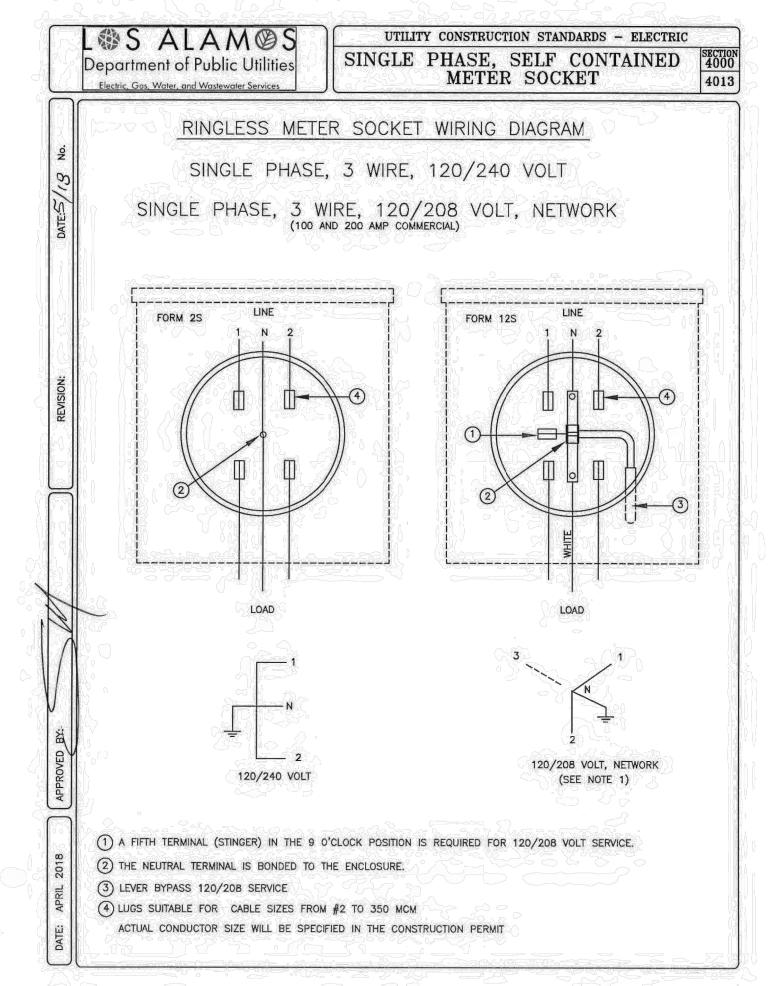
UTILITY CONSTRUCTION STANDARDS - ELECTRIC

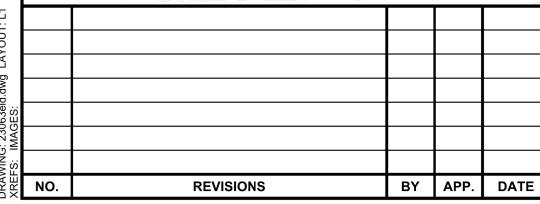
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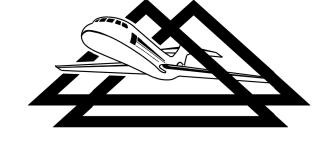






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CONSTRUCT T-HANGARS		NMDOT-AD PROJECT NO. LAM-24-02	
ELECTRICAL DETAILS	DRAWN BY:	DRAWN BY: MJH DESIGNED BY: CAR/ABH	
ELECTRICAL DETAILS	DESIGNED I		
OS ALAMOS COUNTY AIRPORT	SCALE: NONE	DATE: SEPTEMBER 2024	14