

LOS ALAMOS COUNTY AIRPORT

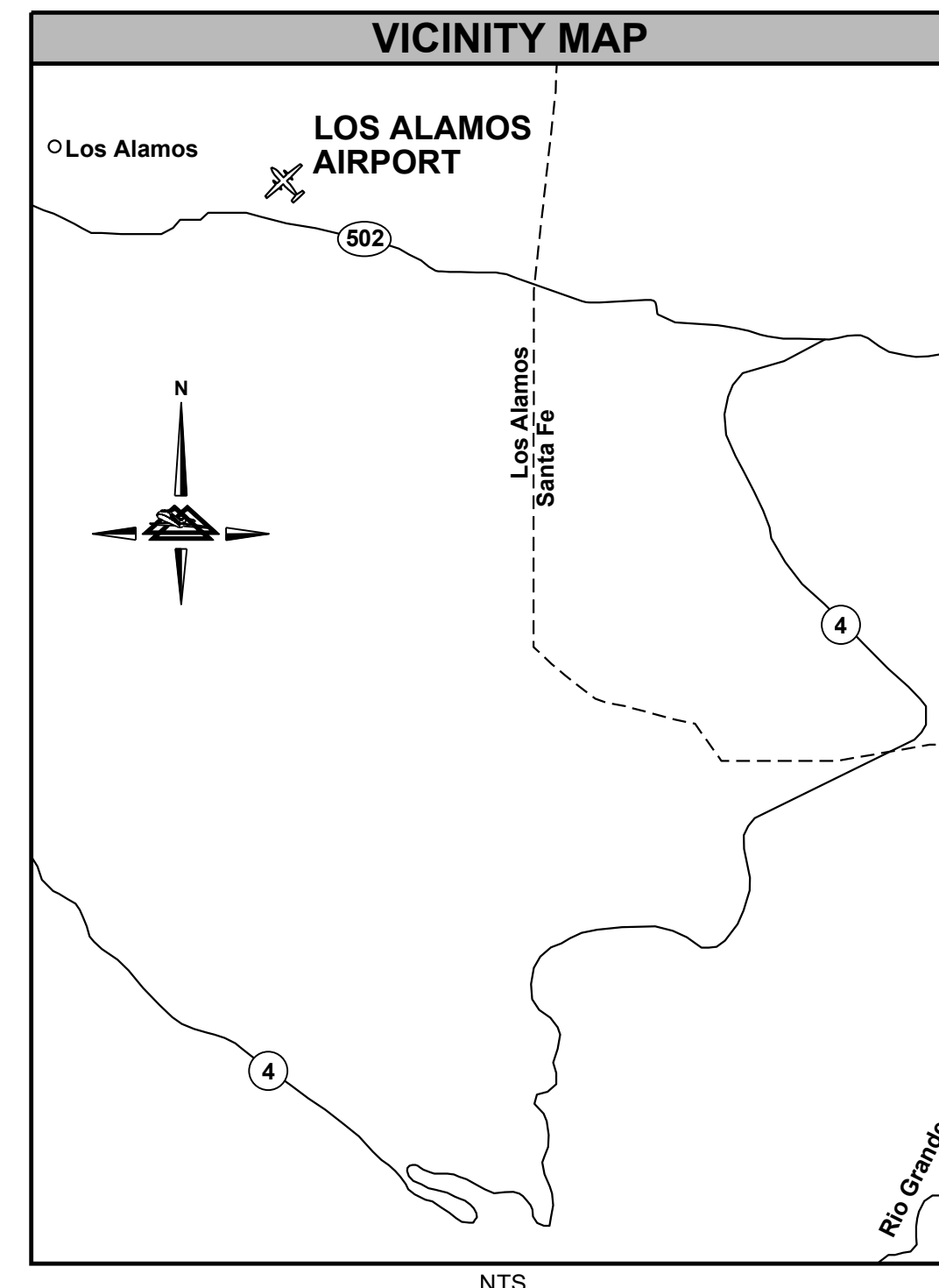
LOS ALAMOS, NEW MEXICO



CONSTRUCT T-HANGARS

NMDOT-AD PROJECT NO. LAM-24-02
 COUNTY OF LOS ALAMOS BID NO. IFB25-15
 DELTA PROJECT NO. 23063

FOR THE
 INCORPORATED COUNTY OF LOS ALAMOS
 SEPTEMBER 2024



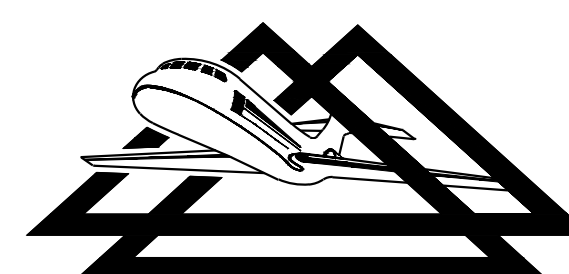
SHEET INDEX	
SHEET #	TITLE
1	COVER SHEET
2	GENERAL LAYOUT & PROJECT NOTES
3	PHASING LAYOUT
4	PHASING NOTES & DETAILS
5	DEMOLITION PLAN
6	GEOMETRIC & MARKING LAYOUT
7	GRADING & DRAINAGE
8	DRAINAGE DETAILS
9	DRAINAGE DETAILS
10	DRAINAGE PROFILES
11	MARKING NOTES & DETAILS
12	T-HANGAR LAYOUT
13	T-HANGAR DETAILS
14	ELECTRICAL DETAILS

AS-BID
 FOR BIDDING PURPOSES
 DO NOT USE FOR CONSTRUCTION
 CONFORMED DOCUMENTS WILL BE
 ISSUED SEPARATELY FOR CONSTRUCTION
 SEPTEMBER 2024

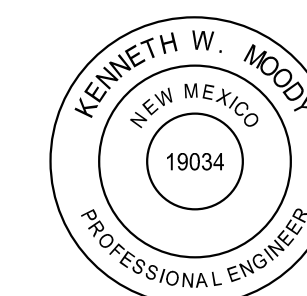
DRAWING PREPARED BY: LAYOUT, L1
 XREFS: IMAGES: LAMNEW LOGO.DWG

NO.	REVISIONS	BY	APP.	DATE

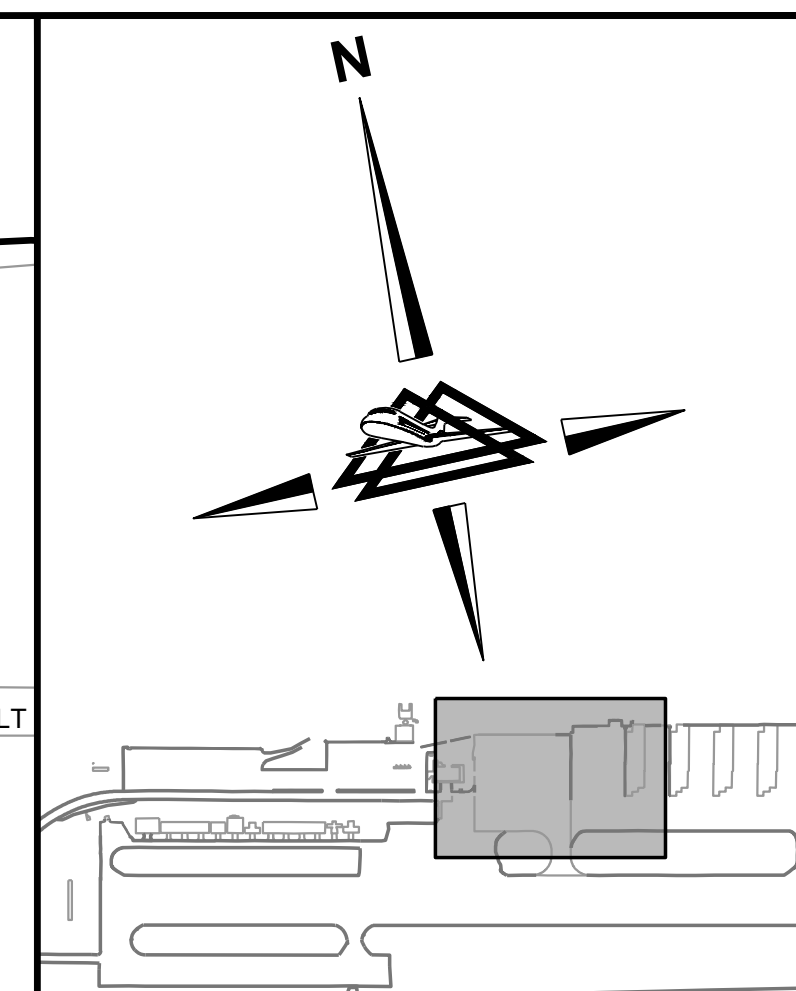
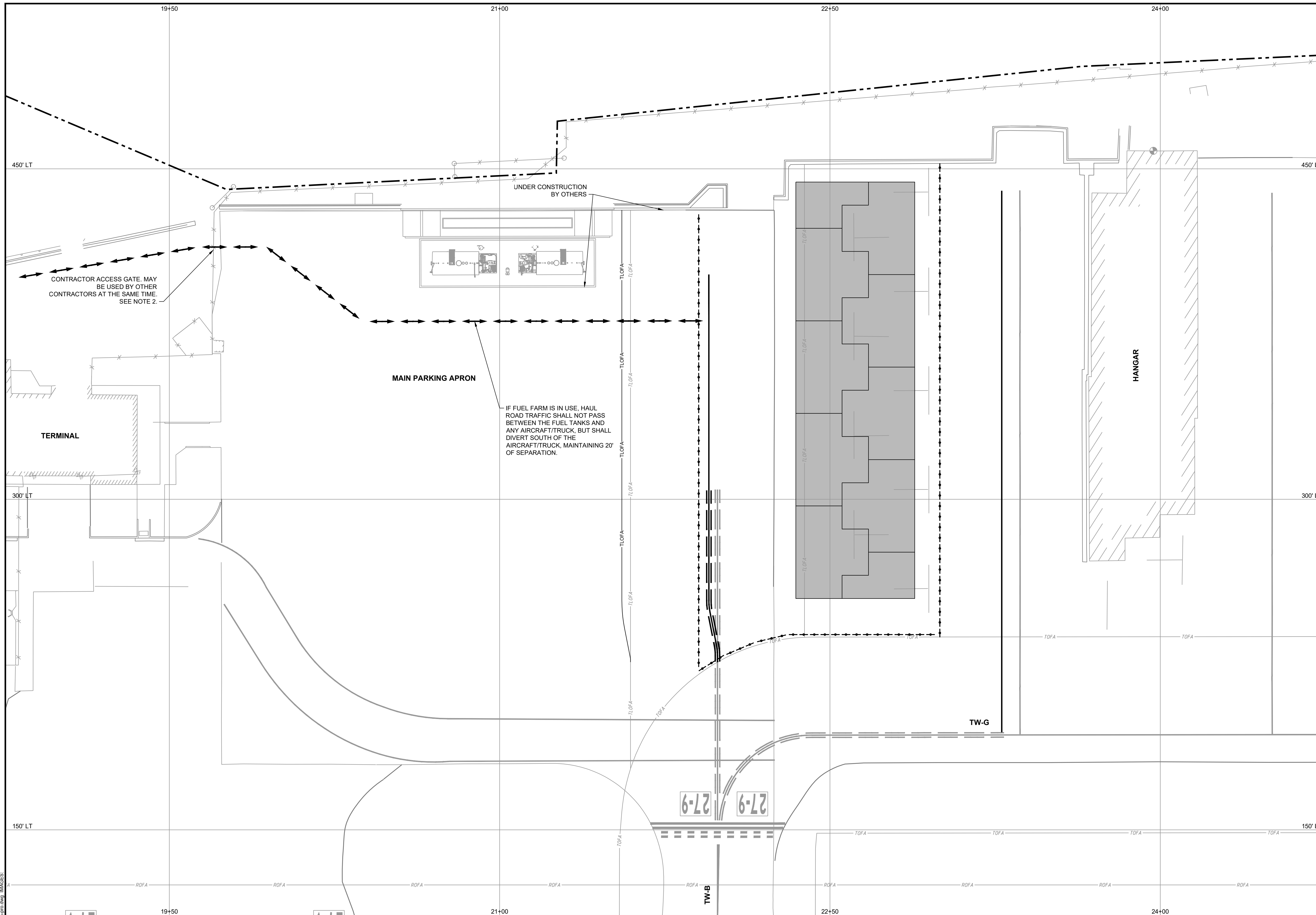
7804 Pan American East Freeway NE, Suite 4
 Albuquerque, New Mexico 87109
 phone: (505) 797-4921 • fax: (505) 797-1725
 www.deltaairport.com



**DELTA AIRPORT
 CONSULTANTS, INC.**



CONSTRUCT T-HANGARS	NMDOT-AD PROJECT NO. LAM-24-02	JOB NO. 23063
COVER SHEET	DRAWN BY: MJH	SHEET 1 OF 14
	DESIGNED BY: CAR/ABH	
LOS ALAMOS COUNTY AIRPORT	SCALE: NONE	DATE: SEPTEMBER 2024



SHEET INDEX

LEGEND

- AVIATION BARRICADES
- HAUL ROUTE (TWO-WAY TRAFFIC)
- EXISTING FENCE
- EXISTING AIRPORT PROPERTY LINE
- EXISTING RUNWAY OBJECT FREE AREA
- EXISTING RUNWAY PROTECTION ZONE
- EXISTING TAXILANE OBJECT FREE AREA
- PROPOSED TAXILANE OBJECT FREE AREA
- EXISTING TAXIWAY OBJECT FREE AREA
- EXISTING MARKING
- PROPOSED MARKING
- PROPOSED CONSTRUCTION

NOTES

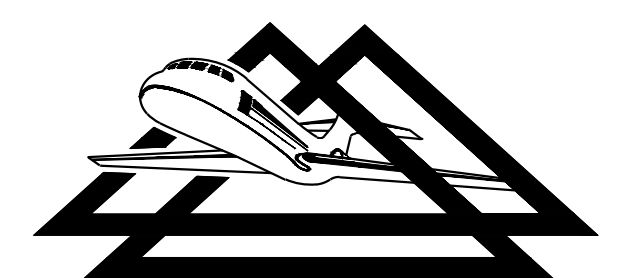
1. ALL STATIONS AND OFFSETS GIVEN WITH RESPECT TO BASELINE "RW" (RUNWAY 9-27 CENTERLINE) UNLESS OTHERWISE NOTED.
2. REFER TO "GENERAL LAYOUT & PROJECT NOTES" FOR CONTRACTOR HAUL ROUTE, STAGING AREA, AND EMPLOYEE PARKING.



DRAWING: 23063phs.dwg, 11/10/17, L1, XREFS: 23063phs.dwg, 23063phs.dwg, 23063phs.dwg, 23063phs.dwg

NO.	REVISIONS	BY	APP.	DATE

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**DELTA AIRPORT
 CONSULTANTS, INC.**



CONSTRUCT T-HANGARS	NMDOT-AD PROJECT NO. LAM-24-02	JOB NO. 23063
	DRAWN BY: MJH	SHEET 3
	DESIGNED BY: CAR/ABH	OF 14
PHASING LAYOUT		
LOS ALAMOS COUNTY AIRPORT		
SCALE: 1" = 20'	DATE: SEPTEMBER 2024	

PHASING NOTES:

GENERAL

1. THE CONTRACTOR SHALL HAVE 100 CALENDAR DAYS TO COMPLETE THE PROJECT.
2. LIQUIDATED DAMAGES IN THE AMOUNT OF TWO THOUSAND DOLLARS (\$2,000.00) PER CALENDAR DAY WILL BE ASSESSED AGAINST THE CONTRACTOR FOR EACH CALENDAR DAY, OR PORTION THEREOF, THAT THE TOTAL CONTRACT TIME IS EXCEEDED.
3. THE INTENT OF THIS PHASING PLAN IS TO MINIMIZE THE IMPACT OF CONSTRUCTION ON THE OPERATION OF THE AIRPORT. ALL REQUIREMENTS CONTAINED IN THE CONTRACT DOCUMENTS WILL BE STRICTLY ENFORCED.
4. THE CONTRACTOR SHALL SCHEDULE THE WORK TO MINIMIZE CONSTRUCTION IMPACTS ON AIRPORT OPERATIONS. THE CONTRACTOR SHALL PROVIDE THE OWNER WITH A SEVEN (7) DAY ADVANCE NOTICE OF INTENT TO START WORK. THE OWNER WILL ISSUE THE APPROPRIATE NOTICE-TO-AIRMEN (NOTAMS).
5. THE CONTRACTOR SHALL NOT MAKE ANY CHANGES TO THE PHASING PLAN WITHOUT APPROVAL OF THE ENGINEER AND OWNER.
6. PRIOR TO SUBMITTAL OF THE NOTICE OF INTENT TO START WORK, SUBMITTAL AND ACCEPTANCE OF ALL SHOP DRAWINGS FOR ALL ITEMS INCLUDED IN THE WORK SHALL BE COMPLETED.
7. THE CONTRACTOR SHALL SUBMIT A DETAILED SCHEDULE TO THE ENGINEER A MINIMUM OF SEVEN (7) DAYS PRIOR TO THE PRE-CONSTRUCTION CONFERENCE. THE SCHEDULE SHALL CLEARLY DELINEATE THE PROPOSED WORK AREAS AND THE NUMBER OF DAYS ANTICIPATED FOR THE COMPLETION OF EACH WORK AREA. REFERENCE IS DIRECTED TO THE SPECIAL PROVISIONS OF THE SPECIFICATIONS.
8. THE CONTRACTOR SHALL MARK AND LIGHT AREAS UNDER CONSTRUCTION IN ACCORDANCE WITH THE PLANS AND SPECIFICATIONS. AIRCRAFT AND PUBLIC SAFETY SHALL NOT BE COMPROMISED, AND PROPER SEPARATION SHALL BE MAINTAINED AT ALL TIMES.
9. BARRICADES SHALL BE INSTALLED AS THE FIRST TASK AND REMOVED AS THE LAST TASK. AVIATION BARRICADES SHALL NOT BE REMOVED UNTIL THE PAVEMENT AND SAFETY AREAS ARE ESTABLISHED IN ACCORDANCE WITH THE PLANS AND SPECIFICATIONS AND ARE SUITABLE FOR USE BY AIRCRAFT.
10. THE CONTRACTOR SHALL BE REQUIRED TO RELOCATE THE AVIATION BARRICADES DURING THE PROJECT. MULTIPLE RELOCATION OF THE BARRICADES SHALL BE CONSIDERED INCIDENTAL TO THE PROJECT.
11. AIRCRAFT SHALL HAVE THE RIGHT-OF-WAY AT ALL TIMES. CONTRACTOR VEHICLES SHALL GIVE WAY TO AIRCRAFT AT ALL TIMES.
12. ANY REOPENING OF TEMPORARILY CLOSED AIRFIELD PAVEMENTS SHALL BE COORDINATED IN ADVANCE WITH THE OWNER. PRIOR TO REOPENING PAVEMENT TO AIRCRAFT OPERATIONS, THE CONTRACTOR SHALL CLEAN ALL DEBRIS (FOD) FROM THE PAVEMENT SURFACES, RESTORE MARKINGS, AND ESTABLISH THE SAFETY AREAS IN ACCORDANCE WITH THE SPECIFICATIONS. THE OWNER AND ENGINEER SHALL BE NOTIFIED SUFFICIENTLY IN ADVANCE OF THE OPENING TIME TO ALLOW FOR INSPECTION AND ANY REMEDIAL CLEANING.
13. REFER TO THE PHASING DETAILS SHEET FOR DETAILS OF AVIATION BARRICADES AND FAR PART 77 SURFACES.
14. REFER TO THE SPECIFICATIONS AND THE CONSTRUCTION SAFETY AND PHASING PLAN (CSPP) FOR THE CONTRACTOR'S RESPONSIBILITIES FOR OPERATIONAL SAFETY ON AIRPORTS DURING CONSTRUCTION.
15. THE CONTRACTOR IS RESPONSIBLE FOR COMPLIANCE WITH FAA ADVISORY CIRCULAR (AC) 150/5370-2 "OPERATIONAL SAFETY ON AIRPORTS DURING CONSTRUCTION" AND THE PROJECT CONSTRUCTION SAFETY AND PHASING PLAN (CSPP).
16. THE CONSTRUCTION SHALL SUBMIT A SAFETY PLAN COMPLIANCE DOCUMENT (SPCD) IN ACCORDANCE WITH THE SPECIAL PROVISIONS OF THE SPECIFICATIONS AND THE CURRENT VERSION OF FAA ADVISORY CIRCULAR (AC) 150/5370-2 "OPERATIONAL SAFETY ON AIRPORTS DURING CONSTRUCTION".
17. THE CONTRACTOR SHALL PROVIDE THE ENGINEER AND OWNER WITH A SEVEN (7) DAY WRITTEN ADVANCE NOTICE OF INTENT TO BEGIN EACH WORK AREA. THE OWNER WILL ISSUE THE APPROPRIATE NOTICE-TO-AIRMEN (NOTAMS).
18. THE CONTRACTOR SHALL SCHEDULE AND CONDUCT WEEKLY SAFETY/PROGRESS MEETINGS WITH THE ENGINEER AND OWNER.
19. ALL CONTRACTOR VEHICLES SHALL BE MARKED WITH YELLOW FLASHING STROBE LIGHTS OR 3' X 3' ORANGE AND WHITE CHECKER FLAGS WHEN OPERATING ON THE AIRFIELD.

WORK AREA NOTES:

GENERAL

20. PRIOR TO STARTING WORK, THE CONTRACTOR SHALL COORDINATE WITH THE OWNER AND CONFIRM THAT THE PROPER NOTAMS HAVE BEEN ISSUED AND ARE ON FILE. ALL NOTAMS SHALL BE ISSUED BY THE OWNER.
21. THE CONTRACTOR SHALL PROVIDE THE ENGINEER AND OWNER WITH A 7 DAY WRITTEN ADVANCE OF INTENT TO BEGIN WORK
22. ALL MATERIALS SHALL BE APPROVED AND ON-SITE PRIOR TO THE START OF WORK
23. THE CONTRACTOR'S PERSONNEL AND EQUIPMENT SHALL REMAIN WITHIN THE WORK AREA LIMITS AND DESIGNATED HAUL ROUTES AT ALL TIMES

WORK AREA: HANGAR CONSTRUCTION AND APRON MARKING

LIMITATIONS:	NONE
MILESTONE TIME:	100 CALENDAR DAYS
MILESTONE LIQUIDATED DAMAGES:	\$2,000.00 PER CALENDAR DAY
ADVANCE NOTICE:	7 CALENDAR DAYS (WRITTEN) 48 HOURS
AOA CLOSURES:	NONE
AOA CLOSURE PERIODS:	NONE

NOTICE / OTHER WORK REQUIREMENTS BEFORE STARTING

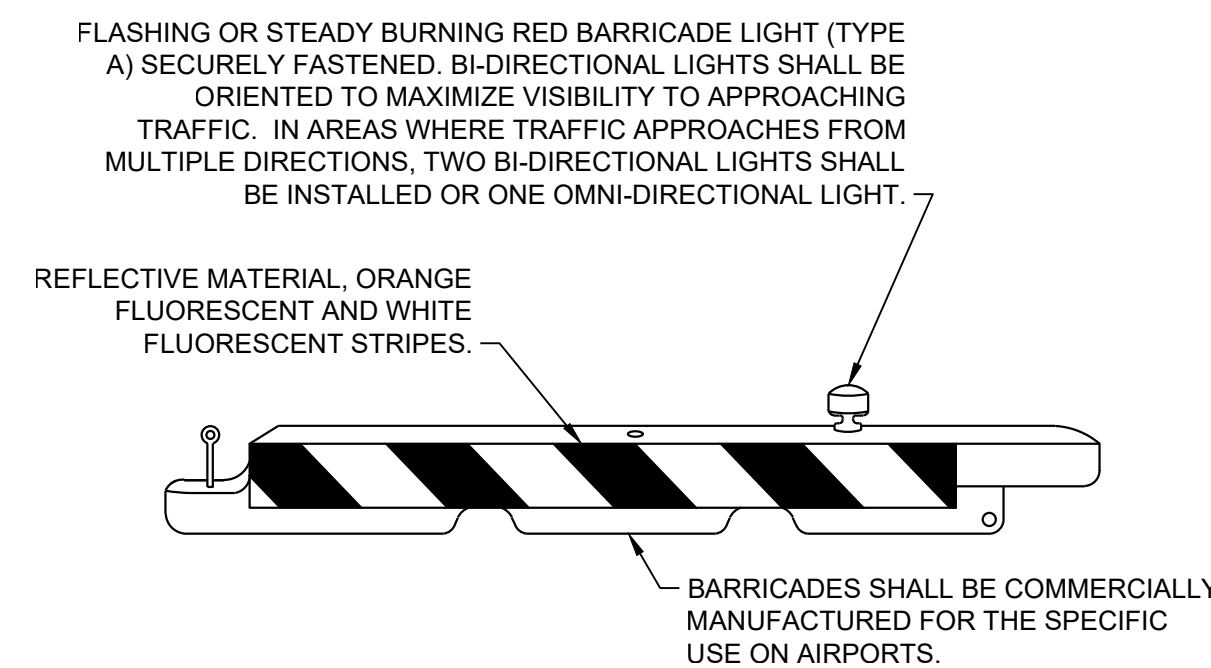
- INSTALL ALL LOW-PROFILE BARRICADES PER THE PHASING PLAN FOR PHASE 1.

REQUIRED WORK TO BE COMPLETED

- MARKING REMOVAL ON HANGAR SITE AND ADJACENT TAXILANES
- INSTALLATION OF UTILITIES AND INFRASTRUCTURE
- CONSTRUCTION OF T-HANGAR FOUNDATION
- ERECTION OF T-HANGAR BUILDING
- INSTALLATION OF T-HANGAR ELECTRICAL AND LIGHTING
- MARKING OF ADJACENT TAXILANE PAVEMENTS

NOTES

- AIRFIELD PAVEMENTS SHALL NOT BE PERMITTED TO REOPEN UNTIL THE SAFETY AREA REQUIREMENTS ARE MET AND THE PAVEMENT HAS BEEN CLEARED OF FOD.



WATER FILLED AVIATION BARRICADE DETAIL

NTS
(LOW PROFILE)

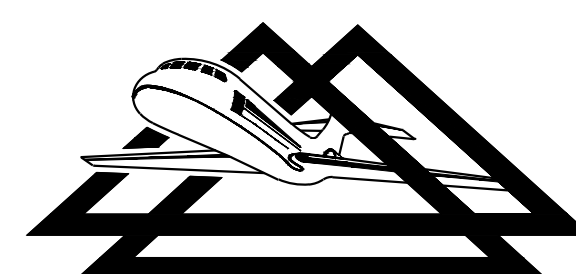
NOTES:

1. BARRICADES SHALL BE SPACED A MAXIMUM OF 4' APART, EXCEPT AT VEHICLE ACCESS/ARFF ACCESS POINTS WHERE A 15' GAP IS ALLOWED.
2. BARRICADES SHALL BE COMMERCIALY MADE IN LENGTHS OF 6' OR 8'.
3. BARRICADES SHALL BE FILLED WITH WATER TO ADEQUATELY WITHSTAND HIGH WINDS AND/OR JET BLAST.
4. THE CONTRACTOR SHALL MAKE FREQUENT INSPECTION OF THE BARRICADES AND MAKE PROMPT REPAIRS AS NECESSARY.
5. LIGHTS SHALL BE INSPECTED AND OPERATING EVERY NIGHT PRIOR TO LEAVING THE SITE.
6. RELOCATIONS AND MULTIPLE SETUPS AND REMOVALS OF THE AVIATION BARRICADES SHALL BE INCIDENTAL TO THE UNIT COST WHEREIN NO SEPARATE PAYMENT WILL BE MADE.

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NO.	REVISIONS	BY	APP.	DATE

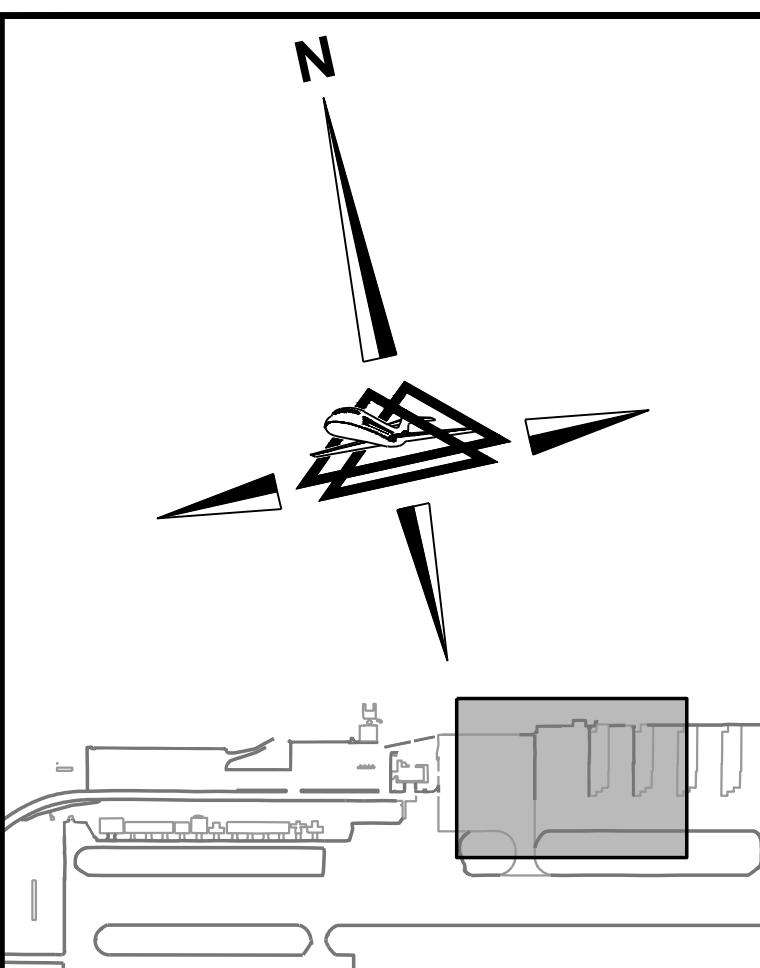
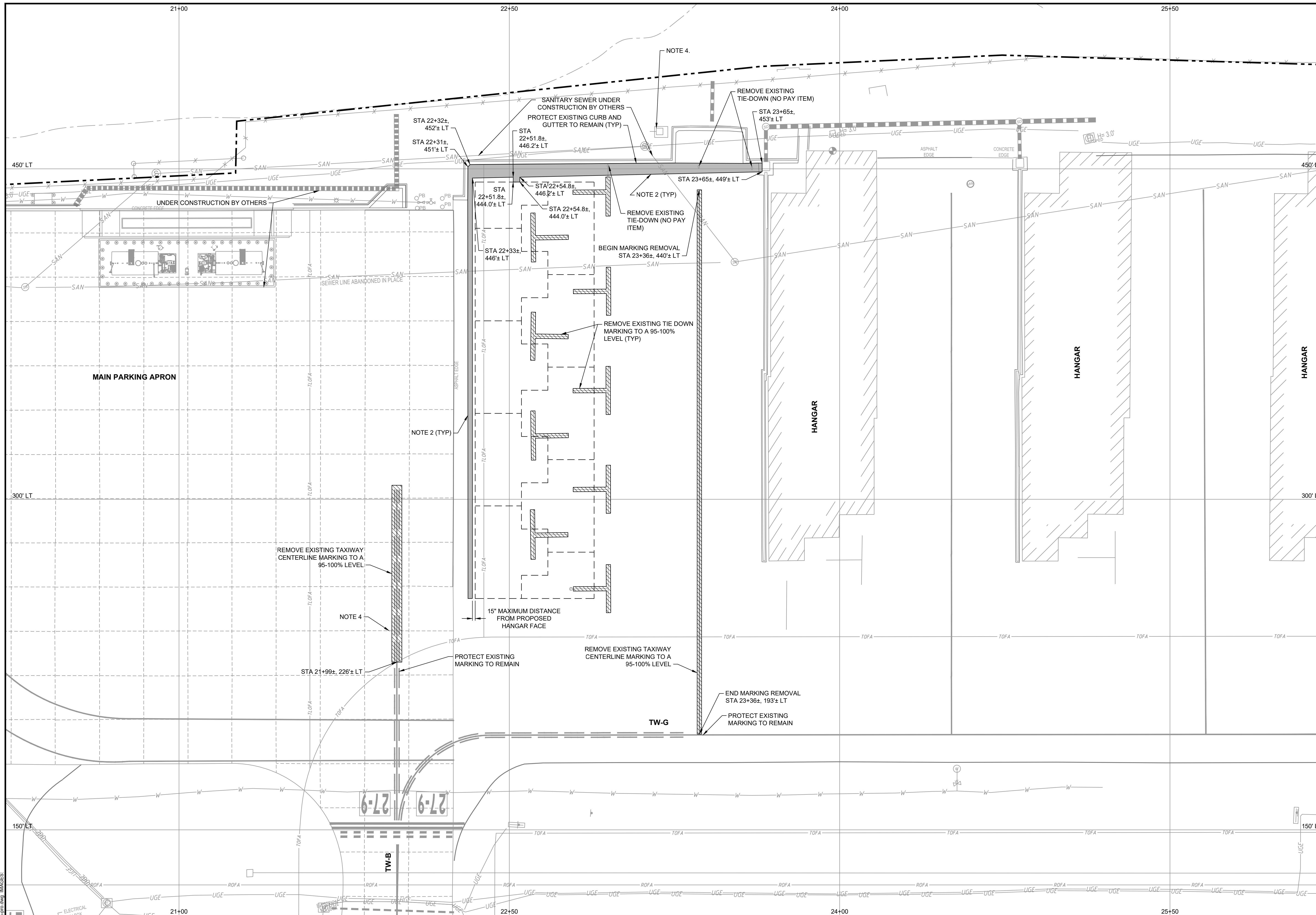
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CONSTRUCT T-HANGARS		NMDOT-AD PROJECT NO. LAM-24-02	JOB NO. 23063
PHASING NOTES & DETAILS		DRAWN BY: MJH	SHEET 4
LOS ALAMOS COUNTY AIRPORT		DESIGNED BY: CAR/ABH	OF 14
SCALE: NONE	DATE: SEPTEMBER 2024		



SHEET INDEX
LEGEND

- × 918.33 EXISTING SPOT ELEVATION
- EXISTING DROP INLET
- EXISTING STORM MANHOLE
- ⊙ EXISTING SANITARY MANHOLE
- ⊕ EXISTING FIRE HYDRANT
- ⊗ EXISTING WATER METER
- ⊘ EXISTING WATER VALVE
- ⊚ EXISTING AIRFIELD GUIDANCE SIGN
- ⊛ EXISTING TIE-DOWN ANCHOR
- ⊜ EXISTING PIPE BOLLARD
- ⊝ EXISTING ROADWAY SIGN
- ⊞ EXISTING POLE (LIGHTED)
- EXISTING STORM DRAIN
- - - EXISTING EDGE DRAIN/UNDERDRAIN
- - - EXISTING CONCRETE JOINT
- - - EXISTING ELECTRICAL CONDUIT
- UGE EXISTING UNDERGROUND ELECTRICAL LINE
- X EXISTING FENCE
- - - EXISTING AIRPORT PROPERTY LINE
- - - ROFA EXISTING RUNWAY OBJECT FREE AREA
- - - SAN EXISTING SANITARY SEWER LINE
- - - TOFA EXISTING TAXIWAY OBJECT FREE AREA
- - - W EXISTING WATER LINE
- - - PROPOSED HANGAR
- ▨ PROPOSED MARKING REMOVAL
- ▩ PROPOSED PAVEMENT REMOVAL

NOTES

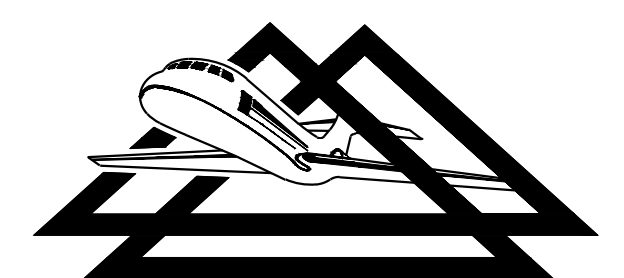
1. ALL STATIONS AND OFFSETS GIVEN WITH RESPECT TO BASELINE "RW" (RUNWAY 9-27 CENTERLINE) UNLESS OTHERWISE NOTED.
2. SAWCUT NEAT VERTICAL FACE (NO SEPARATE PAY ITEM).
3. LIMITS OF PAVEMENT REMOVAL ARE APPROXIMATE AND SHALL BE CONFIRMED IN THE FIELD BY THE CONTRACTOR.
4. HANDHOLE, TRANSFORMER AND 3" CONDUIT UNDER CONSTRUCTION BY OTHERS. PROTECT CONDUIT STUB OUT TO REMAIN.
5. CONTRACTOR SHALL USE STEEL REBAR OR OTHER MEANS TO PROTECT EXISTING JOINT SEALANT FROM DAMAGE DURING MARKING REMOVAL AND PAVEMENT PREPARATION FOR NEW MARKINGS. JOINT SEALANT DAMAGED BY CONTRACTOR OPERATIONS SHALL BE REPAIRED AT NO EXPENSE TO THE OWNER.



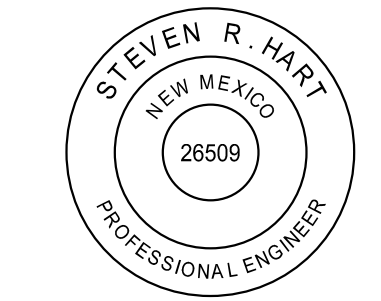
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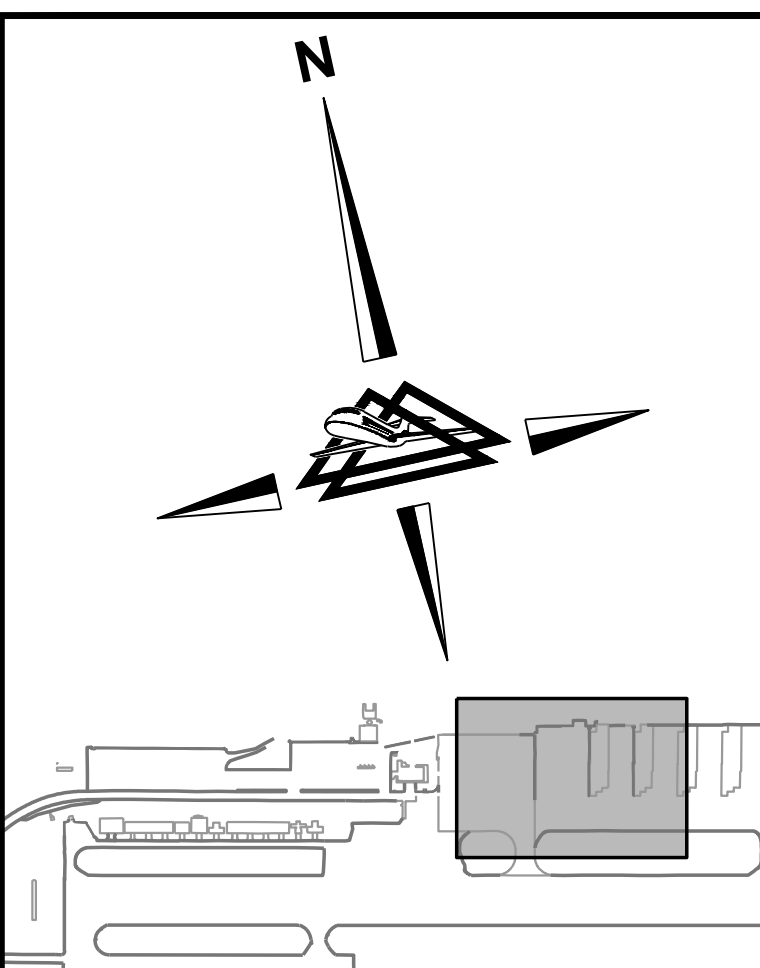
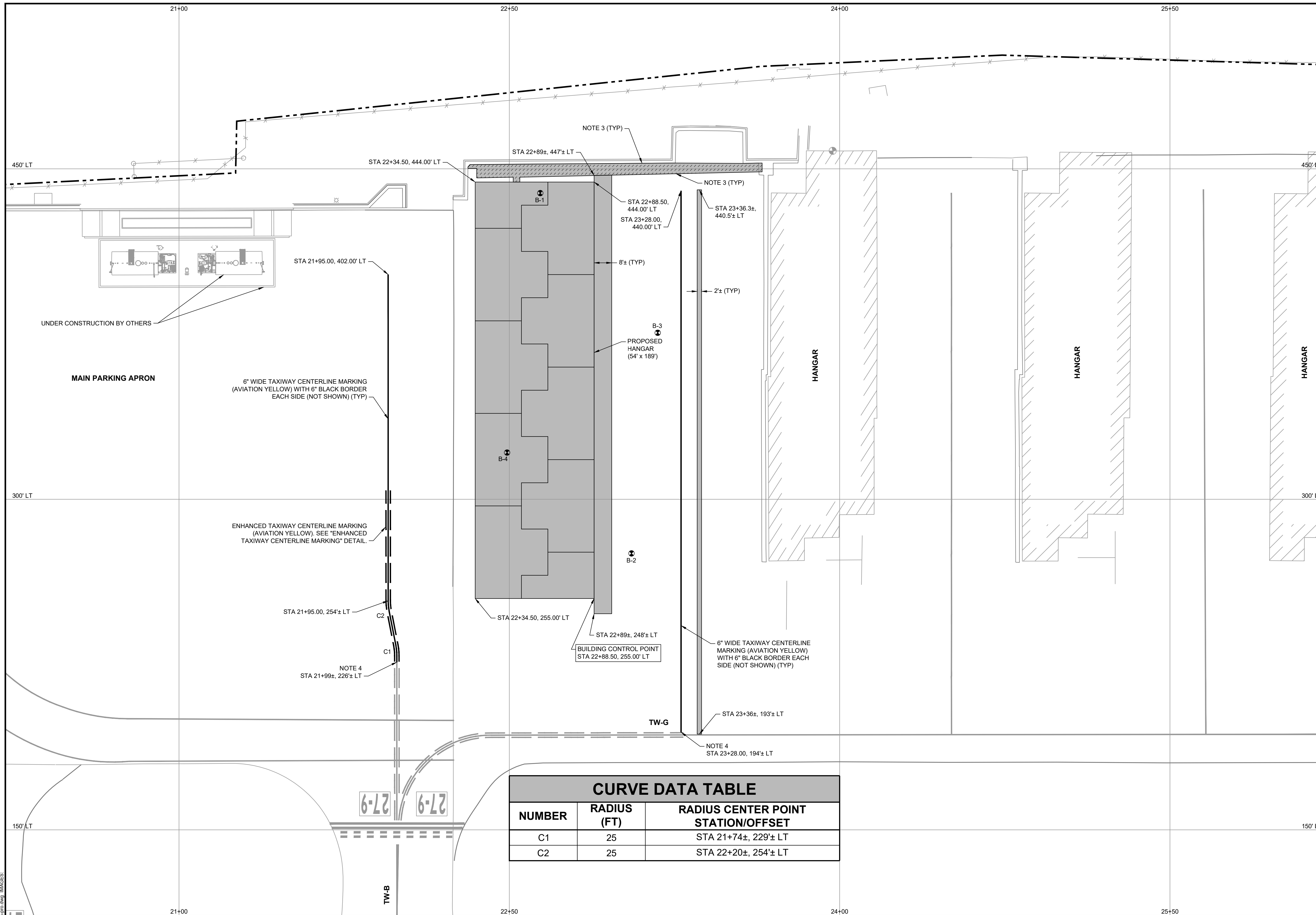
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**DELTA AIRPORT
CONSULTANTS, INC.**



CONSTRUCT T-HANGARS		NMDO-AD PROJECT NO. LAM-24-02	JOB NO. 23063
DEMOLITION PLAN		DRAWN BY: MJH	SHEET 5
LOS ALAMOS COUNTY AIRPORT		DESIGNED BY: CAR/ABH	OF 14
SCALE: 1" = 20'	DATE: SEPTEMBER 2024		



SHEET INDEX

LEGEND

- B-X BORING LOCATION (APPROX)
- EXISTING FENCE
- EXISTING AIRPORT PROPERTY LINE
- EXISTING MARKING
- PROPOSED MARKING
- PROPOSED SEAL COAT LIMITS
- PROPOSED PAVEMENT

NOTES

1. ALL STATIONS AND OFFSETS GIVEN WITH RESPECT TO BASELINE "RW" (RUNWAY 9-27 CENTERLINE) UNLESS OTHERWISE NOTED.
2. REFER TO "DEMOLITION PLAN" FOR PAVEMENT AND MARKING REMOVAL LIMITS.
3. MATCH EXISTING GRADE. REFER TO "DRAINAGE PIPE INSTALLED IN PAVEMENT DETAIL".
4. TIE INTO EXISTING TAXIWAY CENTERLINE MARKING AS ORDERED BY THE ENGINEER. PROTECT EXISTING MARKING TO REMAIN.
5. SEAL COAT LIMITS SHALL INCLUDE ALL AREAS ON BITUMINOUS PAVEMENT WHERE MARKING REMOVAL HAS COMPROMISED THE EXISTING SEAL COAT. CONTRACTOR SHALL MINIMIZE OVERLAP AND OVERSPRAY TO PREVENT "DOUBLE SEALING" THE PAVEMENT.

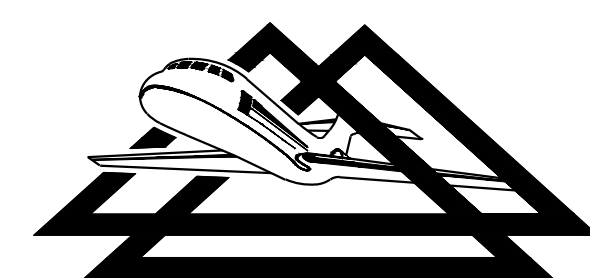


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C2	25	STA 22+20±, 254± LT

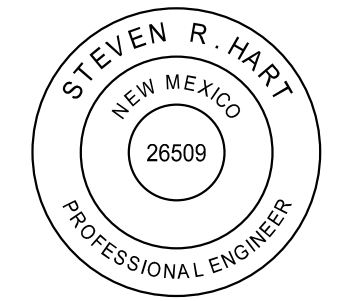
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NO.	REVISIONS	BY	APP.	DATE

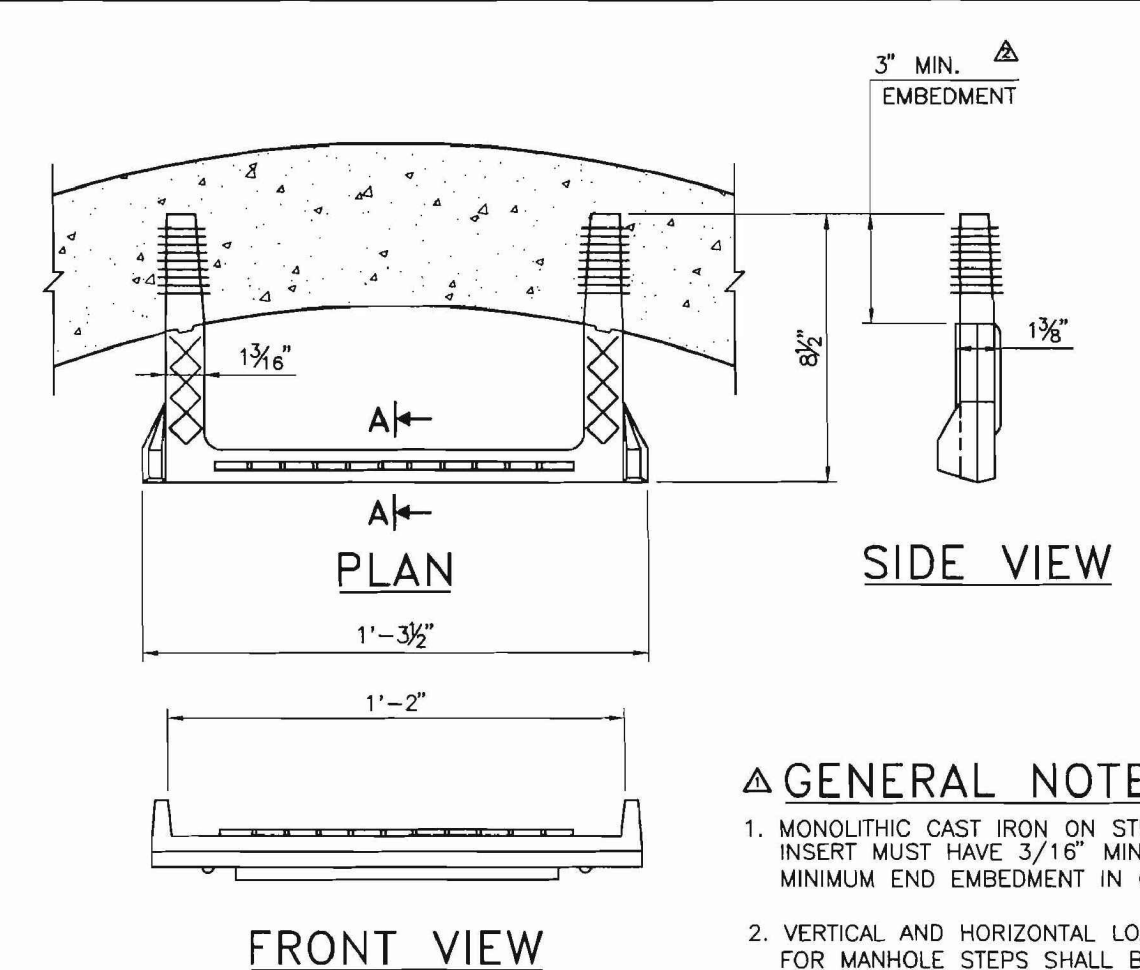
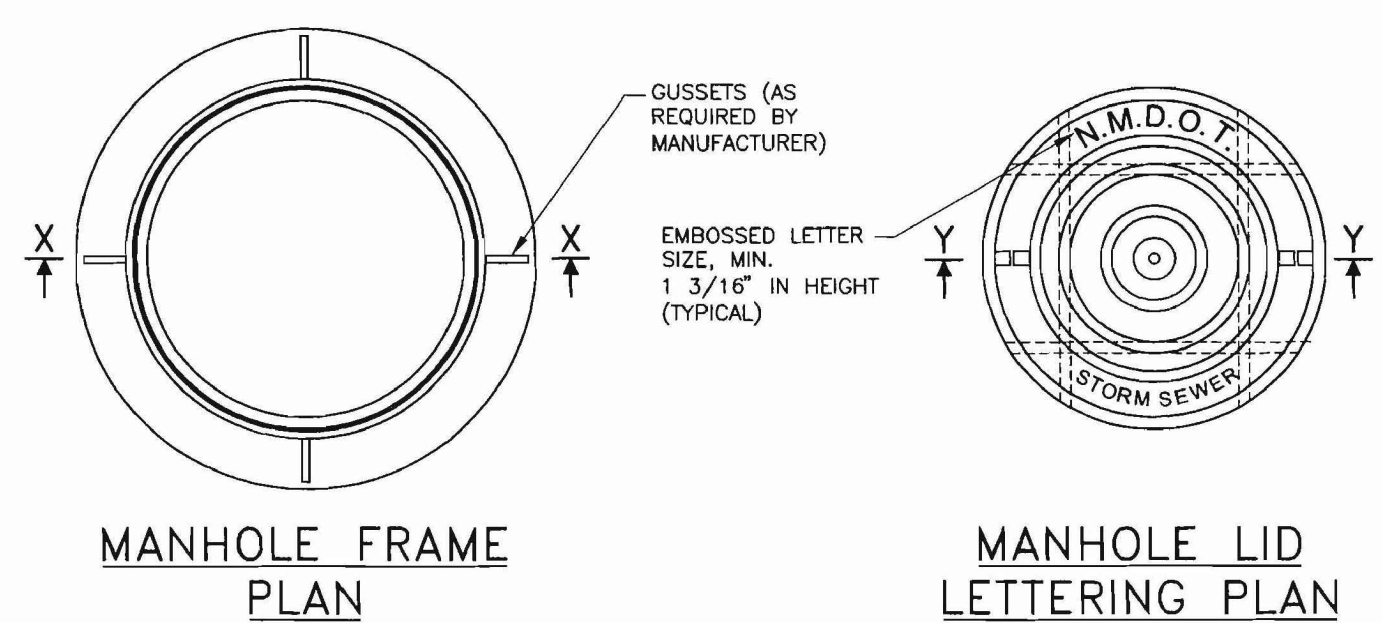
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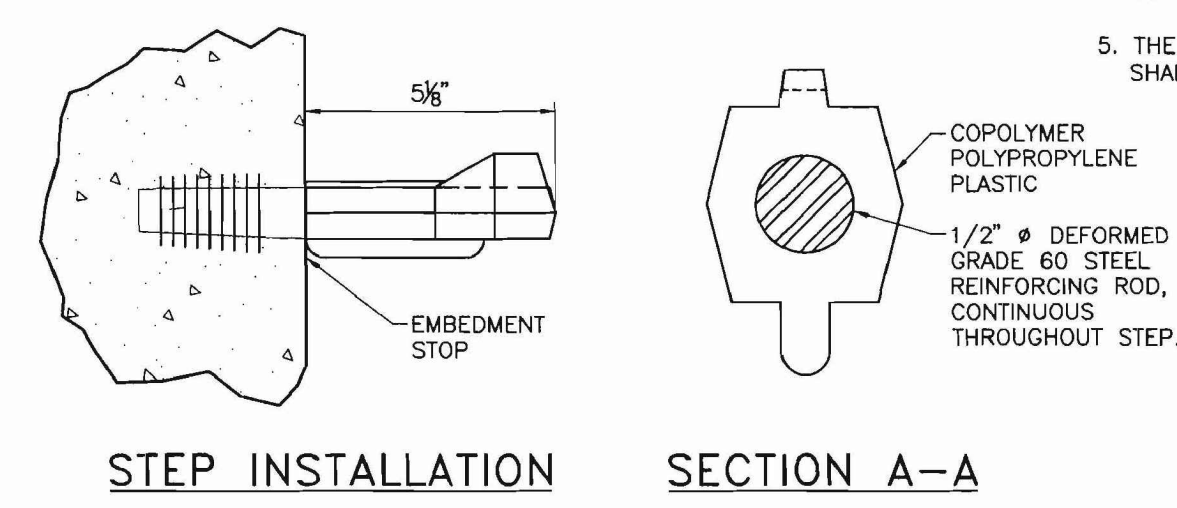
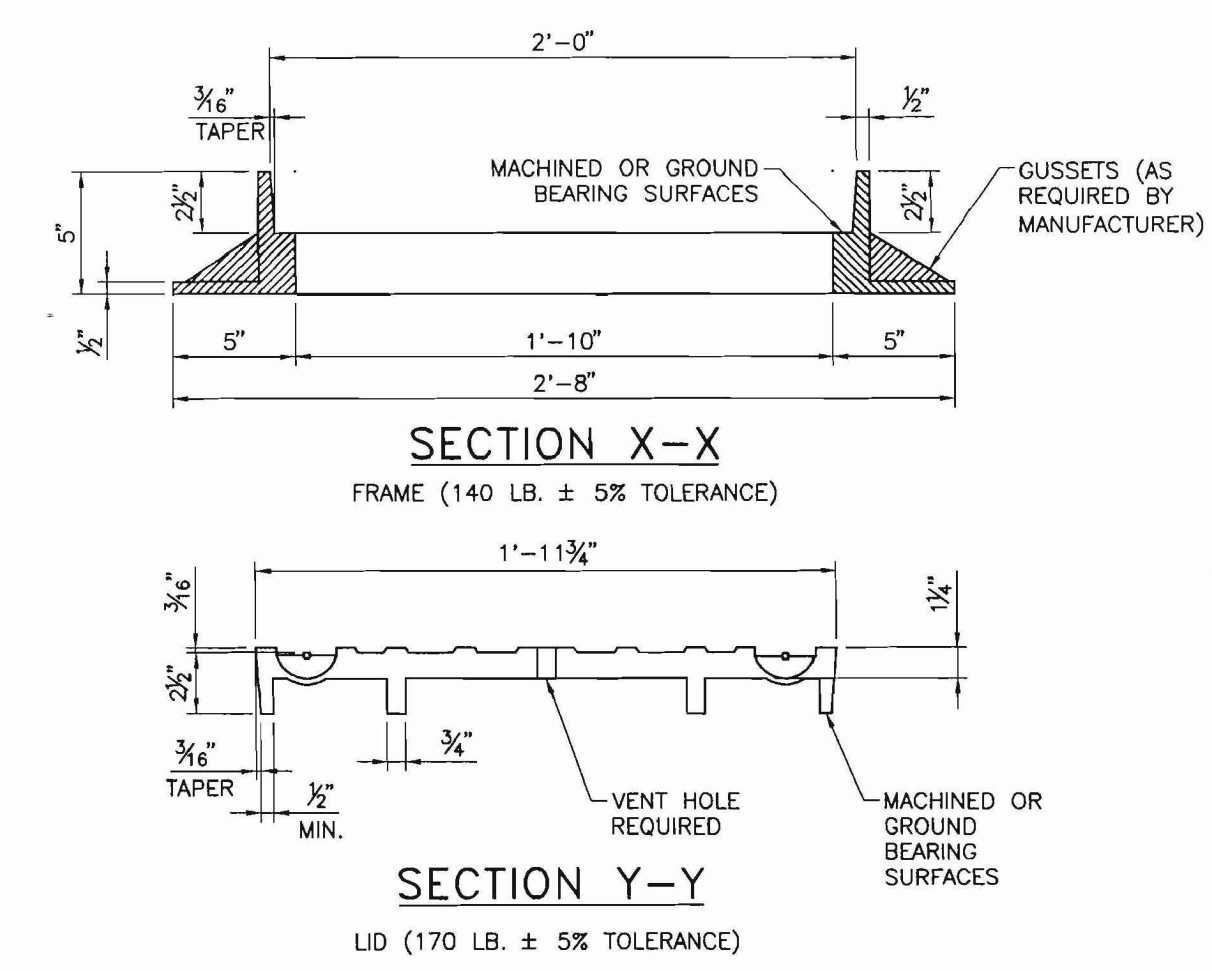
**DELTA AIRPORT
 CONSULTANTS, INC.**



CONSTRUCT T-HANGARS		NMDOT-AD PROJECT NO. LAM-24-02	JOB NO. 23063
GEOMETRIC & MARKING LAYOUT		DRAWN BY: MJH	SHEET 6
LOS ALAMOS COUNTY AIRPORT		DESIGNED BY: CAR/ABH	OF 14
SCALE: 1" = 20'	DATE: SEPTEMBER 2024		



- GENERAL NOTES:**
1. MONOLITHIC CAST IRON ON STEEL ROD INSERTS. INSERT MUST HAVE 3/16" MINIMUM COVER AND 3" MINIMUM END EMBEDMENT IN CASTING.
 2. VERTICAL AND HORIZONTAL LOAD TEST PROCEDURES FOR MANHOLE STEPS SHALL BE IN ACCORDANCE WITH THE METHODS ASTM G497, OTHERWISE THE CERTIFIED TEST RESULTS SHALL BE AVAILABLE FOR VERIFICATION.
 3. THE HORIZONTAL PULL OUT LOAD SHALL BE 400 LB. AND THE VERTICAL LOAD SHALL BE 800 LB. ACCORDING TO ASTM-G478.
 4. THE STEP IS TO BE INSERTED AT THE PRECASTING PLANT, BY PRESSURE, INTO PRECISELY PRE-FORMED TAPERED HOLES IN THE MANHOLE WALL.
 5. THE VERTICAL SPACING BETWEEN INSTALLED STEPS SHALL NOT BE MORE THAN SIXTEEN INCHES.



NO.	DATE	REV. BY	DESCRIPTION
1	2/08/08	YML	CHANGED FROM 5" to 5 1/2", 3/4" to 2"
2	2/08/08	YML	ADDED GENERAL NOTES

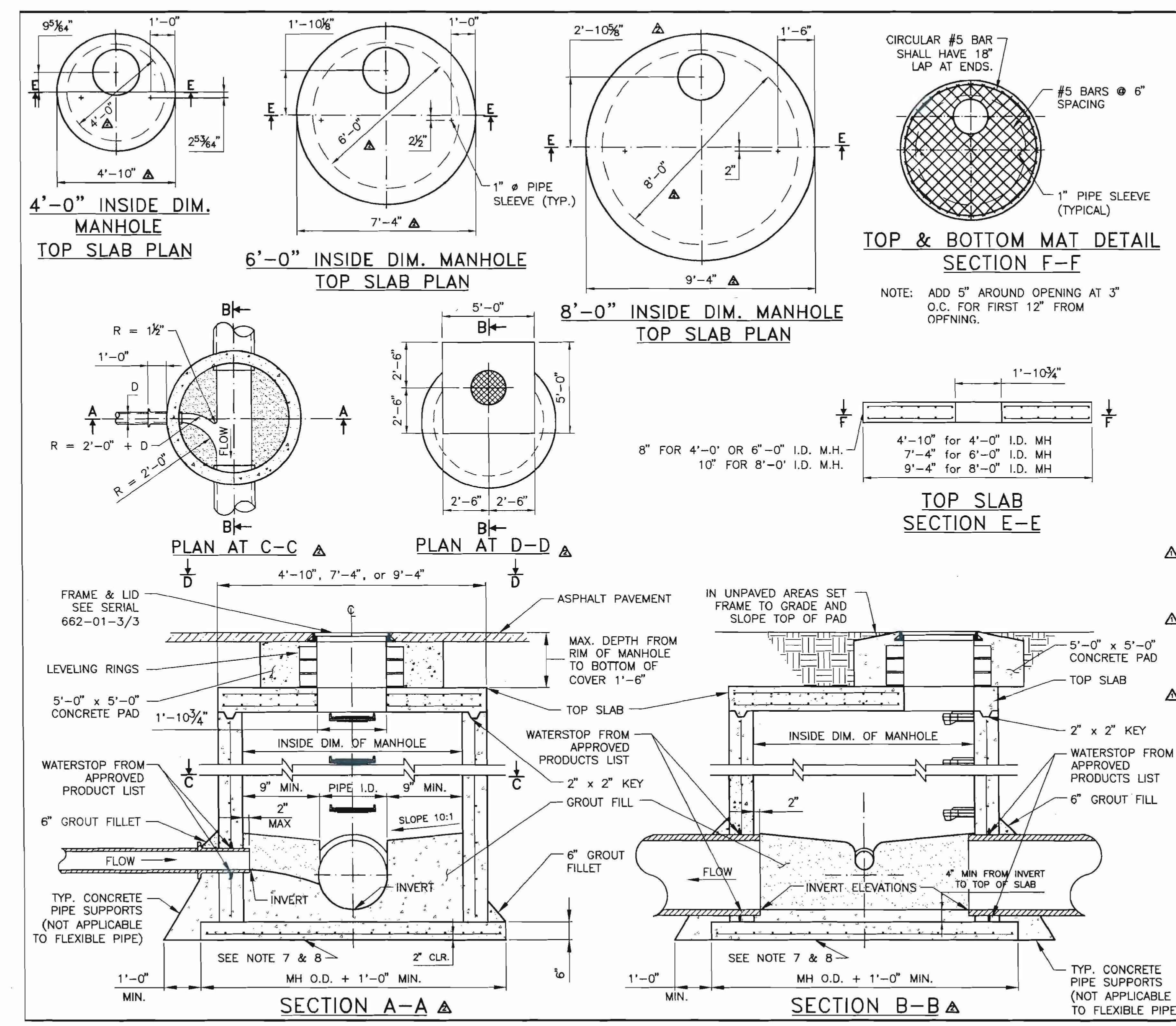
REVISIONS (OR CHANGE NOTICES)

NEW MEXICO
DEPARTMENT OF TRANSPORTATION
STANDARD DRAWING

PRECAST CONCRETE MANHOLE
FRAME LID DETAILS
AND STEP DETAILS

APPROVED: *[Signature]* 3/7/08
DESIGNED BY: T.M. DRAWN BY: SKL. CHECKED BY: YML.

662-01-3/3



- GENERAL NOTES:**
1. DESIGN SHOWN ON THIS DRAWING APPLIES TO 4'-0", 6'-0" AND 8'-0" INSIDE DIMENSION MANHOLES ONLY.
 2. USE TYPE "C" MANHOLE FOR DEPTHS OF LESS THAN 6'-0" MEASURED FROM INVERT TO RIM.
 3. USE NON-SHRINK GROUT FOR FILLETS, PENETRATIONS AND FILL.
 4. COMPACT ALL BACKFILL AROUND AND UNDER MANHOLE TO 95%.
 5. POSITION MANHOLE OPENING OVER THE UPSTREAM SIDE OF MAIN LINE.
 6. USE APPROVED MASTIC OR RUBBER JOINT MATERIAL FOR ALL JOINTS.
 7. BASE TO BE POURED IN PLACE USING #5 BARS AND 6" O.C. EACH WAY FOR MANHOLE DEPTH OF 16'-0" OR GREATER, #5 BARS AT 1'-0" O.C. EACH WAY FOR MANHOLE DEPTH LESS THAN 16'-0".
 8. CONCRETE SHALL BE CLASS "A".
 9. FOR FRAME AND LID SEE SERIAL 662-01-3/3.
 10. CONCRETE PIPE SUPPORTS SHALL EXTEND OUTSIDE OF MANHOLE TO BELL OF FIRST JOINT AND SHALL GRADLE PIPE TO SPRING LINE NOT APPLICABLE TO FLEXIBLE PIPE.
 11. USE 1" OR 2" PRECAST RINGS ON UNPAVED STREET FOR FUTURE ADJUSTMENT OF MANHOLE FRAME TO PAVEMENT GRADE. PLASTER INSIDE WITH 1/2" MORTAR.
 12. REBAR AND CLASS "A" CONCRETE ARE INCIDENTAL TO THE MANHOLE.
 13. ALL PRECAST SECTIONS OF MANHOLE SHALL BE IN CONFORMANCE WITH ASTM C-478, EXCEPT THAT THE MINIMUM THICKNESS AND THE REINFORCEMENT SHALL BE AS INDICATED IN THE CONTRACT.
 14. THE USAGE, DESIGNING AND DETAILING OF THE REINFORCEMENT OF THE PRECAST SECTIONS OF MANHOLE SHALL BE IN CONFORMANCE WITH ASTM C-478, IF THE REINFORCEMENT OF THE PRECAST SECTIONS OF MANHOLE IS NOT INDICATED IN THE CONTRACT.
 15. THE MINIMUM COMPRESSIVE STRENGTH OF THE CONCRETE IN THE MANHOLE BASE, RISER AND TOP SECTION SHALL BE 4000 PSI AT 28 DAYS AS REQUIRED BY ASTM C-478.

NO.	DATE	REV. BY	DESCRIPTION
1	1/08/08	YML	MADE GENERAL REVISIONS
2	1/08/08	YML	ADDED GENERAL NOTES 13, 14, & 15.

REVISIONS (OR CHANGE NOTICES)

NEW MEXICO
DEPARTMENT OF TRANSPORTATION
STANDARD DRAWING

PRECAST CONCRETE MANHOLE
TYPE "C"
FOR STORM SEWERS

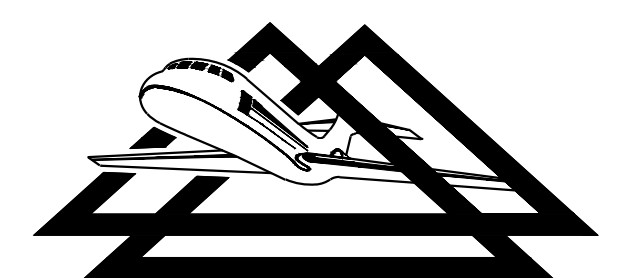
APPROVED: *[Signature]* 3/7/08
DESIGNED BY: T.M. DRAWN BY: SKL. CHECKED BY: YML.

662-01-1/3

DRAWING: 662-01-3/3, LAYOUT: L.P. XREFS: IMAGES: 623 Page: 01, 662 Page: 1, 662 Page: 2, 662 Page: 3, 662 Page: 4

NO.	REVISIONS	BY	APP.	DATE

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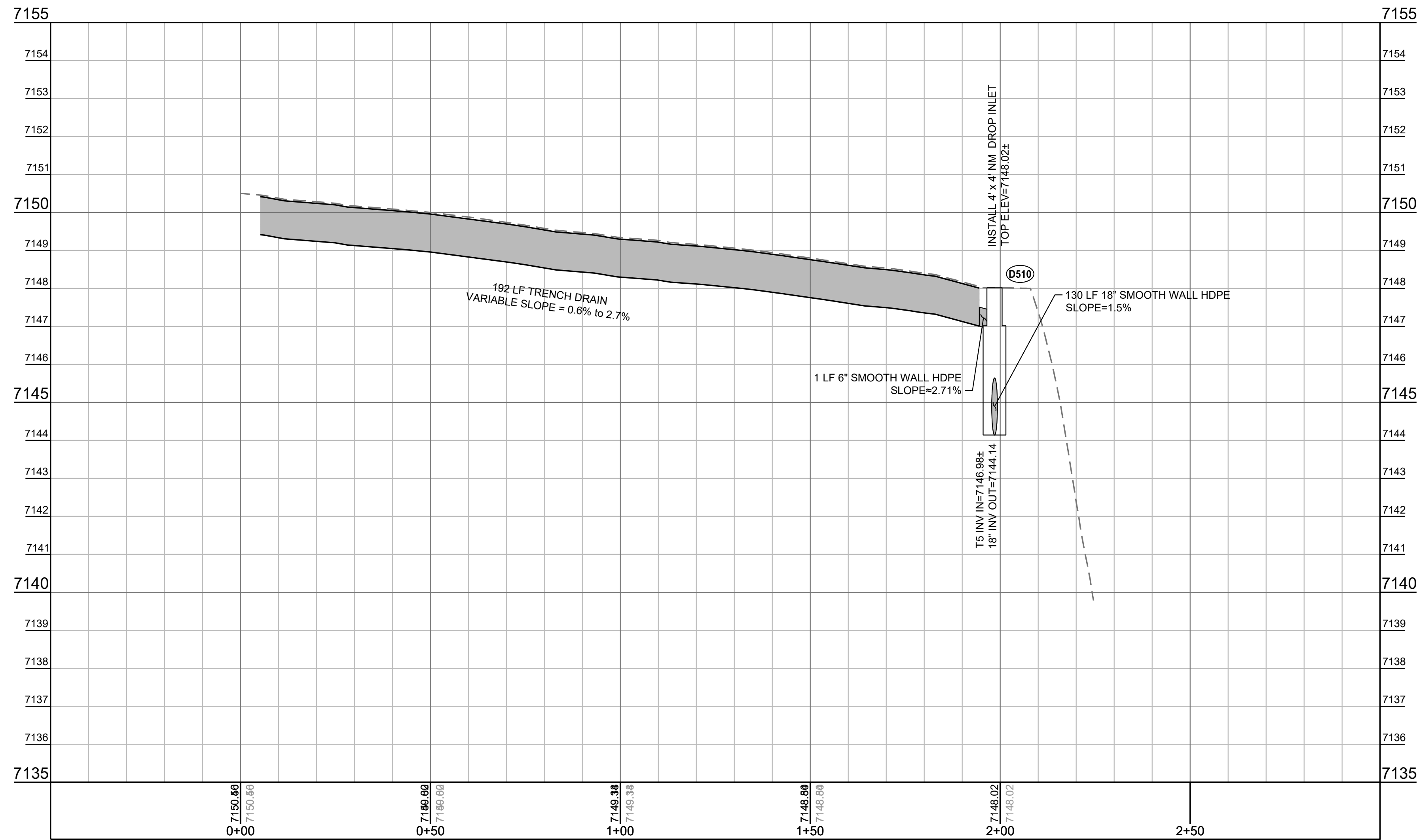
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CONSULTANTS, INC.**



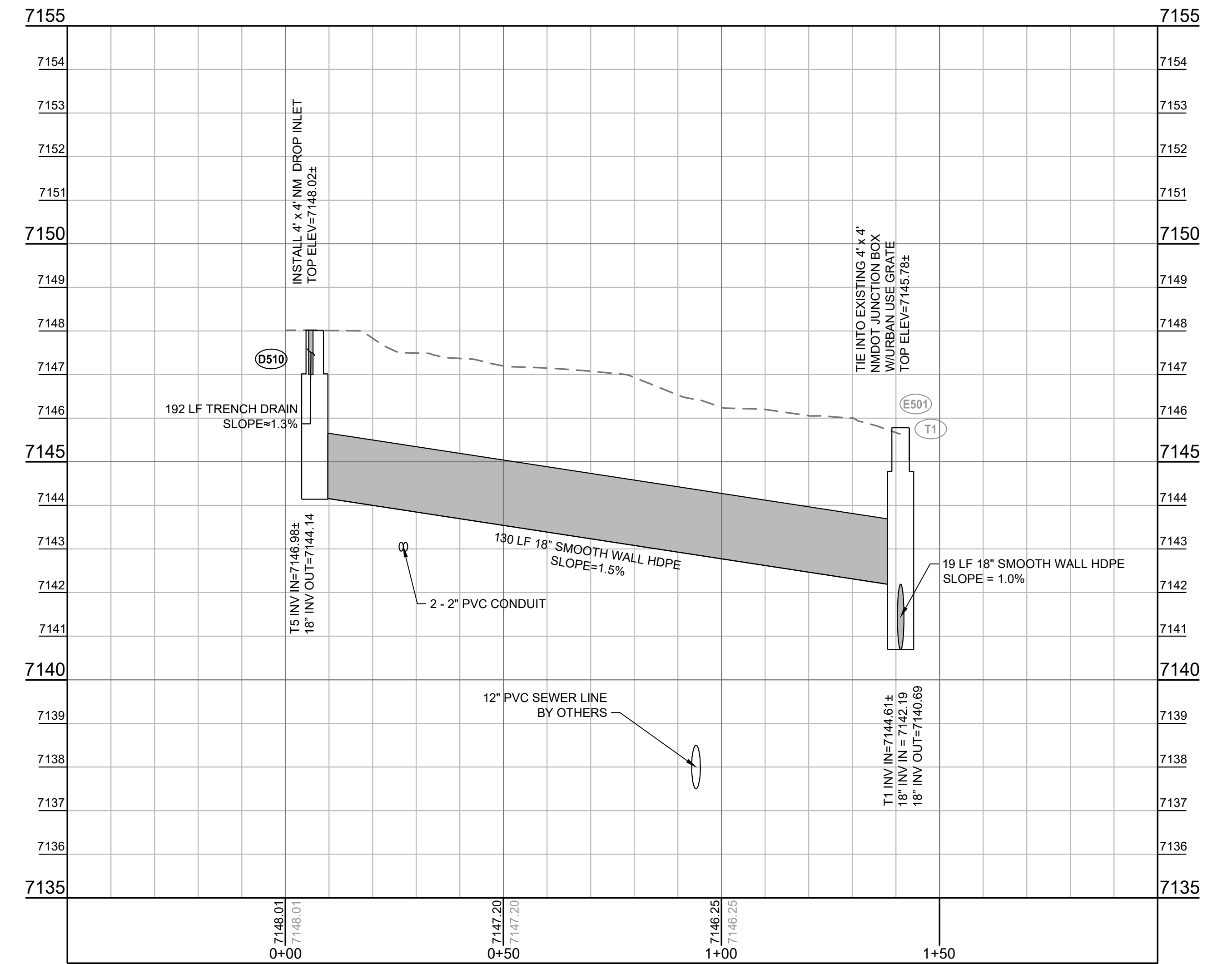
CONSTRUCT T-HANGARS		NMDO-AD PROJECT NO. LAM-24-02	JOB NO. 23063
DRAINAGE DETAILS		DRAWN BY: MUH	SHEET 9
LOS ALAMOS COUNTY AIRPORT		DESIGNED BY: CAR/ABH	OF 14
SCALE: NONE	DATE: SEPTEMBER 2024		

LEGEND

- EXISTING GRADE
- PROPOSED GRADE
- ▭ PROPOSED PIPE



BASELINE "T5 - D510"

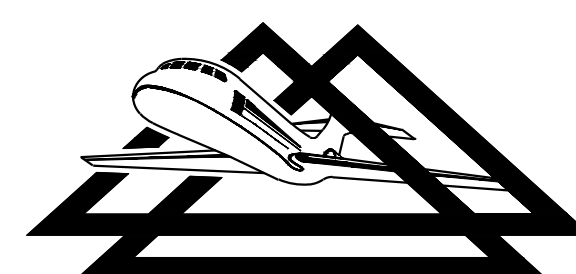


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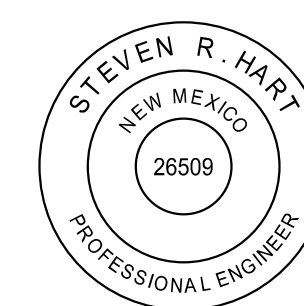
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CONSTRUCT T-HANGARS

DRAINAGE PROFILES

LOS ALAMOS COUNTY AIRPORT

NMDOT-AD PROJECT NO.
LAM-24-02

DRAWN BY: MJH

DESIGNED BY: CAR/ABH

SCALE:
HORIZ: 1" = 20'
VERT: 1" = 2'

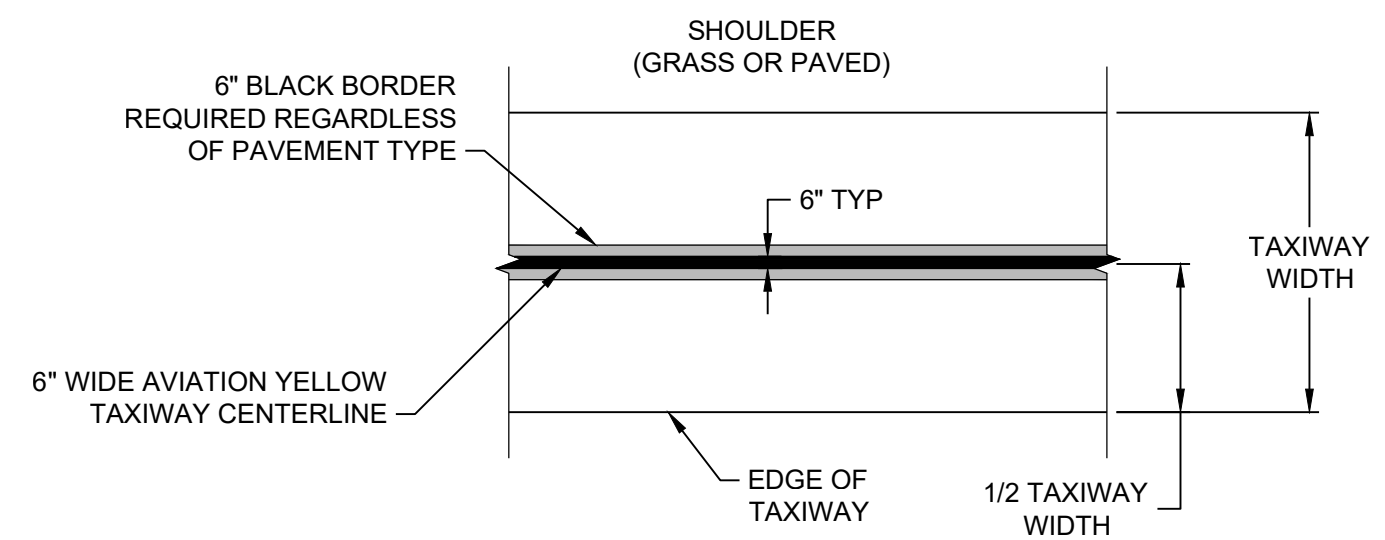
JOB NO.
23063

SHEET

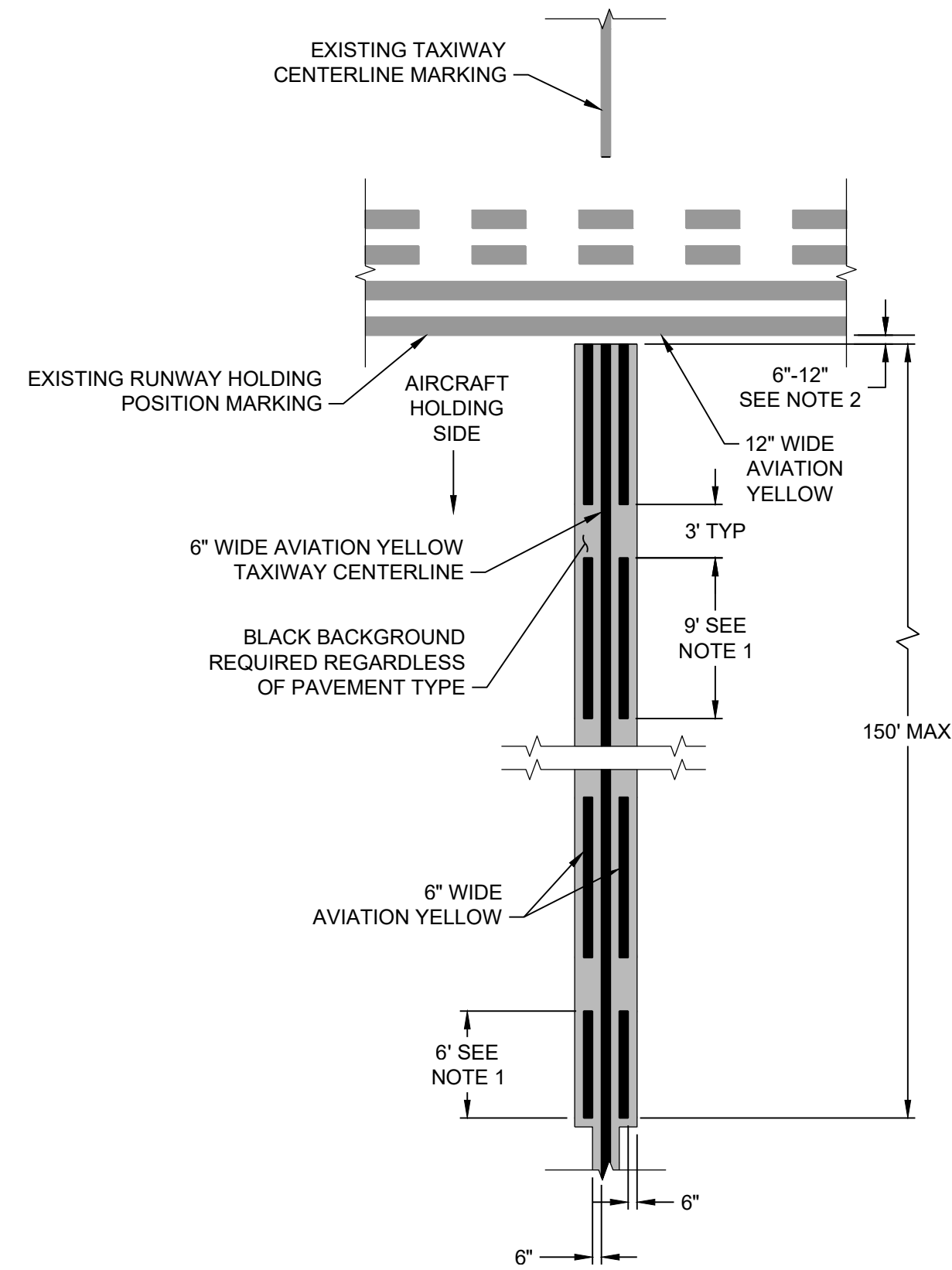
OF

**10
14**

DATE:
SEPTEMBER 2024



6" TAXIWAY CENTERLINE MARKING
NTS



ENHANCED TAXIWAY CENTERLINE MARKING
NTS

NOTES:

- 9 FEET AND 6 FEET DIMENSION IS ALONG TAXIWAY CENTERLINE. IN SKEWED CONDITIONS, DIMENSION ON CENTER WILL BE THAT SHOWN. DIMENSION OF 6 INCHES WIDE DASHED ENHANCEMENT LINE WILL BE LONGER OR SHORTER THAN 9 FEET DEPENDING ON SIDE OF SKEW. DASHED LINE SHALL END AT SAME LOCATION RELATIVE TO THE CENTERLINE.
- MATCH EXISTING FIELD CONDITIONS ELSEWHERE ON AIRPORT (6" SHOWN).

MARKING NOTES

GENERAL:

- THE CONTRACTOR SHALL SUBMIT DOCUMENTATION THAT THE PERSONNEL RESPONSIBLE FOR MARKING LAYOUT AND APPLICATION HAS A MINIMUM OF FIVE (5) YEARS OF SIMILAR EXPERIENCE WORKING ON AIRPORTS. THE CONTRACTOR SHALL PROVIDE REFERENCES TO SUBSTANTIATE THE REQUIRED EXPERIENCE.
- ALL MARKINGS SHALL BE IN ACCORDANCE WITH FAA ADVISORY CIRCULAR (AC) 150/5340-1M, "STANDARDS FOR AIRPORT MARKINGS".
- AFTER PROPER PAVEMENT CURE FOR BITUMINOUS PAVEMENT, ALL MARKINGS ON THE "MARKING LAYOUT" SHALL BE MARKED AT THE FULL RATE (115 SF/GAL) AND PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE FOOT FOR PERMANENT MARKING. REFERENCE IS DIRECTED TO ITEM P-620 OF THE SPECIFICATIONS.
- REFLECTIVE MEDIA SHALL BE PROVIDED AS OUTLINED IN ITEM P-620. BLACK MARKINGS SHALL NOT BE REFLECTORIZED. MARKINGS WITHOUT BEADS MUST BE DRY PRIOR TO APPLYING MARKING WITH BEADS.
- THE PAVEMENT SHALL BE THOROUGHLY CLEANED PRIOR TO THE APPLICATION OF THE MARKINGS. THE CLEANING SHALL BE ACCOMPLISHED WITH HIGH PRESSURE WATER; THE USE OF CHEMICALS WILL NOT BE PERMITTED.
- ALL STATIONS AND OFFSETS ARE APPROXIMATE AND SHALL BE CONFIRMED IN THE FIELD BY THE CONTRACTOR.
- ALL MARKINGS SHALL BE LAID OUT ON THE PAVEMENT BY THE CONTRACTOR FOR ACCEPTANCE BY THE ENGINEER PRIOR TO PLACEMENT.
- PRIOR TO THE START OF WORK, A CONTROL STRIP SHALL BE APPLIED IN ACCORDANCE WITH ITEM P-620-3.7. ALL STRIPING EQUIPMENT SHALL BE CALIBRATED PRIOR TO THE TEST LINE APPLICATION. THE TEST SECTION SHALL INCLUDE THE APPLICATION OF A MINIMUM OF 5 GALLONS OF PAINT AND APPLICATION OF 50 POUNDS OF TYPE III GLASS BEADS. ALL TEST LINES WILL BE INSPECTED DURING DARKNESS PRIOR TO THE COMMENCEMENT OF STRIPING OPERATIONS.
- FINAL MARKINGS SHALL BE WITHIN THE TOLERANCES OUTLINED IN ITEM P-620-3.5.
- EXCESS GLASS BEADS SHALL BE REMOVED BY VACUUM TRUCK, FOD BOSS, OR SIMILAR EQUIPMENT; BLOWING OF BEADS IS NOT ACCEPTABLE.

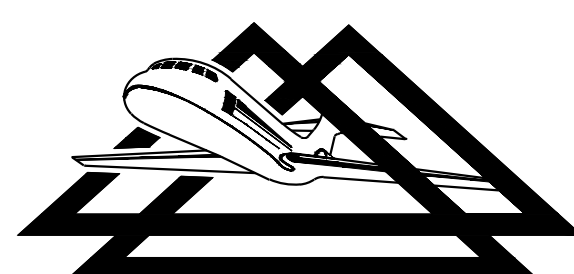
MARKING REMOVAL NOTES:

- PAINT REMOVAL SHALL BE ACCOMPLISHED WITH EQUIPMENT AND IN A MANNER THAT WILL NOT DAMAGE THE PAVEMENT. APPROVED METHODS INCLUDE HIGH PRESSURE WATERBLASTING OR SCARIFICATION (GRINDING); THE USE OF CHEMICALS IS NOT PERMITTED. BLACKING OUT OF MARKINGS IS NOT PERMITTED. SCARIFICATION (GRINDING) SHALL BE LIMITED TO UNGROOVED PAVEMENTS OR TO REMOVING PAINT ABOVE THE SURFACE.
- ANY DAMAGE CAUSED BY THE CONTRACTOR'S OPERATIONS SHALL BE CORRECTED AT THE CONTRACTOR'S EXPENSE AND IN A MANNER APPROVED BY THE ENGINEER.
- THE CONTRACTOR SHALL PROVIDE CERTIFICATION THAT THE CONTRACTOR'S EQUIPMENT HAS BEEN USED IN THE PERFORMANCE OF A SIMILAR CONTRACT.
- PRIOR TO THE START OF WORK, PAVEMENT MARKINGS SHALL BE REMOVED FROM A DESIGNATED TEST SECTION, NOT LESS THAN 50 SQUARE YARDS IN SIZE. THE METHOD AND EQUIPMENT USED FOR THE TEST SECTION SHALL BE THE SAME AS THAT INTENDED FOR THE REMAINDER OF THE WORK. THE TEST SECTION SHALL BE INSPECTED AND APPROVED BY THE ENGINEER PRIOR TO BEGINNING ANY FURTHER PAINT REMOVAL. REFERENCE IS DIRECTED TO ITEM P-619 OF THE SPECIFICATIONS.
- DIFFERENT DEGREES OF PAINT REMOVAL SHALL BE REQUIRED AS SHOWN ON THE PLANS AND DEFINED BELOW:
 - AT LEAST 95-100 PERCENT OF MARKINGS SHALL BE REMOVED WHEN THE MARKING IS OBSOLETE.
 - THE REMOVAL LEVEL IS DEFINED SUCH THAT THE PAVEMENT IS CLEARLY EXPOSED TO THE DEGREE SPECIFIED. THE DEGREE WILL BE VERIFIED BY THE GRID METHOD AS OUTLINED IN ITEM P-619.
- THE CONTRACTOR SHALL USE REBAR, METAL STRIPS, OR OTHER APPROVED METHODS TO PROTECT EXISTING JOINT SEAL DURING PAINT REMOVAL OPERATIONS.
- WATER FOR THE CONTRACTOR'S USE IS AVAILABLE ON THE AIRFIELD. THE CONTRACTOR SHALL PROVIDE AN APPROVED METER AND METER ANY WATER USED. THE COST FOR THE WATER WILL BE CHARGED TO THE CONTRACTOR BY THE OWNER AND WILL BE BASED UPON THE RATES CHARGED TO THE OWNER.
- USED OR WASTE WATER FROM PAINT REMOVAL OPERATIONS SHALL BE COLLECTED BY THE CONTRACTOR AND DISPOSED OF OFF-SITE IN ACCORDANCE WITH LOCAL, STATE, AND FEDERAL GUIDELINES.
- ACCUMULATIONS OF WATER, DUST, OR OTHER RESIDUE RESULTING FROM THE PAINT REMOVAL SHALL BE REMOVED AS THE WORK PROGRESSES. PRIOR TO ANY PAINTING OPERATIONS, THE SURFACE SHALL BE FREE OF ANY DIRT, REMOVAL RESIDUE, OR OTHER CONTAMINANTS THAT WOULD PREVENT THE BOND OF THE NEW COATING TO THE PAVEMENT. QUALITY CONTROL MEASURES SHALL INCLUDE A SIMPLE "PULL TEST" WITH ADHESIVE MATERIAL; EVIDENCE OF EXCESSIVE DEBRIS ON THE ADHESIVE INDICATES THAT ADDITIONAL CLEANING SHALL BE REQUIRED.

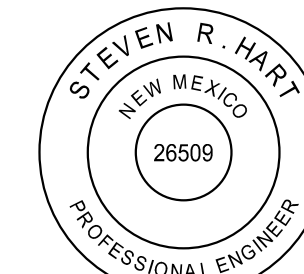
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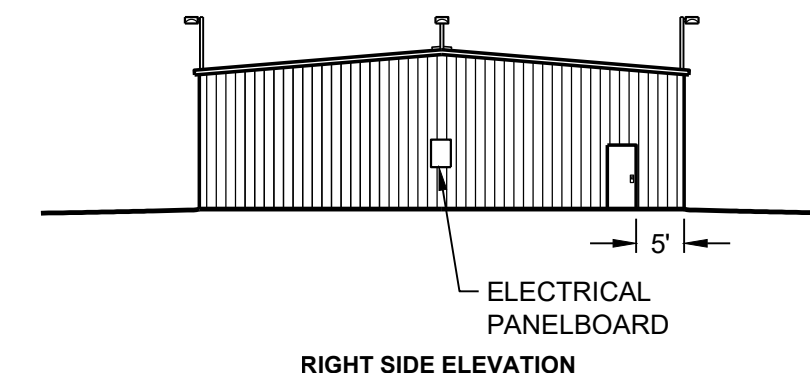
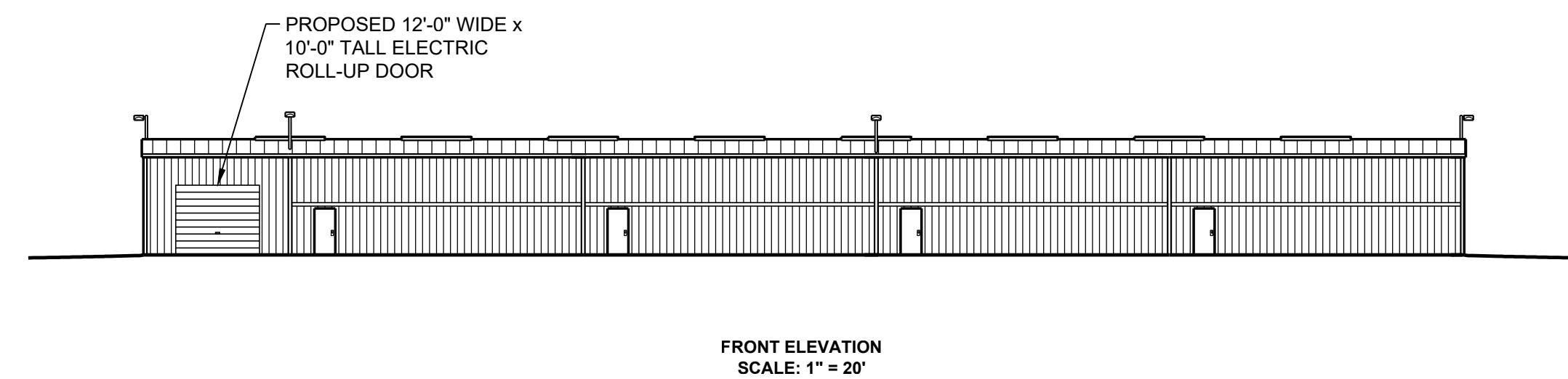
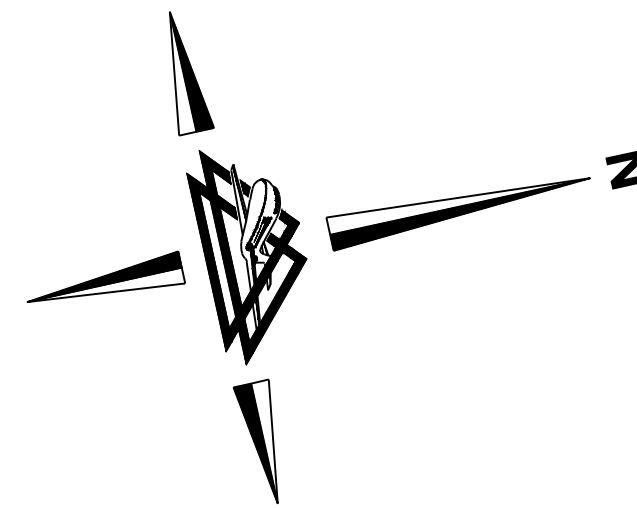
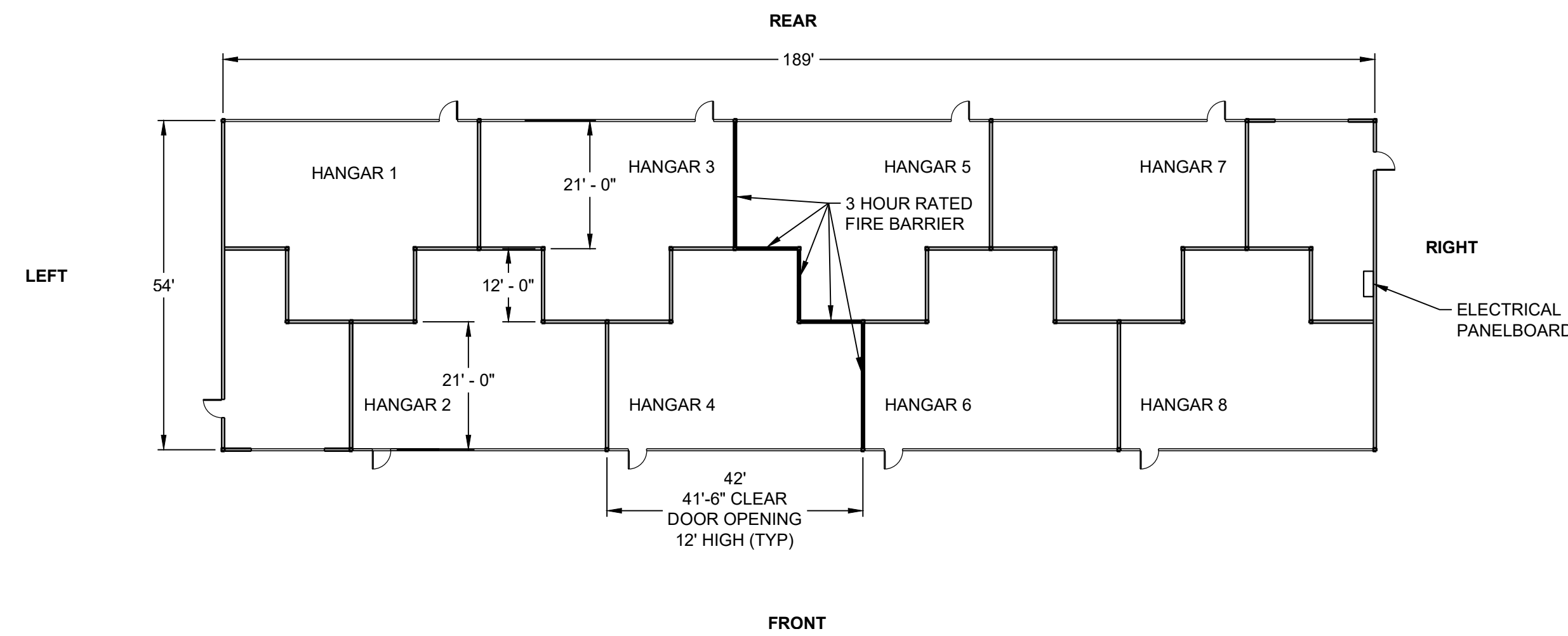
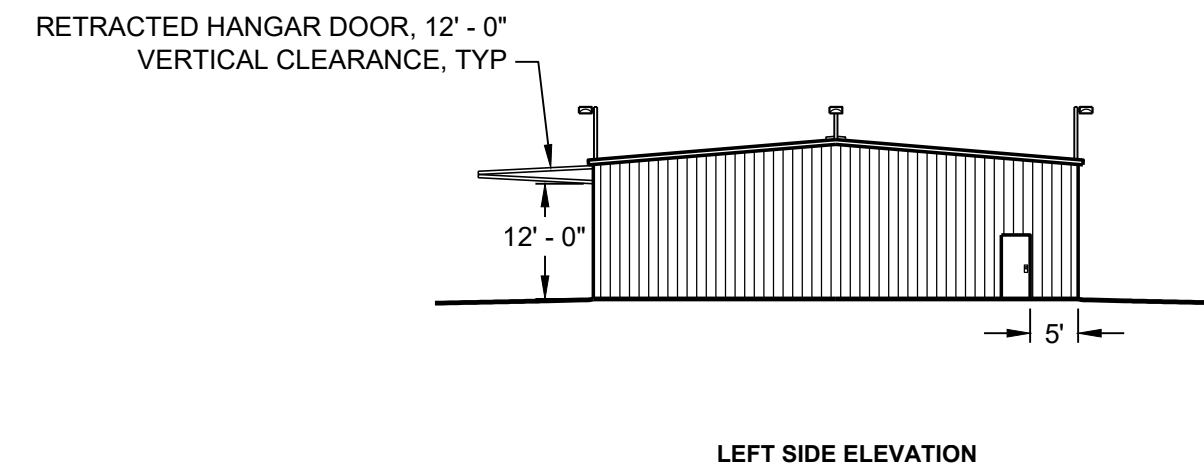
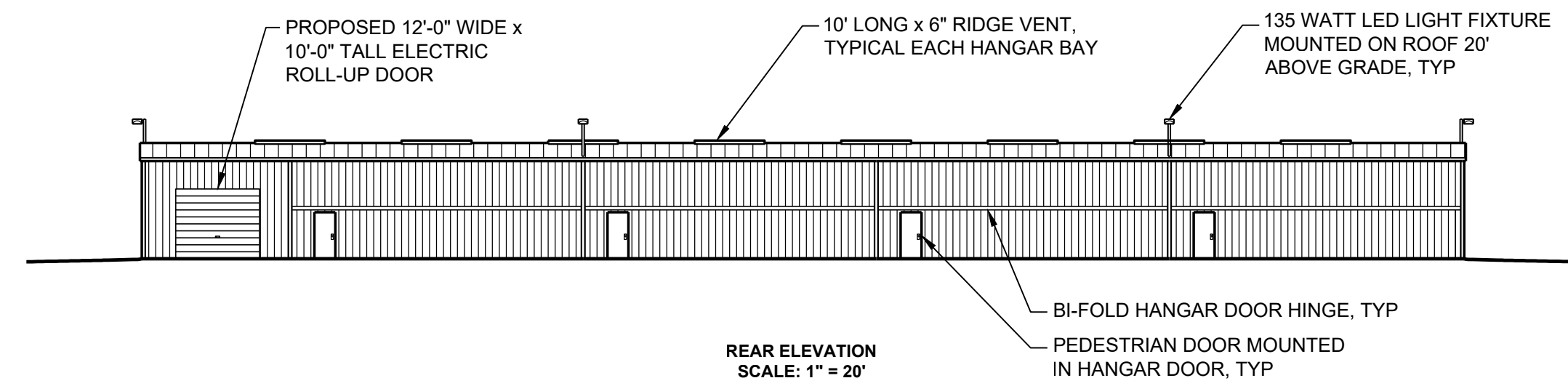
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CONSTRUCT T-HANGARS		NMDOT-AD PROJECT NO. LAM-24-02	JOB NO. 23063
MARKING NOTES & DETAILS		DRAWN BY: MJH	SHEET 11
LOS ALAMOS COUNTY AIRPORT		DESIGNED BY: CAR/ABH	OF 14
SCALE: NONE	DATE: SEPTEMBER 2024		



T-HANGAR BUILDING PLAN & ELEVATIONS DETAIL
SCALE: 1" = 20'

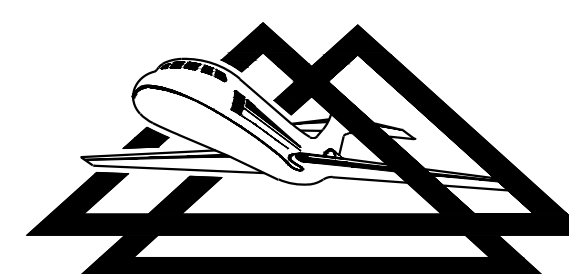
NOTES:

1. SEE SPECIFICATION SECTION M-108 FOR HANGAR BUILDING SPECIFICATIONS.
2. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE FOLLOWING:
 - A. DESIGN AND CONSTRUCTION OF THE FOUNDATION FOR THE HANGAR BUILDING
 - B. ERECTION OF THE HANGAR BUILDING IN ACCORDANCE WITH THE REQUIREMENTS OF IBC 2021
 - C. DESIGN AND CONSTRUCTION OF ELECTRICAL SYSTEM FOR THE HANGAR BUILDING AS PER NFPA 409 (STANDARD ON AIRCRAFT HANGARS), THE NEC AND ALL LOCAL CODES.
3. DIMENSIONS OF HANGAR BUILDINGS ARE NOMINAL. FINAL DIMENSIONS SHALL BE BASED ON DESIGN DRAWINGS PROVIDED BY CONTRACTOR.
4. ALL INTERIOR PARTITION WALLS SHALL EXTEND FROM FLOOR TO ROOF.
5. THE CONTRACTOR SHALL CONSTRUCT A 3 HOUR RATED FIRE BARRIER IN THE LOCATION SHOWN ON THE BUILDING LAYOUT. THE FIRE BARRIER SHALL DIVIDE THE HANGAR BUILDING FROM FLOOR TO CEILING.
6. EXTERIOR LIGHTING SHALL BE BEACON ALPHA MODEL AL-X-60L-136-3K7-5X5-UNV OR EQUAL. FULL CUTOFFS SHALL BE PROVIDED FOR FIXTURES FACING RUNWAY.
7. THE CONTRACTOR SHALL BE RESPONSIBLE FOR ALL PERMITS AND FEES NECESSARY TO CONSTRUCT THE HANGAR BUILDING AND ASSOCIATED SITE WORK.
8. THE CONTRACTOR SHALL BE RESPONSIBLE FOR COORDINATING ALL UTILITY CONNECTIONS. ALL UTILITY CONNECTION FEES WILL BE PAID FOR BY THE OWNER.
9. HOLD ALL SHEATHING MATERIAL (FRP, GYPSUM BOARD) 1/2" ABOVE EXISTING PAVEMENT ELEVATION IN HANGAR.
10. FIBERGLASS GYPSUM BOARD USED IN THE 3-HOUR RATED FIRE BARRIER SHALL BE TAPED, FINISHED, AND PAINTED WITH TWO COATS OF EXTERIOR PAINT. ALL OTHER INTERIOR PARTITION WALLS WILL NOT USE TAPE, FINISH, OR PAINT.
11. PROVIDE AND INSTALL ONE (1) TEN (10) POUND ABC TYPE FIRE EXTINGUISHER WITH WALL HOOK IN EACH HANGAR. MOUNTING LOCATION SHALL BE WITHIN FIVE (5) FEET OF THE PEDESTRIAN DOOR.
12. HANGAR IS NOT BEING INSTALLED ON A LEVEL FOUNDATION. CONTRACTOR SHALL PROVIDE APPROPRIATE MEASURES AT EACH HANGAR DOOR TO PROVIDE A WATER-TIGHT SEAL ALONG THE BOTTOM OF EACH DOOR. 12'-0" VERTICAL CLEARANCE WHEN OPEN IS THE MINIMUM AT EACH HANGAR.
13. CONTRACTOR SHALL PROVIDE APPROPRIATE MEASURES AT THE SOUTH-FACING HANGAR WALL TO PROVIDE A WATER-TIGHT SEAL ALONG THE BOTTOM OF THE WALL.
14. CONTRACTOR SHALL BE RESPONSIBLE TO ENSURE DOORS MOVE FREELY AND DO NOT BIND.
15. THE DRAWINGS SHOWN ON THIS SHEET ARE FOR GUIDELINE PURPOSES ONLY. CONTRACTOR SHALL PROVIDE PERMIT DRAWINGS WHICH SHALL BE SEALED BY A LICENSED ARCHITECT AND/OR ENGINEER IN THE STATE OF NEW MEXICO AS REQUIRED BY ANY AUTHORITIES HAVING JURISDICTION. REFERENCE IS ALSO DIRECTED TO THE PROJECT SPECIFICATIONS FOR FURTHER HANGAR PERFORMANCE REQUIREMENTS.

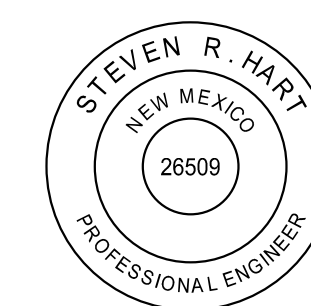
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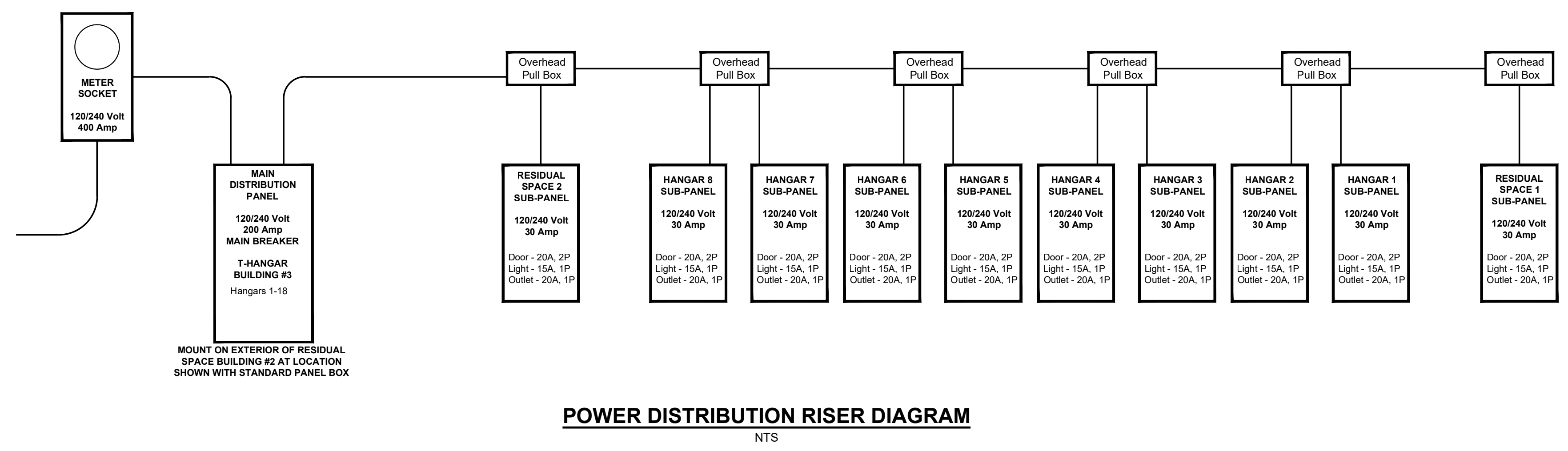
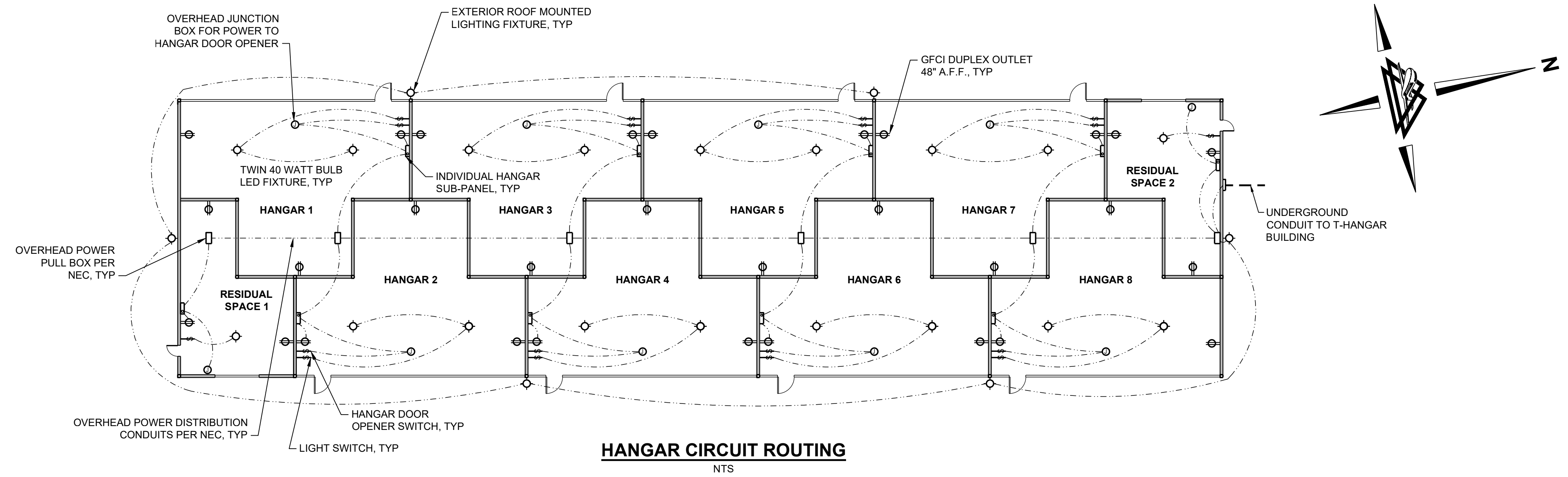
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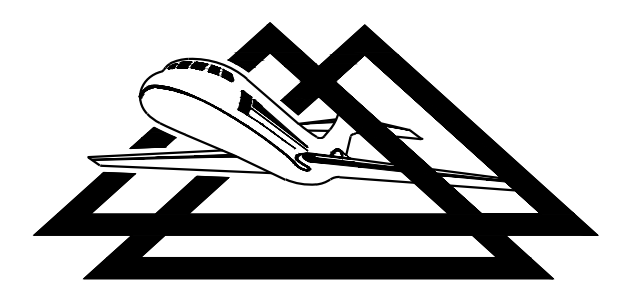
CONSTRUCT T-HANGARS	NMDOT-AD PROJECT NO. LAM-24-02	JOB NO. 23063
	T-HANGAR LAYOUT	
	SCALE: AS SHOWN	DATE: SEPTEMBER 2024
LOS ALAMOS COUNTY AIRPORT		DRAWN BY: MJH DESIGNED BY: CAR/ABH
		SHEET OF 12 OF 14



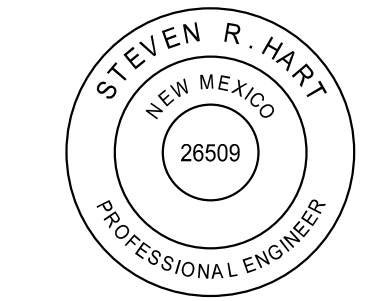
NOTE:
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NO.	REVISIONS	BY	APP.	DATE

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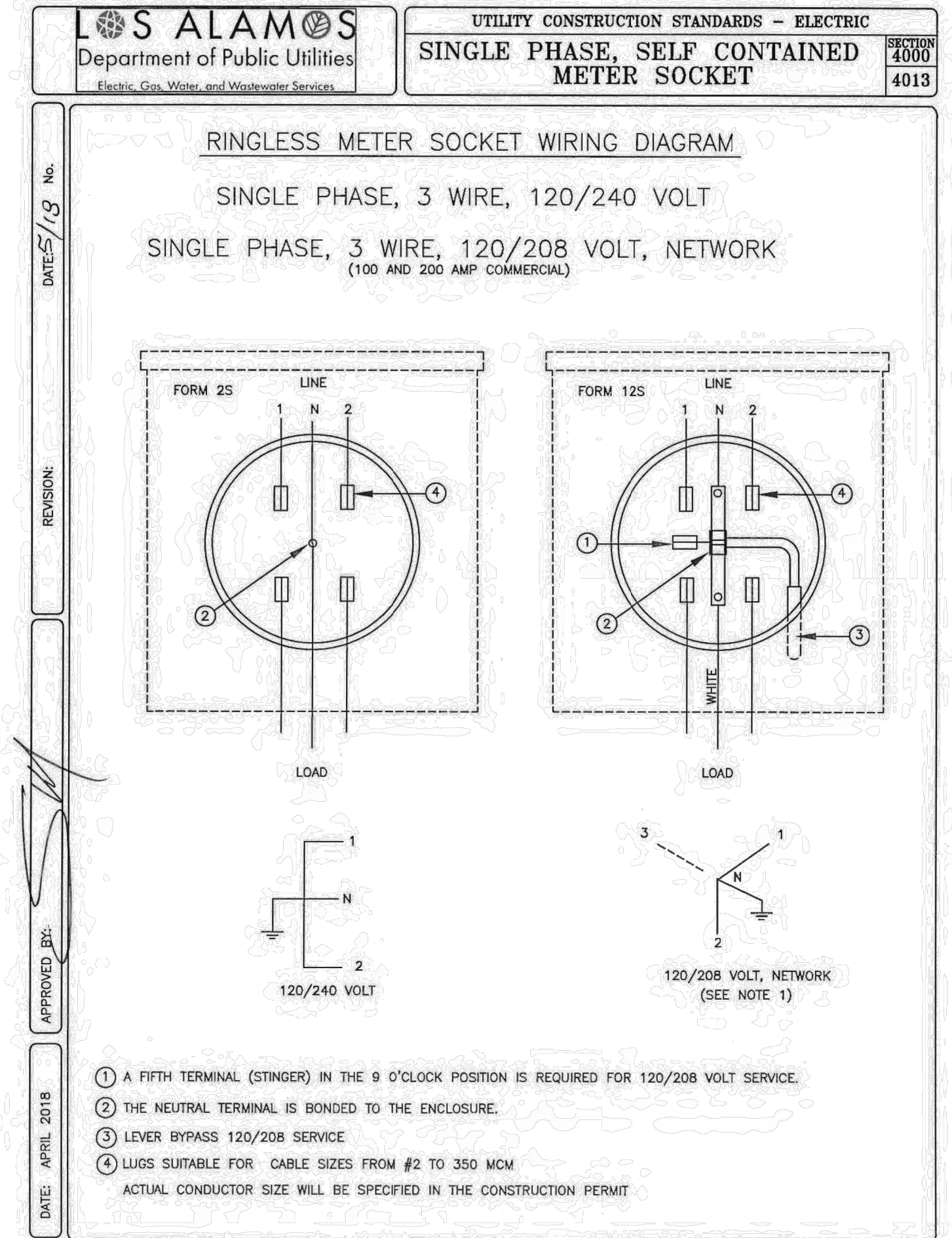
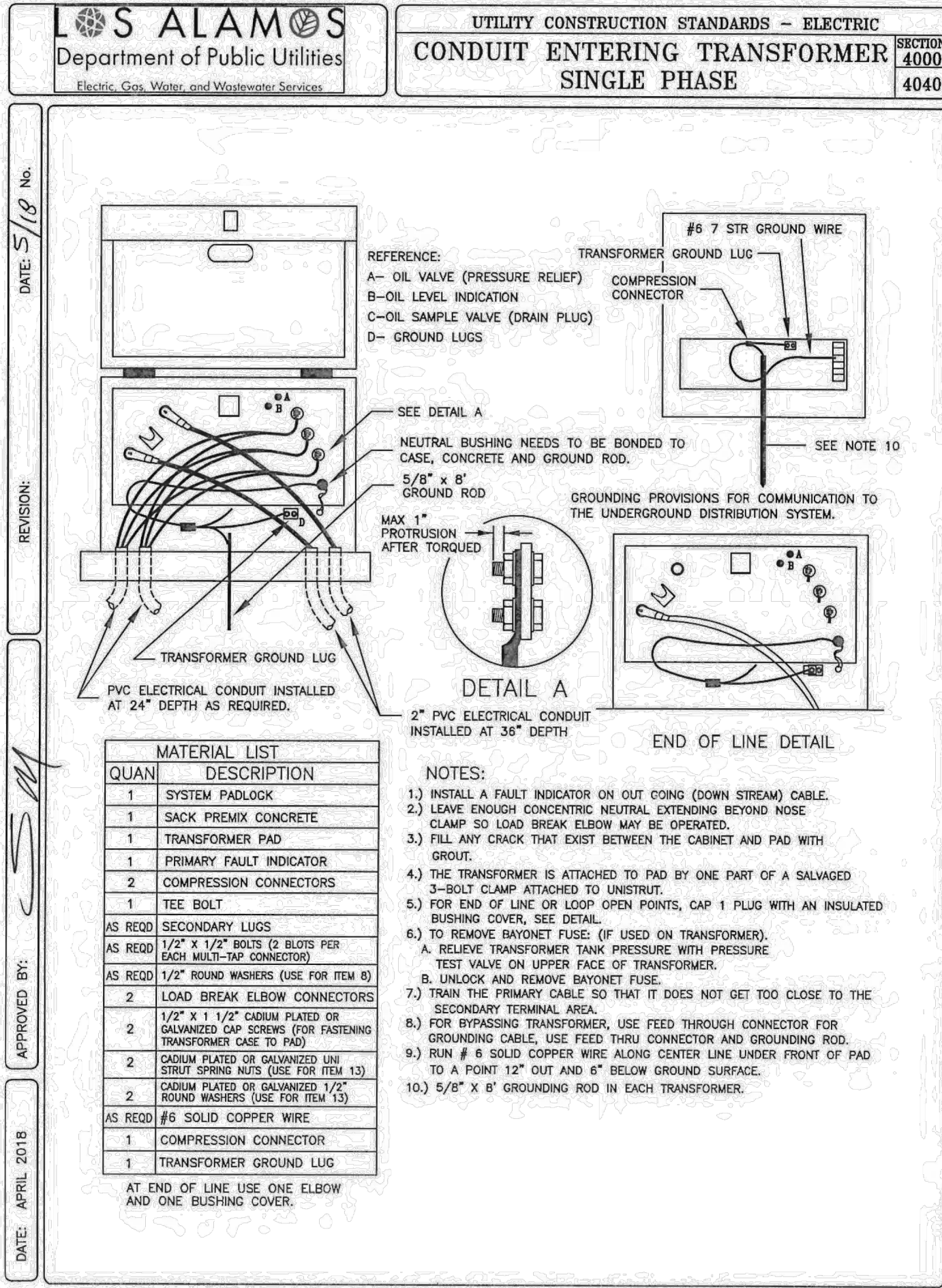
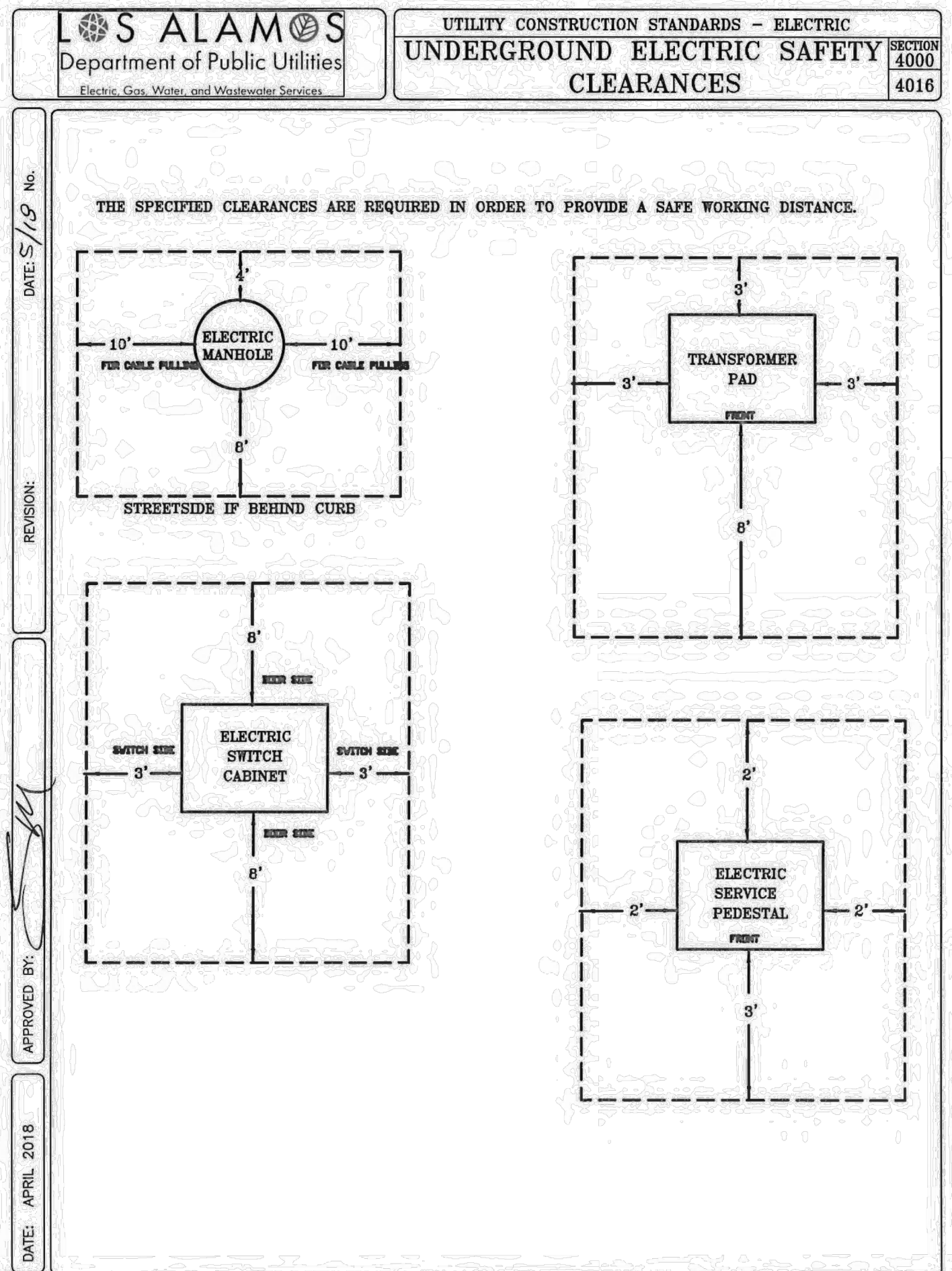
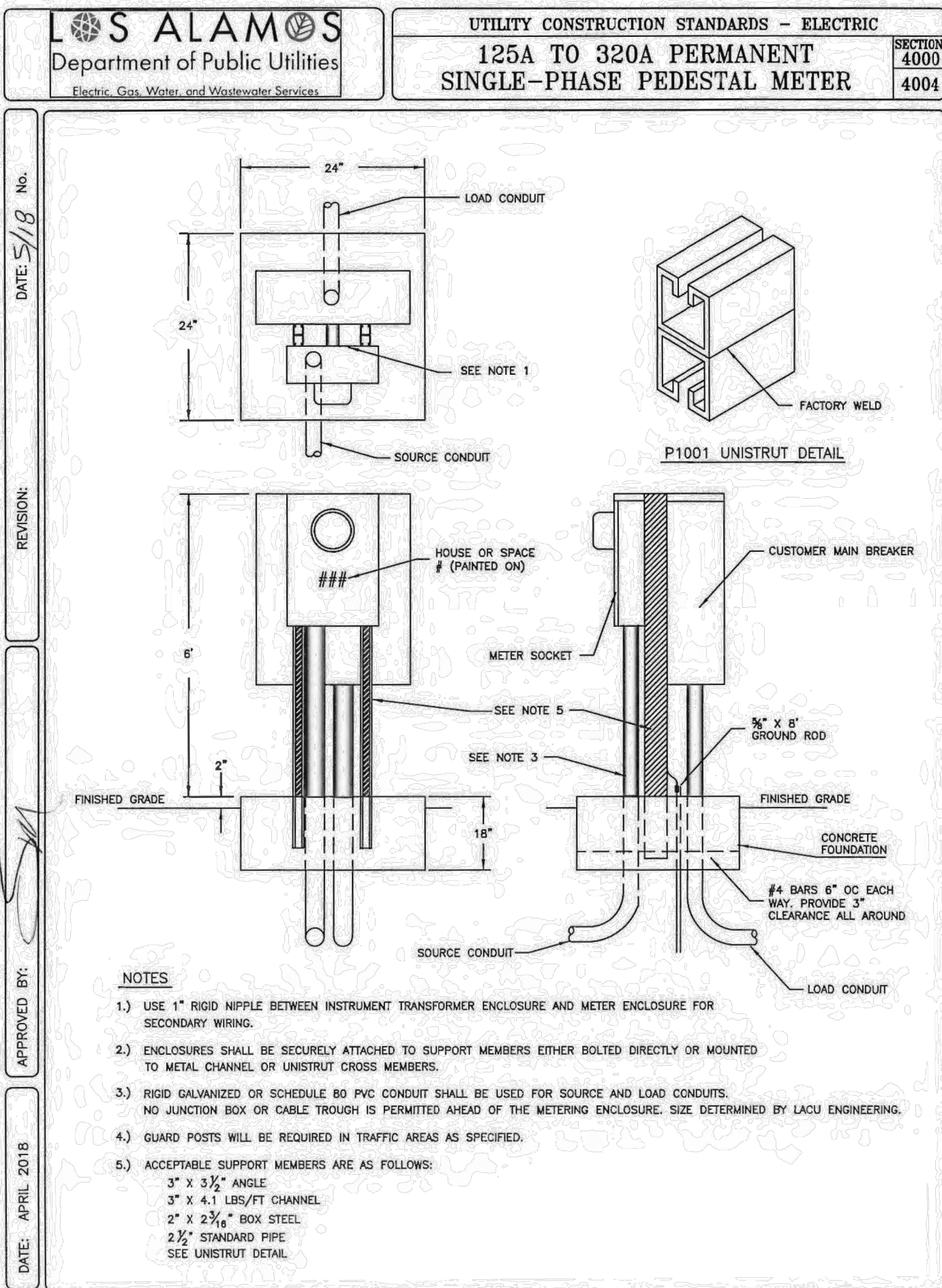
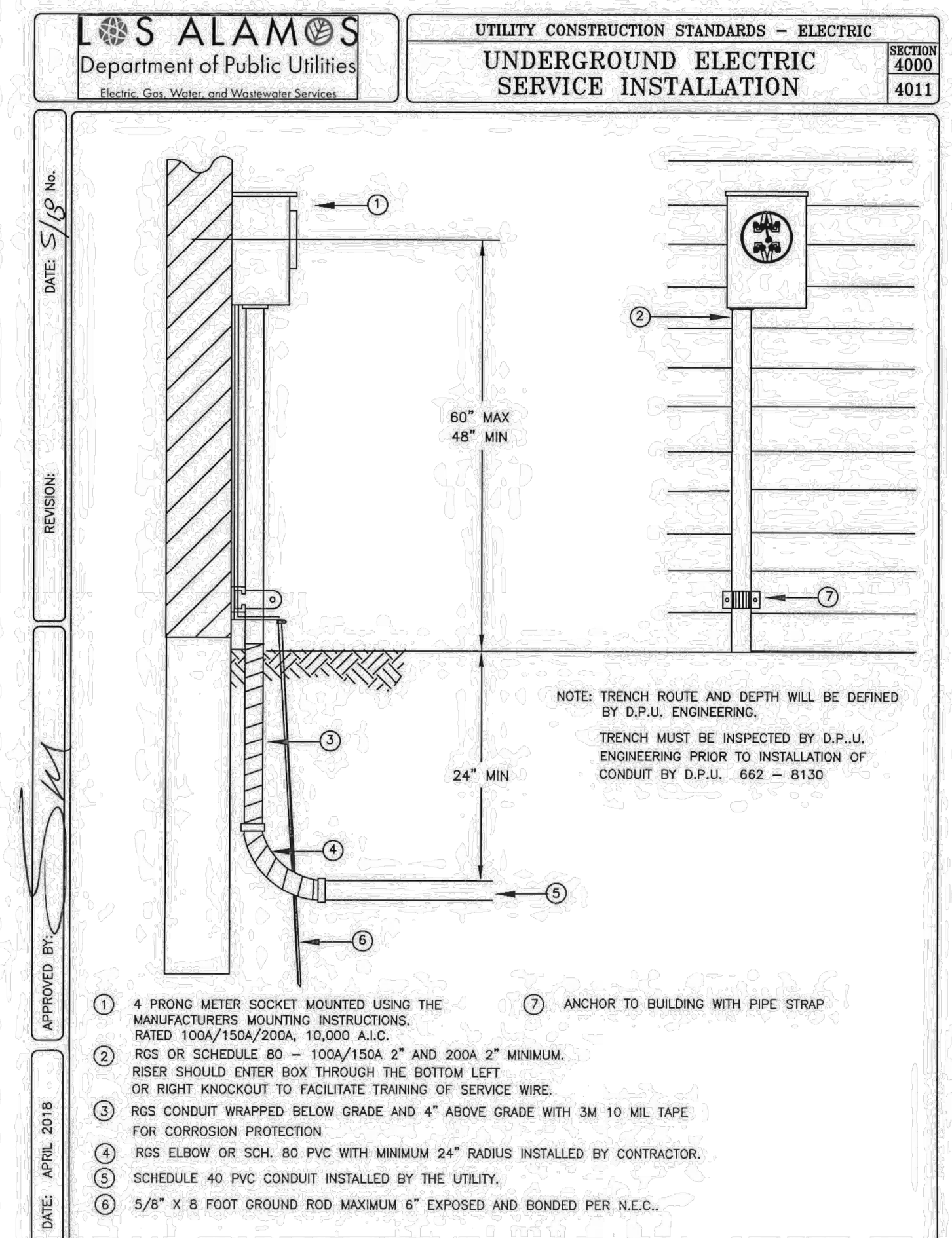
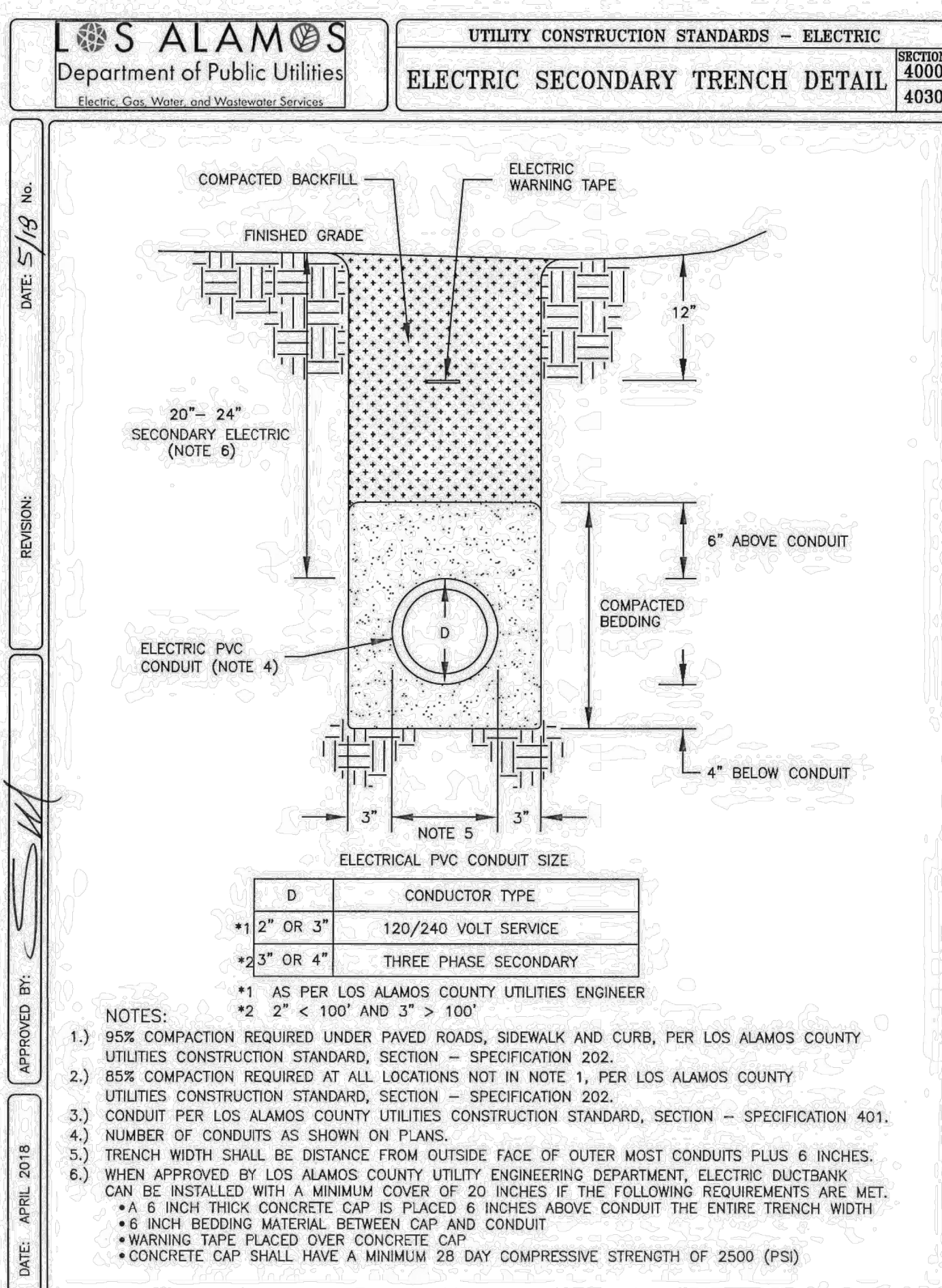
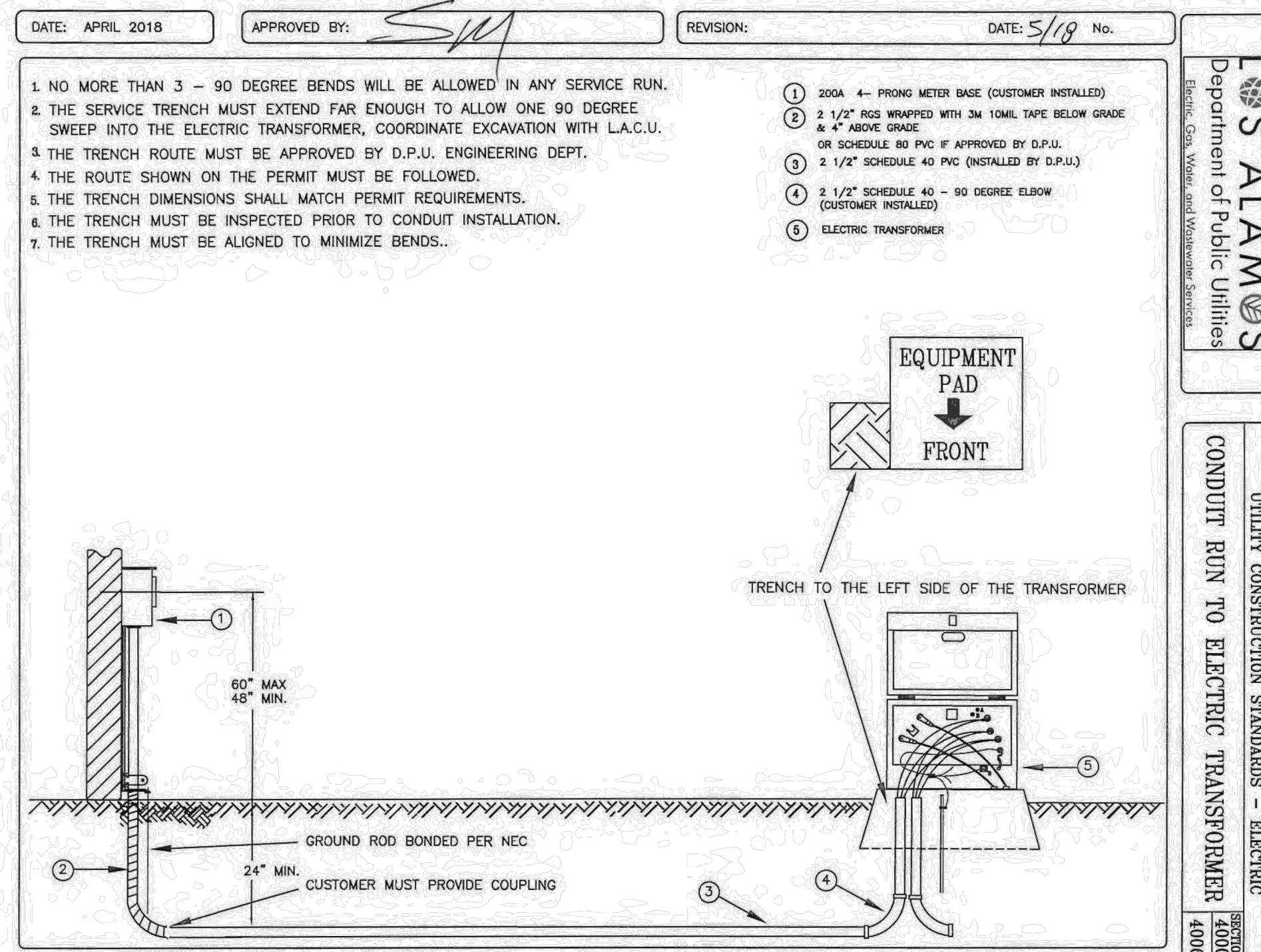


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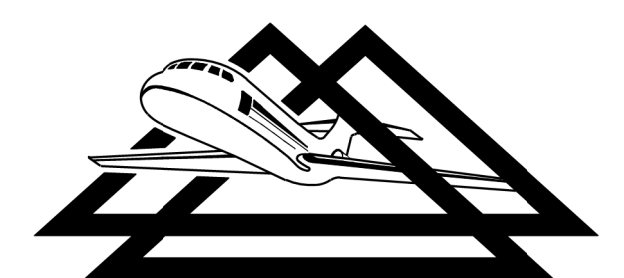


CONSTRUCT T-HANGARS	NMDOT-AD PROJECT NO. LAM-24-02	JOB NO. 23063
	DRAWN BY: MJH	SHEET 13
	DESIGNED BY: CAR/ABH	OF 14
LOS ALAMOS COUNTY AIRPORT	SCALE: NONE	DATE: SEPTEMBER 2024

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CONSTRUCT T-HANGARS	NMDOT-AD PROJECT NO. LAM-24-02	JOB NO. 23063
ELECTRICAL DETAILS	DRAWN BY: MJH	SHEET 14
LOS ALAMOS COUNTY AIRPORT	DESIGNED BY: CAR/ABH	OF 14
	SCALE: NONE	DATE: SEPTEMBER 2024

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NO.	REVISIONS	BY	APP.	DATE