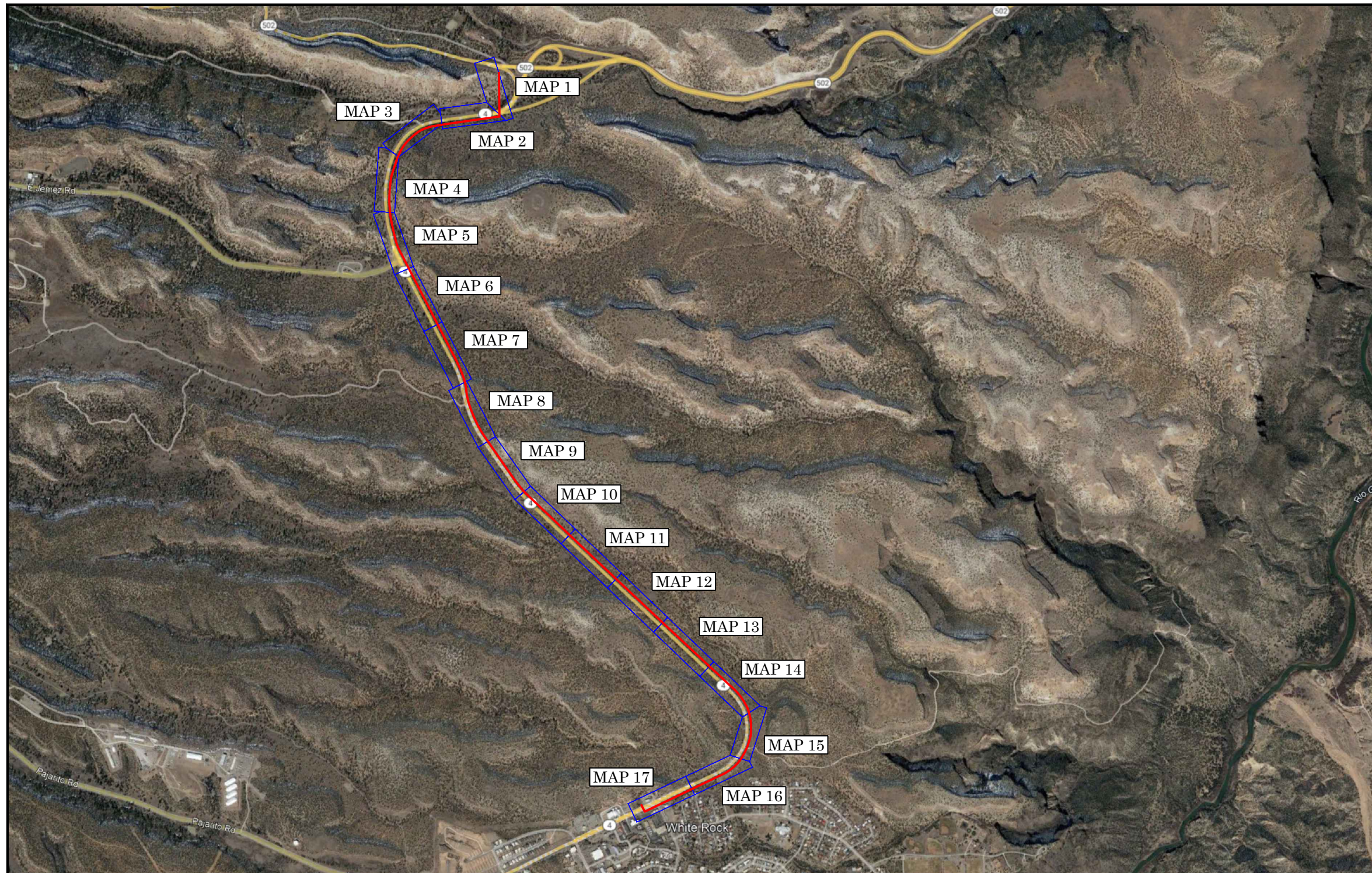


# SAN I NMDOT 502

SANTA FE, NM  
DECEMBER 2023

SHEET INDEX	
PAGE #	DESCRIPTION
1	COVER SHEET / VICINITY MAP / SHEET INDEX
2	LEGEND
3	CLARIFICATION SUMMARY SHEET
4	GENERAL NOTES
5-10	CONSTRUCTION DETAIL
11-27	MAP SHEET(S) (1-17)
28-29	NMDOT RIGHT-OF-WAY MAPS



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DRAWN BY:  
Robert Paroski  
APPROVED BY:  
Bryan Schalipp  
DATE:  
12/03/2023

DRAWING REVISIONS:

REV. NO.	DATE	DESCRIPTION
01	1/18/24	REVISED
02	2/29/24	REVISED
03	5/24/24	REVISED

SHEET TITLE:  
COVER SHEET / SHEET INDEX

PROJECT NAME:  
SAN I\_NMDOT 4 PARALLEL  
PROJECT LOCATION:  
4 ELK TRAIL RD SANTA FE, NM

PAGE  
1 OF 29



Know what's below.  
Call before you dig.



# LEGEND

## LINETYPES

	UG FIBER – EXISTING
	UG FIBER – PROPOSED
	UG FIBER – RETIRED
	AERIAL FIBER – EXISTING
	AERIAL FIBER – PROPOSED
	AERIAL FIBER – RETIRED
	LEASED – EXISTING
	LEASED – PROPOSED
	LEASED – RETIRED
	GAS
	WATER
	TELEPHONE
	ELECTRIC
	SANITARY SEWER (SEW)
	STORM DRAIN
	FENCE
	CABLE TV
	STEAM
	OIL
	UNKNOWN UTILITY
	RIGHT OF WAY
	EDGE OF PAVEMENT
	SIDEWALK
	UTILITY EASEMENT
	PROPERTY LINE
	RAILROAD

SYMBOL	DESCRIPTION
ASW	ASPHALT SIDEWALK
BOC	BACK OF CURB
BOS	BACK OF SIDEWALK
CSW	CONCRETE SIDEWALK
EOP	EDGE OF PAVEMENT
EOTW	EDGE OF TRAVEL WAY
FOC	FIBER OPTIC CABLE
HDPE	HIGH DENSITY POLYETHYLENE
HH	HANDHOLE
JB	JUNCTION BOX
MH	MANHOLE
MP	MILE POST
O/S	OFFSET
PVC	POLY VINYL CHLORIDE
RGS	RIGID GALVANIZED STEEL CONDUIT
ROW	RIGHT OF WAY
STA.	STATION

HANDHOLE – EXISTING

HANDHOLE – PROPOSED

TERMINATION PANEL – EXISTING

TERMINATION PANEL – PROPOSED

TERMINATION PANEL – RETIRED

MARKER POST

TEST STATION MARKER POST

TEST STATION INSIDE ACCESS POINT

AERIAL STORAGE – EXISTING

AERIAL STORAGE – PROPOSED

POLE ANCHOR/DOWN GUY – EXISTING

POLE ANCHOR/DOWN GUY – PROPOSED

PROPOSED DOWN GUY ON EXISTING ANCHOR

SPLICE POINT – EXISTING

SPLICE POINT – PROPOSED

CELLTOWER / 5G TOWER  
CELL TOWER/5G TOWER

MPT – EXISTING

MPT – PROPOSED

MPT – RETIRED

EXISTING MANHOLE OTHERS

EXISTING HANDHOLE OTHERS

EXISTING TELEPHONE

EXISTING POWER VAULT

CATCH BASIN/INLET

FIRE HYDRANT

GROUND/BOND

STREET LIGHT

TREE

CULVERT

WING WALL

BRIDGE

MISC. UTILITY

UTILITY POLE – EXISTING

POLE – PROPOSED

#F IN: 0  
OUT: 0

SEQUENTIAL CALLOUT

#F IN: 0  
TAIL: 0

SEQUENTIAL IN TAIL CALLOUT

#F TAIL: 0  
OUT: 0

SEQUENTIAL TAIL OUT CALLOUT



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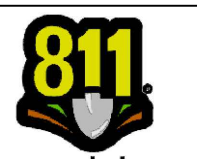
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SAN\_LNMDOT 4 PARALLEL  
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4 ELK TRAIL RD SANTA FE, NM

PAGE  
2 OF 29



Know what's below.  
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# BOM

MATERIAL BOM SHEET				
ITEM	ITEM DESCRIPTION	UNITS	QUANTITY	MMID
1	30" x 48" x 36" HAND HOLE	EA	13	N/A
2	PULL STRING	PER FT	41,816'	N/A
3	Tracer Wire (2500/reel)	PER FT	20,908'	N/A
4	(2) 1.25" HDPE CONDUIT	PER FT	20,548'	N/A



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SAN L\_NMDOT 4 PARALLEL

PROJECT LOCATION:  
4 ELK TRAIL RD SANTA FE, NM

PAGE  
3 OF 29



THIS STAKING SHEET IS INTENDED & PREPARED FOR ROUTE DESIGNATION ONLY & THE ENGINEER HEREBY & EXPRESSLY SET OUT IN THE CONTRACT, DISCLAIMS ANY & ALL RESPONSIBILITY AND/OR OBLIGATION FOR MARKING, NOTING AND/OR REFLECTING THE EXISTENCE AND/OR LOCATION OF ANY & ALL UNDERGROUND FACILITIES, INCLUDING BUT NOT LIMITED TO PETROLEUM BEARING PIPELINES, NOT WITHSTANDING SAID FACILITIES MAY BE VISIBLE AND/OR MARKED ON THE SURFACE ALONG, OVER OR ACROSS SAID PROPOSED ROUTE AS SHOWN ON THIS STAKING SHEET. SCALE IS APPROXIMATE AND SHOWN NOT TO BE USED TO DETERMINE ACTUAL LOCATIONS.

**GENERAL NOTES**

CONTRACTOR MUST OBTAIN LOCATES PRIOR TO DISTURBING THE GROUND.

CONTRACTOR MUST HAVE A COPY OF THE APPROVED PERMIT FROM THE APPROPRIATE AGENCY ON THE JOBSITE AT ALL TIMES.

ALL CABLE WILL BE PLACED AT STANDARD MINIMUM DEPTH. (SAN I STANDARD IS 36" DEEP UNLESS OTHERWISE DIRECTED BY A SAN I REPRESENTATIVE.)

ANY LANDSCAPING WILL BE REPLACED TO EQUAL OR BETTER THAN THAT WHICH EXISTED PRIOR TO WORK.

PROJECT SITE WILL BE PROPERLY SECURED PRIOR TO THE END OF EACH DAY.

ALL WORK IS TO BE IN ACCORDANCE WITH ALL AUTHORITIES HAVING JURISDICTION IN THE WORK ZONE.

CONTRACTORS ARE ADVISED TO CONTACT SAN I FOR ANY ADDITIONAL INFORMATION OR CLARIFICATION CONCERNING SCOPE OF WORK OR THE REQUIREMENTS NECESSARY FOR PROJECT COMPLETION.

CONTRACTOR IS RESPONSIBLE TO FIELD VERIFY ALL DIMENSIONS, QUANTITIES AND EXISTING CONDITIONS PRIOR TO CONSTRUCTION. IF A SIGNIFICANT CHANGE TO THE RUNNING LINE IS NEEDED, PLEASE CONTACT YOUR SAN I REPRESENTATIVE BEFORE PROCEEDING.

BEFORE CONSTRUCTION BEGINS, CONTRACTOR SHALL TAKE APPROPRIATE PRECAUTIONS TO AVOID ANY POTENTIAL OBSTRUCTIONS PRIOR TO PROCEEDING WITH WORK.

NO CONSTRUCTION ON PRIVATE PROPERTY WILL COMMENCE UNTIL APPROVAL IS GIVEN BY THE APPROPRIATE SAN I EMPLOYEE.

CONTRACTOR SHALL NOT PROCEED WITH WORK UNTIL THEY HAVE RECEIVED A PURCHASE ORDER AND HAVE BEEN DIRECTED TO DO SO BY AN AUTHORIZED SAN I REPRESENTATIVE.

CONTRACTOR SHALL NOT EXCEED THE PURCHASE ORDER VALUE WITHOUT AUTHORIZATION IN WRITING FROM THE APPROPRIATE SAN I REPRESENTATIVE.

AS-BUILTS WILL BE REQUIRED FOR EACH PROJECT INCLUDING CABLE FOOTAGE SEQUENTIALS AT EVERY ACCESS POINT, SLACK LOOP, SPLICE LOCATION, POLE AND TERMINATION POINT. CONTRACTOR SHOULD ALSO PROVIDE NOTES OF ALL CHANGES IN DEPTHS, RUNNING LINES, WH/HH LOCATIONS, AND ANY OTHER APPLICABLE NOTES TO DEPICT THE WORK THAT TOOK PLACE. NOTE: ALL MAJOR CHANGES NEED TO BE PRE-APPROVED BY AN AUTHORIZED SAN I EMPLOYEE PRIOR TO STARTING THE WORK.

**SITE CONDITIONS**

THE ACTUAL LOCATION OF EXISTING CONDUIT AND CABLES MAY VARY FROM THE LOCATION SHOWN. REPAIR OF ANY DAMAGED CONDUIT CONTAINING CABLE SHALL BE MADE BY USE OF PVC SPLIT DUCT. THE CONTRACTOR SHALL ENCLOSE THE EXISTING CABLES IN PVC.

THE LOCATIONS OF EXISTING UTILITIES SHOWN IN THIS PLAN ARE APPROXIMATE. WHEN WORK IS TO BE CONDUCTED IN THE VICINITY OF KNOWN UTILITIES, THEIR ACTUAL LOCATION MUST BE FIELD VERIFIED TO AVOID CONFLICTS IR DAMAGE TO THOSE UTILITIES. VARIATION IN LOCATION BETWEEN "RECORDED POSITIONS" AND ACTUAL POSITIONS SHOULD BE ANTICIPATED.

IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO VERIFY THE LOCATION OF ALL UNDERGROUND UTILITIES. BURIED UTILITIES MAY EXIST IN THE AREA IN ADDITION TO THOSE SHOWN ON THE PLAN. THE CONTRACTOR SHALL CONTACT PROPERTY OWNERS WHEN WORKING WITHIN PRIVATE EASMENTS FOR LOCATION OF UNDERGROUND TANKS, PIPELINES, DRAIN TILES, OR OTHER BURIED IMPROVEMENTS. THE CONTRACTOR SHALL ALSO NOTIFY THE UTILITY NOTIFICATION CENTER PRIOR TO COMMENCING ANY CONSTRUCTION ACTIVITIES.

THE CONTRACTOR MUST ASSUME ALL BURIED UTILITIES ENCOUNTERED ARE ALIVE AND ACTIVE UNLESS SPECIFICALLY INSTRUCTED OTHERWISE BY OWNERS OR OPERATORS OF SAID UTILITIES.

DAMAGE TO SUB-SURFACE STRUCTURES IS THE SOLE RESPONSIBILITY OF THE PLACING CONTRACTOR.

THE CONTRACTOR SHALL PROTECT THE EXISTING TRAFFIC CONTROL LOOPS. IF EXISTING TRAFFIC CONTROL LOOPS ARE DAMAGED DURING CONSTRUCTION, THE ENTIRE LOOPWIRE FROM TERMINAL TO TERMINAL SHALL BE REPLACED IN ACCORDANCE WITH GOVERNING AGENCY STANDARDS AND REGULATIONS AT CONTRACTOR'S EXPENSE.

REMOVAL OF EXISTING ASPHALT PAVEMENT, CONCRETE CURBS, AND CONCRETE SIDEWALKS WILL BE "NEAT LINE" WITH SAW OR PAVEMENT CUTTER, PER REQUIREMENTS AND SPECIFICATIONS OF THE AGENCY OR DEPARTMENT RESPONSIBLE FOR EACH LOCATION. IF CONCRETE PAVEMENT IS ENCOUNTERED WHILE EXCAVATING CONDUIT TRENCHES, THE CONCRETE REMOVAL WILL BE "NEAT LINE" WITH A PAVEMENT SAW.

IF CONCRETE CURB RETURNS AND/OR SIDEWALKS ARE REPLACED DUE TO CONDUIT OR MANHOLE INSTALLATION, THE CONTRACTOR SHALL PLACE APPROVED HANDICAPPED SIDEWALK AND CURB ACCESS RAMPS IN CONFORMANCE WITH STATE OF JURISDICTION STATUTES.

ALL MATERIALS NECESSARY FOR THE REPAIR OF STREETS, CURBS, SIDEWALKS, SANITARY SEWERS, STORM SEWERS, AND PUBLIC SERVICE UTILITIES, AND THE INSTALLATION OF SUCH MATERIALS SHALL BE IN CONFORMANCE WITH THE REQUIREMENTS AND SPECIFICATIONS OF THE AGENCY OR DEPARTMENT RESPONSIBLE FOR THE OPERATION AND MAINTENANCE OF THE REPAIRED FACILITY.

ALL WORK SHALL CONFORM TO THE SPECIFICATIONS OF THE JURISDICTIONAL PERMIT AGENCY.

ALL OPEN TRENCH WILL BE CLEARLY MARKED WITH BARRICADES OR CONES. STEEL PLATES OR OTHER TYPES OF BRIDGING SHALL BE PROVIDED TO COVER OPEN TRENCH IN THE TRAVEL PORTION OF THE STREETS. THESE PLATES OR BRIDGING SHALL BE ADEQUATE TO SUPPORT THE NORMAL VEHICLE LOADS ANTICIPATED IN THIS AREA AND SHALL BE IN PLACE DURING ALL NON-WORKING AREAS.

ALL SURFACES TO BE RESTORED TO ORIGINAL CONDITION, AND BACKFILL TO BE COMPACTED AS SPECIFIED. TRENCH EXCAVATION IN SURFACES WHICH INCLUDE CONCRETE TREATED BASE SHALL FOLLOW LOCAL AREA SPECIFICATIONS.

**HAZARDOUS MATERIALS**

THE CONTRACTOR SHALL NOTIFY THE JURISDICTIONAL PERMIT AGENCY IMMEDIATELY IF ANY MATERIALS ARE ENCOUNTERED THAT ARE CONSIDERED HAZARDOUS BY THE EPA, DEQ, OR OSHA. IF POTENTIALLY HAZARDOUS MATERIALS ARE ENCOUNTERED THE CONTRACTOR SHALL SECURE THE SITE AND PREVENT THE ACCIDENTAL EXPOSURE BY THE PUBLIC OR THE CONTRACTOR'S PERSONNEL.

THE CONTRACTOR MAY EXCAVATE UP TO, BUT SHALL NOT DISTURB KNOWN HAZARDOUS MATERIALS SUCH AS ASBESTOS, OILS, ACID, ETC. THE REMOVAL OF ALL HAZARDOUS MATERIALS MUST BE DONE BY AN APPROVED OR CERTIFIED HAZARDOUS MATERIALS CONTRACTOR LICENSED BY THE STATE OF JURISDICTION.

A COPY OF ALL CORRESPONDENCE PERTINENT TO THE REMOVAL OF HAZARDOUS MATERIALS SHALL BE TRANSMITTED TO OWNER AND A COPY SHALL BE AVAILABLE AT THE PROJECT OFFICE AND THE JOB SITE.

**AERIAL NOTES**

- AERIAL CONSTRUCTION TO BE PERFORMED TO INDUSTRY ACCEPTABLE STANDARDS.
- ALL HEIGHTS OF CABLE PLACEMENT WILL BE RECORDED AT TIME OF CONSTRUCTION. DOCUMENT ALL POINTS OF ATTACHMENT.
- 6.6M STRAND WILL BE USED WITH STANDARD 5/8 HARDWARE.
- ALL EXTENSION ARMS TO BE PLACED WILL BE EPOXY ARMS UNLESS OTHERWISE NOTED OR APPROVED BY THE INSPECTOR.
- BOND STRAND TO POWER MGN WHERE APPLICABLE. ANCHORS TO BE USED WILL BE 3/4 SCREW IN TYPE.
- ALL STRAPS WILL BE PLACED 4" BEFORE AND AFTER EVERY SUPPORTING CLAMP AT A MINIMUM OF 21" APART.
- P.O.A. = POINT OF ATTACHMENT.
- ADD MISSING GROUNDS.
- REPAIR/REPLACE EXISTING LASHING WIRE IF DAMAGED.

**CONSTRUCTION STAKING**

IN AREAS WHERE THE CONDUIT ALIGNMENT IS NOT CLEARLY DEFINED BY CURB LINES, FENCE LINES, OR OTHER EVIDENCE OF THE RIGHT-OF-WAY, THE ENGINEER WILL PROVIDE CENTERLINE STAKES OR PAINT MARKS WHERE REQUIRED TO MAKE THE PROPOSED CONDUIT ALIGNMENT EVIDENT.

MANHOLE CENTERS WILL BE FIELD STAKED BY THE ENGINEER WHEN REQUESTED WITH OFFSET STAKES AT RIGHT ANGLES (90°) TO THE CONDUIT ALIGNMENT.

CLOSURES IDENTIFIED IN THE PLANS SHALL BE LOCATED BY THE CONTRACTOR. DEVIATION FROM PLAN LAYOUT SHALL BE APPROVED BY THE ENGINEER PRIOR TO CONDUIT AND/OR CLOSURE INSTALLATION.

IF ADDITIONAL, FIELD STAKING OR LOCATION OF CONDUITS, MANHOLES, PROPERTY LINES, ETC., BECOMES NECESSARY, THE CONTRACTOR IS TO NOTIFY THE INSPECTOR OR THE ENGINEER TWO WORKING DAYS PRIOR TO BEGINNING THE WORK.

**PERMITS - FRANCHISES - EASEMENTS**

PHYSICAL WORK SHALL NOT BE STARTED UNTIL THE GOVERNING AGENCY INSPECTOR AND THE CONTRACTOR ARE IN POSSESSION OF AND HAVE CAREFULLY REVIEWED AND FULLY UNDERSTAND ALL CONDITIONS AND SPECIFICATIONS SET FORTH IN THE REQUIRED PERMITS, FRANCHISES, AND/OR EASEMENTS.

PLACING FOREMAN TO HAVE A COPY OF THE PERMITS/EASEMENTS ON SITE AT ALL TIMES.

ANY CONFLICT BETWEEN WORK PRINT SPECIFICATIONS AND SPECIFICATIONS SET FORTH UNDER RELATED PERMITS, FRANCHISES, AND/OR EASEMENTS MUST BE CLEARED BY PROPER COMPANY AUTHORITY BEFORE PROGRESSING WITH WORK INVOLVED

**.TRAFFIC CONTROL**

THIS PROJECT WILL INVOLVE WORKING ALONG A MAJOR ARTERIAL ROAD AND HEAVY TRAFFIC VOLUME SHOULD BE ANTICIPATED.

UNIFORM TRAFFIC FLOW SHALL BE MAINTAINED AT ALL TIMES. ONLY EQUIPMENT AND MATERIALS NECESSARY FOR IMMEDIATELY SCHEDULED OR IN PROGRESS WORK WILL BE MAINTAINED IN THE WORK AREA. ALL OTHER EQUIPMENT AND MATERIALS WILL BE "STORED OR STOCKPILED" IN SUCH A MANNER AS TO ELIMINATE HAZARDOUS CONDITIONS FOR TRAFFIC OR PEDESTRIANS DURING NON-WORKING OR SHUT DOWN PERIODS.

TRAFFIC WARNING DEVICES AND SIGNS SHALL CONFORM TO THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES FOR STREETS AND HIGHWAYS (U.S. GOVERNMENT PRINTING OFFICE) AND TO THE STATE HIGHWAY DIVISION STANDARD SPECIFICATIONS FOR HIGHWAY CONSTRUCTION. HIGH LEVEL WARNING TYPE DEVICES ARE TO BE USED AT ALL TIMES AND SPECIAL WARNING DEVICES MAY BE STIPULATED BY THE JURISDICTIONAL PERMIT AGENCY AT ANY TIME THE USE WILL ADD TO THE SAFETY AND PROTECTION OF TRAFFIC OR PEDESTRIANS IN THE CONSTRUCTION AREA.

ALL CONDUIT TRENCHING IN PAVED AREAS SHALL BE BACKFILLED WITH CRUSHED GRAVEL OR COMPLETELY COVERED AT THE COMPLETION OF EACH WORKING DAY. ANY BACKFILLED TRENCH SHALL BE CAPPED WITH A MINIMUM LAYER OF ASPHALTIC CONCRETE COLD PATCH AT THE END OF EACH WORKING DAY.

THE CONTRACTOR SHALL MARK THE CONDUIT TRENCH AND DEFINE HIS CONSTRUCTION AREA CLEARLY WITH BARRICADES, CONES, AND/OR OTHER VISIBLE METHODS THAT ALERT THE PUBLIC OF THE CONSTRUCTION ACTIVITY.

A TRAFFIC CONTROL PLAN SHALL BE PREPARED BY THE CONTRACTOR AS REQUIRED AND SUBMITTED TO EACH PERMITTING AGENCY REQUESTING SUCH PLAN FOR REVIEW AND APPROVAL OR REVISION PRIOR TO COMMENCING ANY CONSTRUCTION ACTIVITY FOR THIS PROJECT. THE APPROVED PLAN SHALL BE SUBMITTED TO THE AGENCY AND A COPY OF THE PLAN SHALL BE KEPT AT THE CONSTRUCTION SITE AND MUST BE READILY AVAILABLE FOR REVIEW BY THE AGENCY REPRESENTATIVES.

**SPECIAL UTILITY CLEARANCES**

ALL WORK CONDUCTED ADJACENT TO WATER MAINS SHALL CONFORM TO THE FOLLOWING CONDITIONS:

A. WHEREVER POSSIBLE CONDUIT SHALL MAINTAIN A HORIZONTAL SEPARATION OF 3.0 FEET, MEASURED SURFACE TO SURFACE (OUTSIDE EDGE TO OUTSIDE EDGE), FROM PARALLEL WATER MAINS.

B. WHEREVER POSSIBLE, CONDUIT SHALL PASS UNDER EXISTING WATER MAINS AND MUST MAINTAIN 12" VERTICAL CLEAR SEPARATION. CONDUITS PASSING OVER WATER MAINS MUST ALSO MAINTAIN THE 12" VERTICAL SEPARATION.

C. THE CONTRACTOR SHALL BE RESPONSIBLE FOR ESTABLISHING AND MAINTAINING THIS REQUIRED VERTICAL SEPARATION BY EITHER EXPOSING THE WATER MAIN EVERY 100 FEET IN THOSE AREAS WHERE HORIZONTAL SEPARATION IS LESS THAN 3.0 FEET OR BY UTILIZING THE DEPTHS OF ADJACENT WATER VALVES. IF THE CONTRACTOR UTILIZES THE ADJACENT WATER TO DETERMINE WATER MAIN DEPTH, HE SHALL CONTACT THE AGENCY AT EACH SUCH LOCATION AND THE AGENCY WILL DETERMINE THE NECESSARY DEPTH OF THE TOP OF THE CONDUIT AT THAT POINT.

D. THE VERTICAL AND HORIZONTAL SEPARATION SHALL BE MAINTAINED AT ALL TIMES UNLESS SPECIFICALLY REVISED BY AGREEMENT BETWEEN THE JURISDICTIONAL PERMIT AGENCY AND THE AGENCY ANY SPECIFIC DEVIATION IN VERTICAL AND HORIZONTAL SEPARATION FROM THOSE DESCRIBED SHALL BE REPORTED TO THE OWNER BY THE CONTRACTOR. THE CONTRACTOR SHALL BE RESPONSIBLE FOR MAINTAINING VERTICAL AND HORIZONTAL SEPARATION AT ALL TIMES AND SHALL BE RESPONSIBLE FOR ANY AND ALL ENCROACHMENTS.

E. CLEARANCES TO STORM SEWERS AND SANITARY SEWERS SHALL BE EXACTLY THE SAME AS THOSE TO WATER MAINS.

**STRUCTURE PROTECTION**

MANHOLES AND CONDUIT TO BE PLACED ADJACENT TO EXISTING STRUCTURES SUCH AS BRIDGE BRIDGE FOOTINGS/PIERS, BUILDING FOUNDATIONS, WALLS, POWER AND TELEPHONE POLES, AND OTHER UTILITIES SHALL MAINTAIN A MINIMUM CLEARANCE AS SHOWN. THE CONTRACTOR SHALL NOT UNDERMINE ANY ADJACENT STRUCTURE WITHOUT SPECIFIC WRITTEN PERMISSION FROM THE OWNER/OPERATOR OF SUCH STRUCTURE.

SHORING USED AS FOUNDATION SUPPORT SHALL BE DESIGNED SPECIFICALLY FOR BOTH THE LIVE AND DEAD LOADS OF THE STRUCTURE, OR IF ONLY THE DEAD LOAD IS USED FOR DESIGN, THE CONTRACTOR SHALL PROVIDE A DETAILED LAYOUT AND PLAN OF THE METHOD OF ESTABLISHING AND MAINTAINING THE DESIGN LOAD CONDITIONS (I.E., ROAD DETOURS, TIEBACKS, ETC.).

SEE UTILITY CLEARANCE SECTION NOTES FOR CLEARANCE CRITERIA TO PARALLEL OR CROSS UTILITIES.

EXISTING UTILITIES EXPOSED DURING EXCAVATION SHALL BE 100% SUPPORTED BY EITHER TRENCH BRIDGING AND SUSPENSION OR BY THE USE OF LONGITUDINAL TRAYS OR PLATFORMS VERTICALLY SUPPORTED BY ADJUSTABLE BUILDING JACKS.

EXISTING SPLICE CASES AND CABLES SHALL BE SUPPORTED BY SUSPENSION FROM A CROSSING BEAM. SUPPORTS SHALL BE PLACED AT A MAXIMUM SPACING OF 4.0 FEET AND SHALL CONSIST OF A CANVAS SLING WITH NYLON BELTING OR ROPE. ALL CABLE SUPPORTS SHALL BE PLACED IN A MANNER THAN PREVENTS KINKS OR OTHER DAMAGE TO THE CABLE SHEATH.

AN ACCEPTABLE ALTERNATIVE TO CABLE SLINGS WOULD BE THE UTILIZATION OF A WIDE FLANGE "I" BEAM OR CHANNEL AS A "CABLE TRAY" WITH THE CABLES/CASES BANDED IN PLACE.

**SHORING**

THE CONTRACTOR SHALL PROVIDE SHORING FOR CONDUIT TRENCH EXCAVATION 42" OR MORE IN DEPTH AS MEASURED FROM THE HIGH SIDE OF THE TRENCH AND FOR ALL MANHOLE EXCAVATION.

MANHOLE SHORING SHALL BE TIGHT-SHEETED.

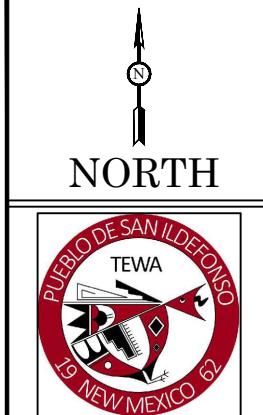
ALL SHORING SHALL CONFORM TO THE STANDARDS AND SPECIFICATIONS OF LOCAL COUNTY AND THE OCCUPATIONAL SAFETY AND HEALTH ADMINISTRATION (OSHA).

SHORING SHALL BE DESIGNED TO MEET H-20 HIGHWAY LOADING.

THE CONTRACTOR SHALL PROVIDE ALL SHORING AND DESIGN CALCULATIONS TO THE PERMIT ISSUING AGENCY PRIOR TO COMMENCING ANY CONSTRUCTION ACTIVITY.

**NMDOT**

- WORK TO BE PREFORMED IN NMDOT ROW SHALL MEET NMDOT STANDARDS, NMDOT MUST BE CONTACTED FIVE (5) DAYS PRIOR TO COMMENCEMENT OF CONSTRUCTION WITHIN NMDOT ROW.
- ANY VEGETATION THAT IS DUG UP SHALL BE REMOVED & DISPOSED OF OUTSIDE OF NMDOT ROW.
- SUBMITTAL OF CONTRACTOR'S AND ALL SUBCONTRACTOR'S CERTIFICATE OF INSURANCE, WITH NMDOT LISTED AS ADDITIONAL INSURED AND THE AMOUNTS STATED IN THE NMDOT RAILROADS & UTILITIES MANUAL PRIOR TO COMMENCEMENT OF CONSTRUCTION.
- ALL EXCAVATION WITHIN THE ROW SHALL BE COMPACTED TO A MINIMUM OF 95% OF MAXIMUM DRY DENSITY AS DETERMINED BY ASTM D-1557.
- ROW SHALL BE RESEEDED AND RESTORED TO ORIGINAL CONDITION TO MEET PERMIT REQUIREMENTS.
- TRAFFIC CONTROL DEVICES WILL BE IN ACCORDANCE WITH NMTCD SPECS.
- EXISTING TRAFFIC CONTROL SHALL BE KEPT INTACT AND BE REINSTALLED TO MATCH OR EXCEED CONDITION PRIOR TO CONSTRUCTION COMMENCEMENT. REPLACEMENT OF DAMAGED TRAFFIC CONTROL SHALL BE DONE AT CONTRACTOR'S EXPENSE.



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APPROVED BY:  
**Bryan Schalipp**

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SHEET TITLE: **GENERAL NOTES**

PROJECT NAME: **SAN L\_NMDOT 4 PARALLEL**

PROJECT LOCATION: **4 ELK TRAIL RD SANTA FE, NM**

PAGE  
**4 OF 29**

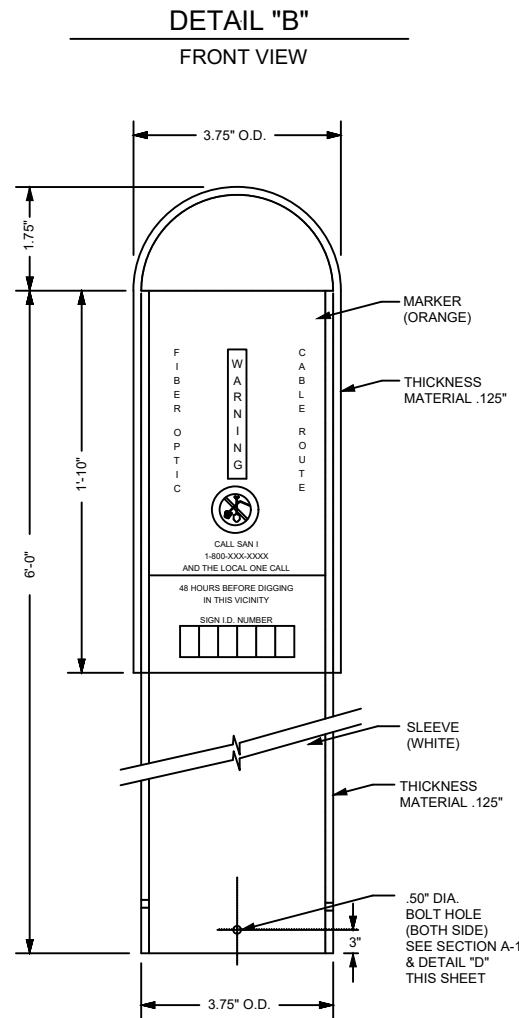
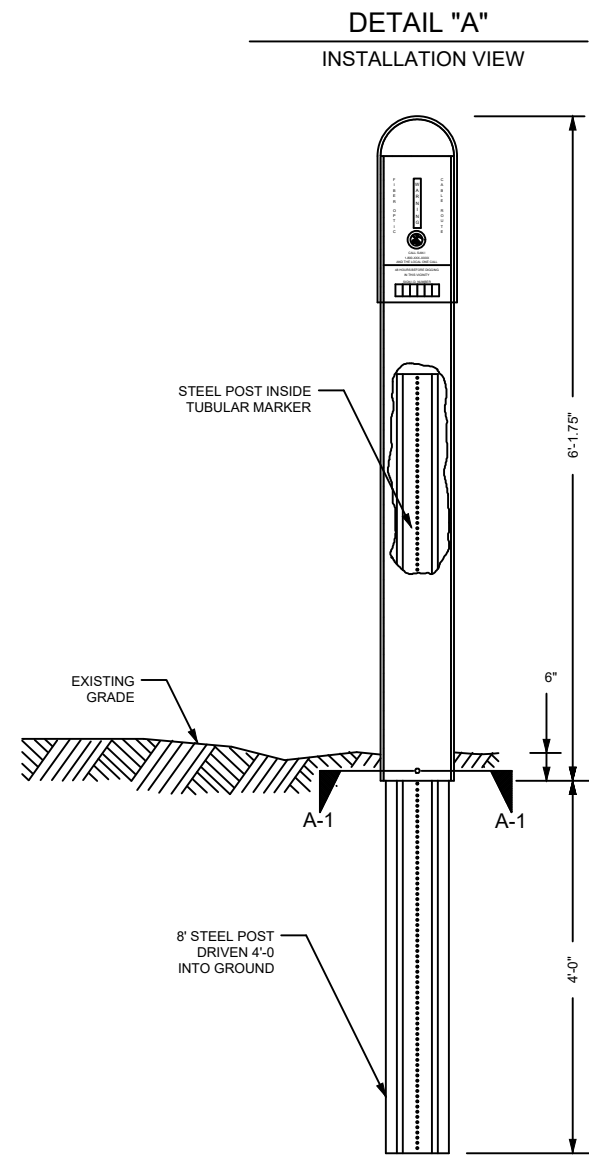




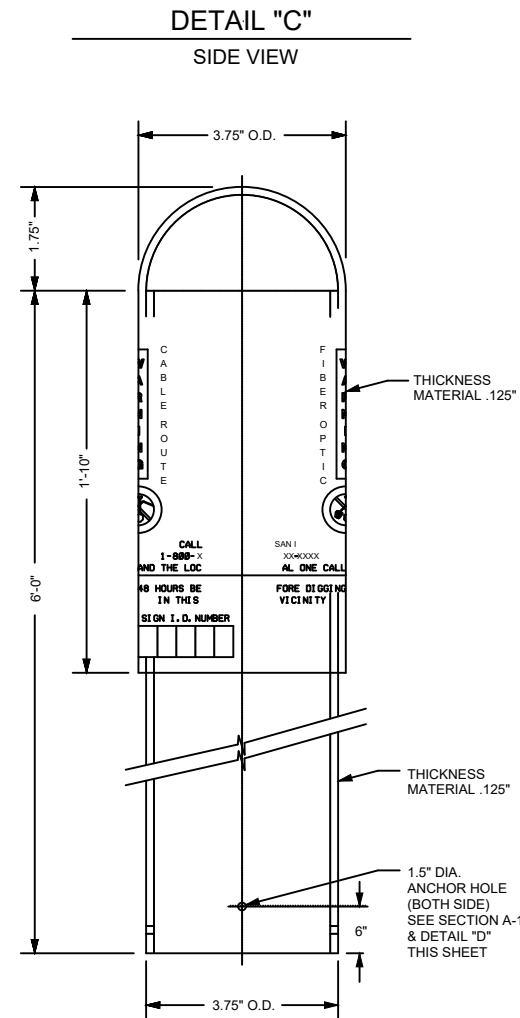
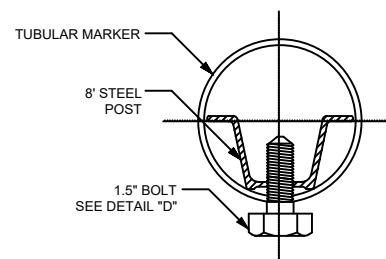
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# BURIED CABLE CONSTRUCTION DETAILS

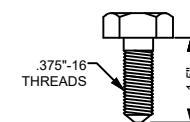
## TUBULAR MARKER DETAILS



**SECTION A-1**  
TUBULAR MARKER



**DETAIL "D"**  
1.5" BOLT



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4 ELK TRAIL RD SANTA FE, NM

PAGE  
5 OF 29



Know what's below.  
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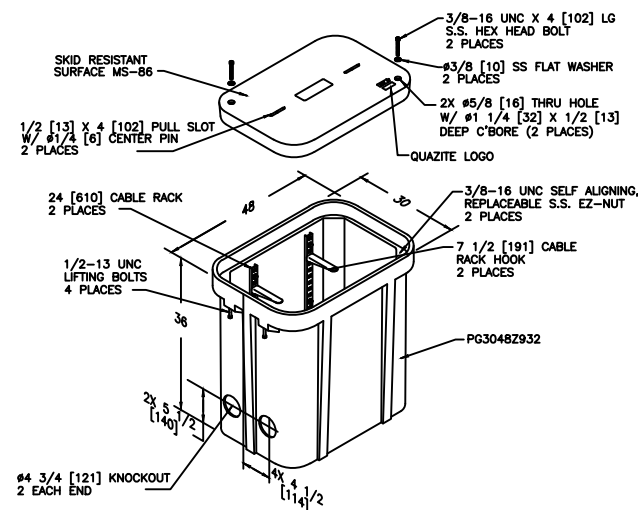


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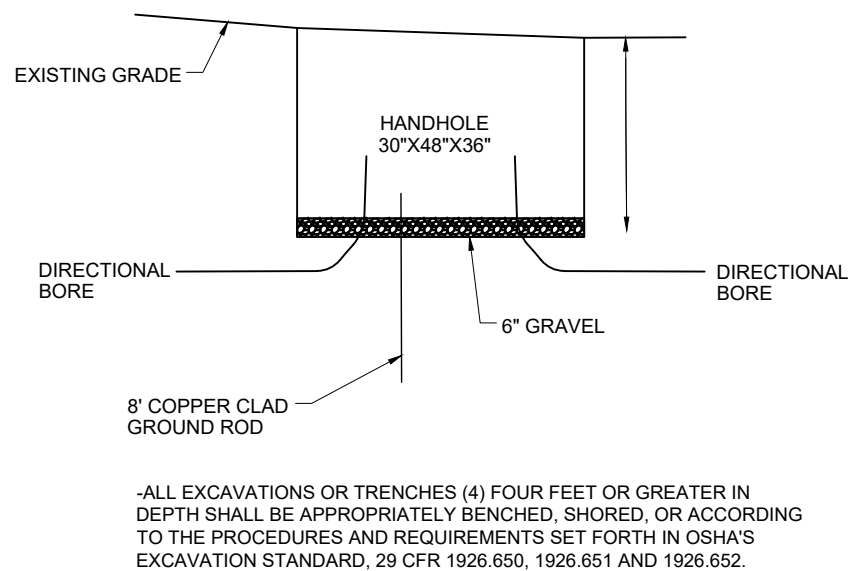
# BURIED CABLE CONSTRUCTION DETAILS

## HANDHOLE SIZE 30"x48"x36"

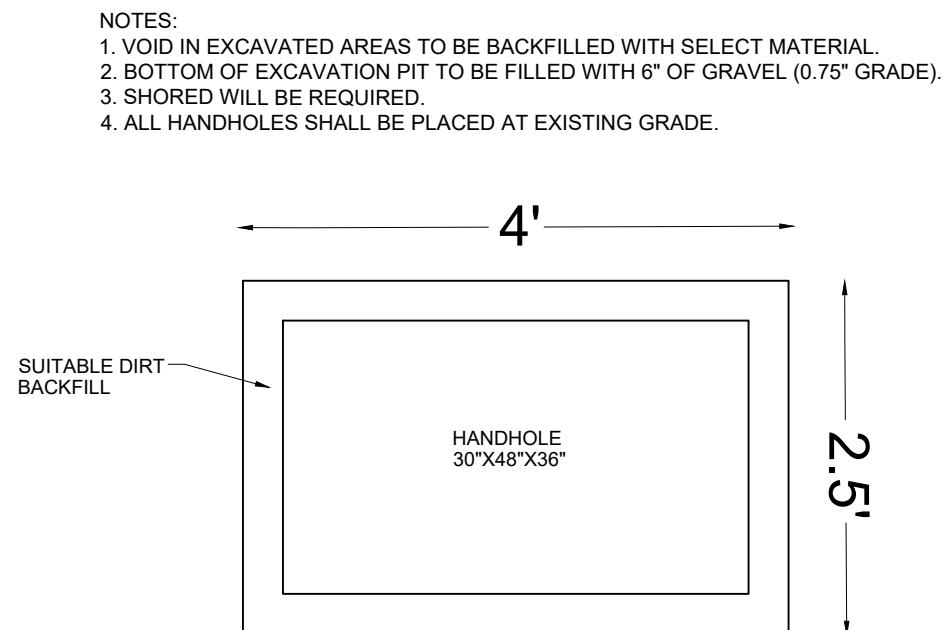
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QUAZITE 30X48X36 BOX ASSEMBLY  
W/(2) 24" CR, (2) 7 1/2" CR, HOOKS  
& (4) Ø4 3/4 KNOCKOUT  
PG3048Z923



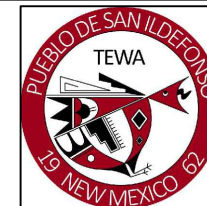
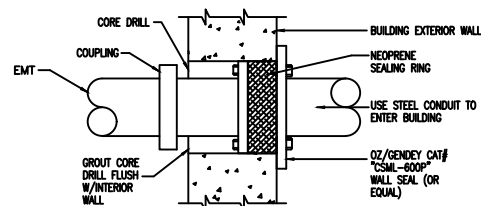
**TYPICAL DETAIL "2"**  
HANDHOLE CONSTRUCTION DETAIL.  
CONDUIT TO HANDHOLE PROFILE FOR RW INSTALLATION.



**TYPICAL DETAIL "3"**  
HANDHOLE EXCAVATION



**TYPICAL DETAIL "4"**  
CONDUIT SEAL



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Robert Paroski  
APPROVED BY:  
Bryan Schalipp  
DATE:  
12/03/2023

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03	5/24/24	REVISED

SHEET TITLE:  
CONSTRUCTION DETAILS SHEET

PROJECT NAME:  
SAN\_L\_NMDOT 4 PARALLEL

PROJECT LOCATION:  
4 ELK TRAIL RD SANTA FE, NM

PAGE  
6 OF 29



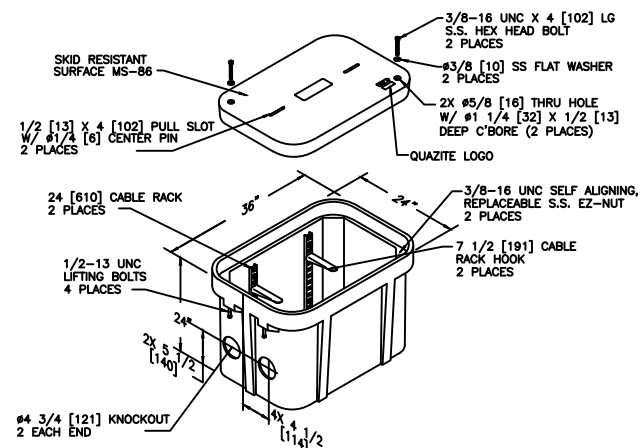
Know what's below.  
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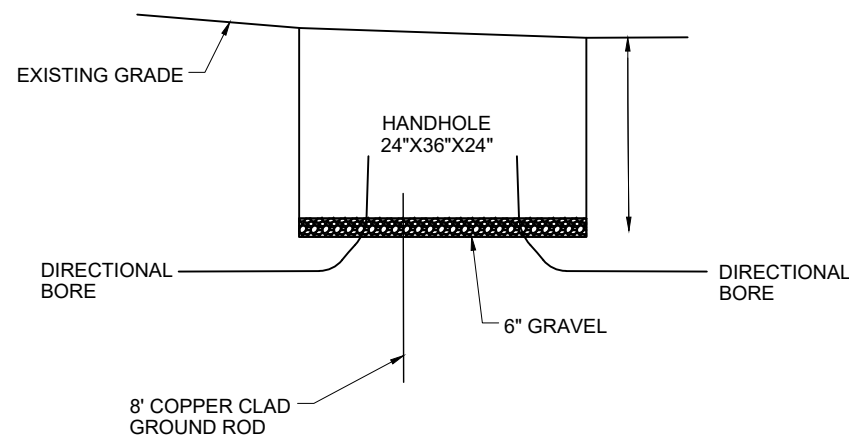
# BURIED CABLE CONSTRUCTION DETAILS

## HANDHOLE SIZE 24"x36"x24"

**TYPICAL DETAIL "1"**  
 QUAZITE 24"x36"x24" BOX ASSEMBLY  
 W/(2) 24" CR, (2) 7 1/2" CR, HOOKS  
 & (4) Ø4 3/4 KNOCKOUT

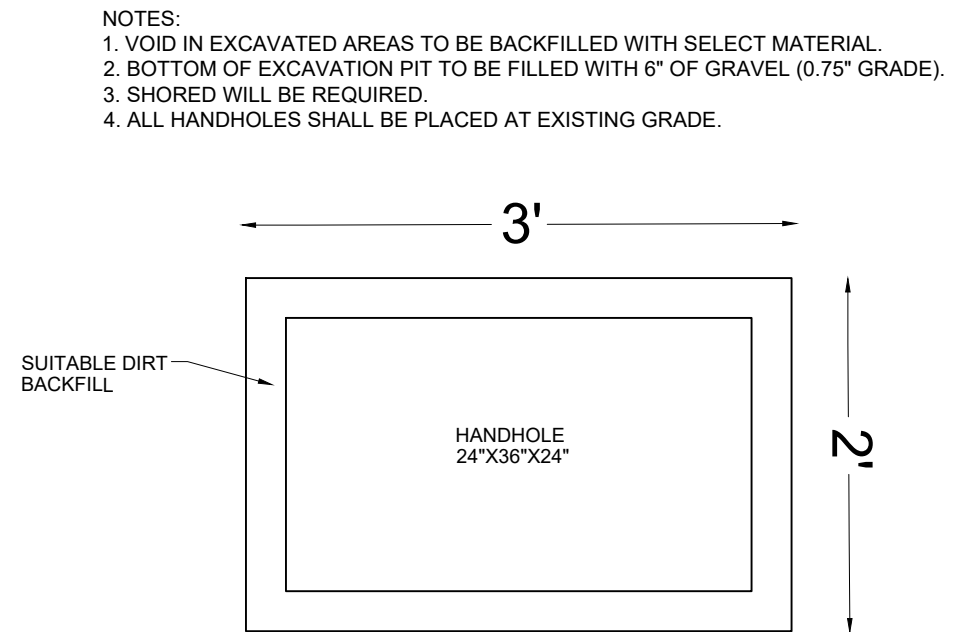


**TYPICAL DETAIL "2"**  
 HANDHOLE CONSTRUCTION DETAIL.  
 CONDUIT TO HANDHOLE PROFILE FOR R/W INSTALLATION.

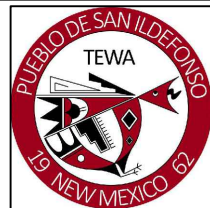
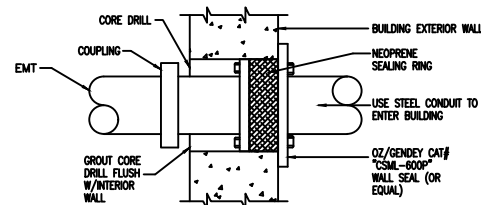


-ALL EXCAVATIONS OR TRENCHES (4) FOUR FEET OR GREATER IN DEPTH SHALL BE APPROPRIATELY BENCHED, SHORED, OR ACCORDING TO THE PROCEDURES AND REQUIREMENTS SET FORTH IN OSHA'S EXCAVATION STANDARD, 29 CFR 1926.650, 1926.651 AND 1926.652.

**TYPICAL DETAIL "3"**  
 HANDHOLE EXCAVATION



**TYPICAL DETAIL "4"**  
 CONDUIT SEAL



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 APPROVED BY:  
 Bryan Schalipp  
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 PROJECT LOCATION:  
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PAGE  
 7 OF 29



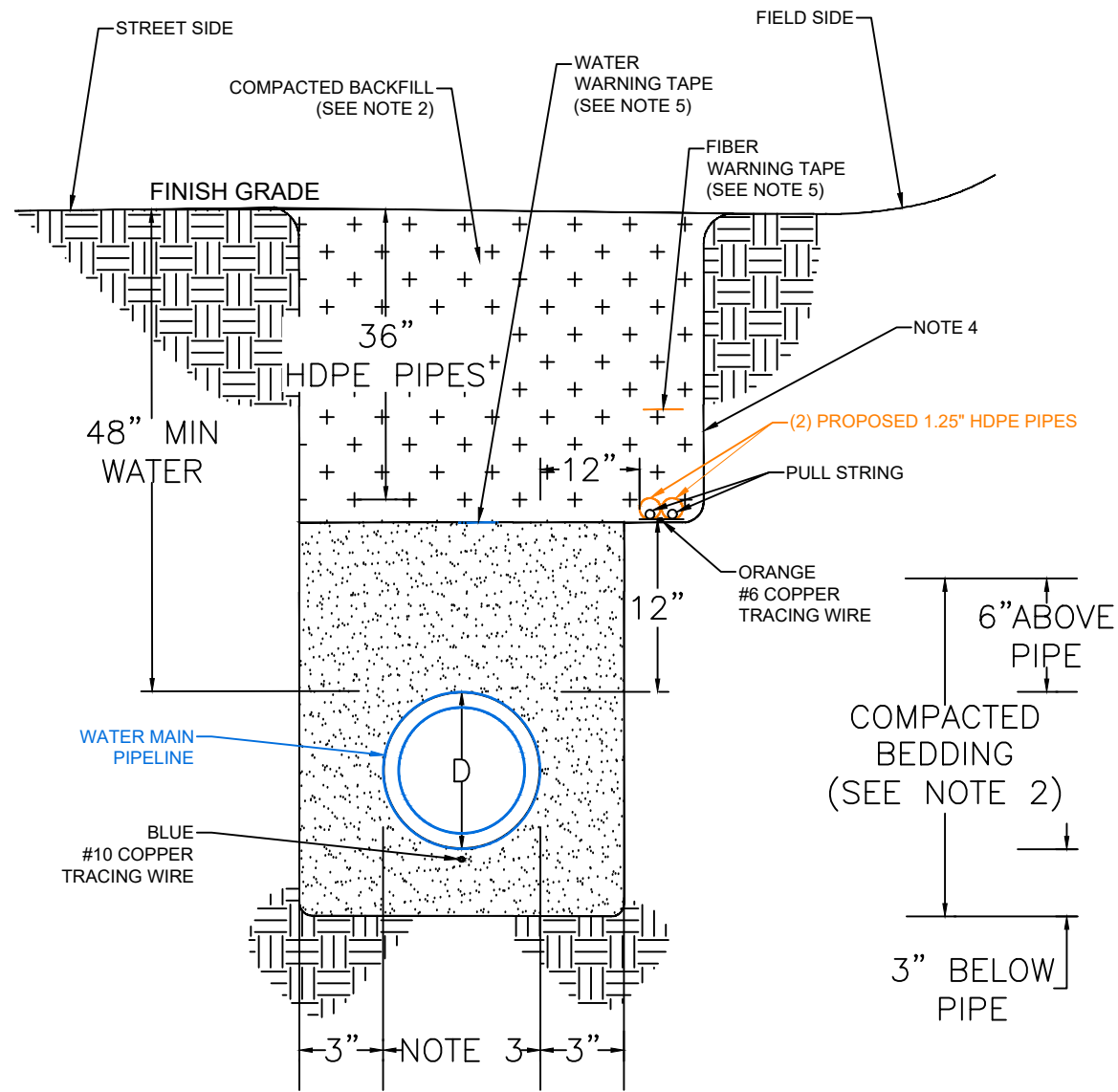
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**NOTES:**  
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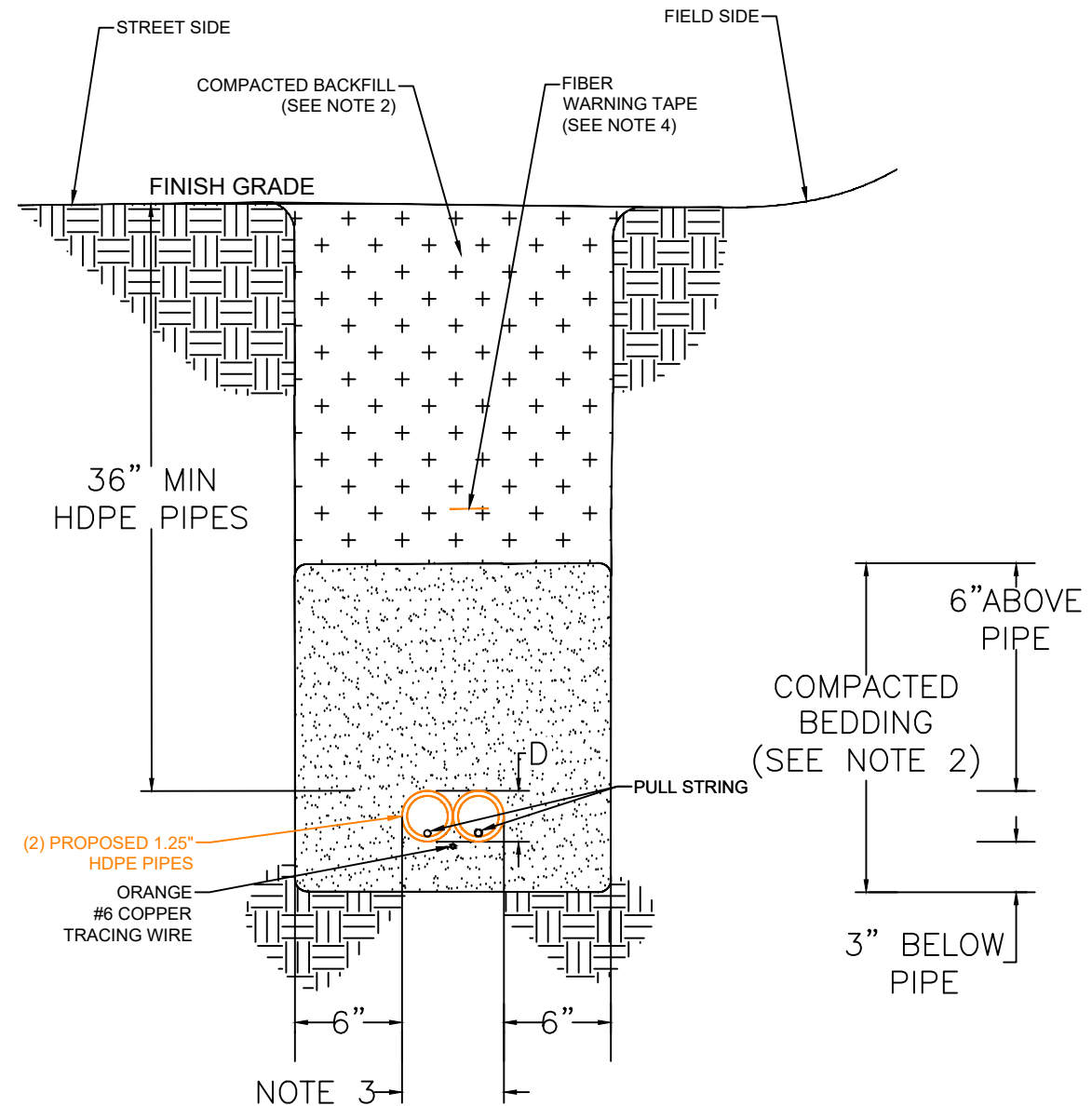
# BURIED CABLE CONSTRUCTION DETAILS

## TRENCH DETAILS



**WATER MAIN TRENCH DETAIL**  
NTS

- NOTES:**
- 1.) D, IS THE OUTSIDE PIPE DIAMETER IN INCHES.
  - 2.) SEE NMDOT COMPACTION SPECIFICATIONS - 95% WITHIN THE ROADWAY PRISM AND 90% ELSEWHERE.
  - 3.) SEE LOS ALAMOS COMPACTION STANDARDS 3.3 & 3.4 (SEE SHEET 8A OF THIS PLAN SET).
  - 4.) TRENCH WIDTH SHALL BE DISTANCE FROM OUTSIDE FACE OF OUTER MOST PIPE PLUS 6 INCHES.
  - 5.) EXTEND TRENCH 1' TO FIELD SIDE 36" MINIMUM DEPTH.
  - 6.) WATER AND FIBER WARNING TAPE SHALL BE TERRA TAPE.



**SAN I CONDUIT TRENCH DETAIL**  
NTS

- NOTES:**
- 1.) D, IS THE OUTSIDE PIPE DIAMETER IN INCHES.
  - 2.) SEE NMDOT COMPACTION SPECIFICATIONS - 95% WITHIN THE ROADWAY PRISM AND 90% ELSEWHERE.
  - 3.) SEE LOS ALAMOS COMPACTION STANDARDS 3.3 & 3.4 (SEE SHEET 8A OF THIS PLAN SET).
  - 4.) TRENCH WIDTH SHALL BE DISTANCE FROM OUTSIDE FACE OF OUTER MOST PIPE PLUS 6 INCHES.
  - 5.) FIBER WARNING TAPE SHALL BE TERRA TAPE.



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PAGE  
8 OF 29





**NOTES:**  
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# BURIED CABLE CONSTRUCTION DETAILS

## LOS ALAMOS COMPACTION STANDARDS

- A. Trenches may be excavated either by hand, or by machine. Trenches shall be cut with vertical sides, and shall be of sufficient width to provide adequate space for working therein. When applicable such space shall have adequate clear distance when shoring is used, so that pipe can be properly placed and aligned in conformity with the plans. Trench sides shall be parallel to and at equal distance from the center-line of the pipe, when aligned as shown on drawings.
- B. Pipe trenches shall be excavated to a depth below the bottom of the pipe sufficient to provide for pipe bedding materials as required by Section 2.2.
- C. Where a trench has been excavated below the designed grade, the bottom of the trench shall be refilled to proper subgrade with approved material well compacted in place, in an approved manner.
- D. No more than 150 feet of trench shall be opened at any one time unless approved by the Project Manager.
- E. If practical, no trench or holes shall be left open overnight. Use steel plating to protect open trenches overnight.
- F. Excavation for thrust blocks shall be neat to the line and dimensions shown or called for on the plans.
- G. Provide for dewatering trenches and excavations and subsequent control of ground water, utilizing such pumps or other equipment as may be necessary to control ground water and seepage until backfilling is completed.
- H. The contractor shall remove and legally dispose of all excess excavated material and demolition debris.

### 3.2 GENERAL BEDDING

- A. Utilities shall be laid on a firm layer of firm bedding material, per section 2.2 A, not less than four (4) inches in depth as shown or as noted on the plans and detail drawings. Compact as specified herein.
- B. Upon completion of bedding operations and, prior to the installation of pipe or appurtenances, notify the Project Manager who will then inspect the bedding layer. Pipe laying shall not commence until the bedding has been approved. Upon completion of placement of 6" of bedding above pipe or conduit notify the Project Manager who will then inspect.

### 3.3 GENERAL BACKFILLING

- A. Backfill shall be as shown on the plans. Place in 8-inch maximum lifts. Bring up evenly on each side, and for the full length of the structure. Ensure that no damage is done to structures or protective coatings thereon. Compact each loose lift as specified in Paragraph "General Compaction" before placing the next lift. Where unacceptable settlements occur in trenches and pits due to improper compaction, excavate to the depth necessary to rectify the problem, then backfill and compact the excavation as specified herein and restore the surface to the required elevation.
- B. No backfill shall be placed until the line has been inspected and bedding approved.

### 3.4 GENERAL COMPACTION

- A. Use hand-operated plate type vibratory or other suitable hand tampers in areas not accessible to larger rollers or compactors. Contractor shall avoid damaging structures, pipes and protective pipe coatings. Compaction shall be in accordance with the following unless otherwise specified. If necessary, the Contractor's selected equipment and construction procedure shall be altered, changed or modified in order to meet the specified compaction requirements.
- B. Initial bedding shall be carefully packed under the haunches of the pipe and brought up simultaneously on both sides so as to prevent any displacement of the pipe from its true alignment. Bedding shall be compacted in layers not more than eight (8) inches in thickness in a manner that will preclude moving the pipe, to not less than 85%, and 95% within road right of ways, and as specified.
- C. Backfill above the bedding shall be placed in loose lifts not exceeding eight (8) inches in thickness before compaction, and compacted by the use of pneumatic tampers or other mechanical means approved. Water or dry, as required, to bring the soils as close as practicable to the optimum moisture content for proper compaction. Compaction equipment or methods that produce horizontal or vertical earth pressures that may cause excessive displacement or may damage the pipeline will not be permitted.
- D. Backfill will be inspected during placement. Backfill not compacted in accordance with these specifications shall be recompacted, or removed as necessary and replaced to meet specified requirements prior to proceeding with the work.
- E. Contractor is responsible for protection and maintenance of work during construction and until the project is accepted. The contractor will not be paid an additional amount for such work.
- F. Open excavations and backfilled trenches that have not been paved shall be protected from moisture that may sacrifice compaction or backfill quality. Basecourse or asphalt shall not be placed on subgrade or backfill that is visibly saturated. Saturated subgrade and backfill shall be removed, replaced, recompacted per these specifications and demonstrated to be in conformance with these specifications by testing performed by an approved testing laboratory at the expense of the contractor. Frequency and location of this testing will be determined by the Project Manager.

### 3.5 GENERAL BRACING AND SHORING

- A. The Contractor shall furnish, place and maintain such bracing and shoring as may be required to support the sides of the excavations for the proper protection of workmen; to facilitate the work; and to prevent damage to or adversely affect adjacent structures, facilities, landscaping, or pavement.
- B. Upon completion of the work, all bracing and shoring shall be removed.

### 3.6 FIELD QUALITY CONTROL

- A. Compaction test are required to be performed by a qualified material testing Laboratory provided by the Contractor and at the expense of the Contractor, and test results shall be provided to the engineer directly from the laboratory.
- B. Definition of road prism in these specifications is all subsurface material directly below paving, sidewalk, curb, valley gutter, roadway islands, landscaping and bar ditches within a road right of way.



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APPROVED BY:  
Bryan Schalipp  
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PROJECT NAME:  
SAN\_LNMDOT 4 PARALLEL  
PROJECT LOCATION:  
4 ELK TRAIL RD SANTA FE, NM

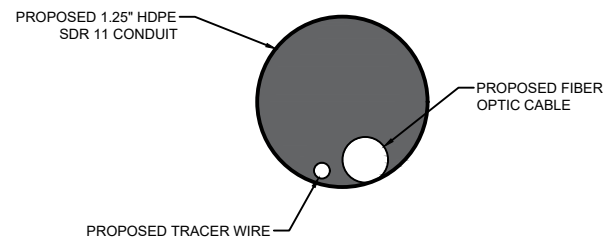
PAGE  
8A OF 29



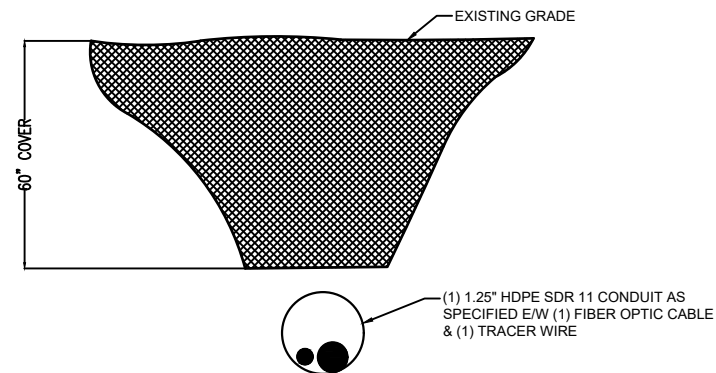
**NOTES:**  
MATERIALS SUPPLIED BY OTHERS.

# BURIED CABLE CONSTRUCTION DETAILS (CONTINUED)

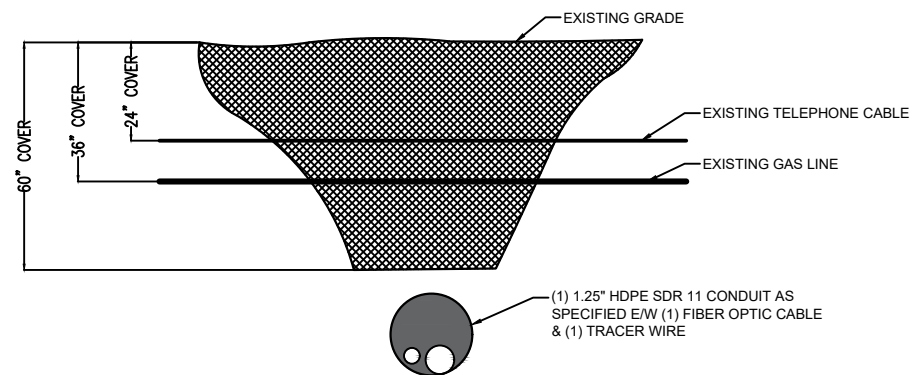
**TYPICAL DETAIL "A"**  
CROSS-SECTION OF PROPOSED HDPE



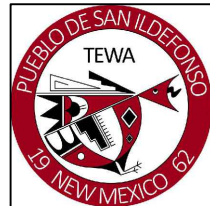
**TYPICAL DETAIL "B"**  
DIRECTIONAL BORE CROSS-SECTION FOR CONDUIT



**TYPICAL DETAIL "C"**  
DIRECTIONAL BORE CROSS-SECTION FOR CONDUIT PLACED BENEATH / PERPENDICULAR TO EXISTING CONDUIT



**TYPICAL DETAIL "D"**  
OMITTED



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PROJECT LOCATION:  
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PAGE  
9 OF 29



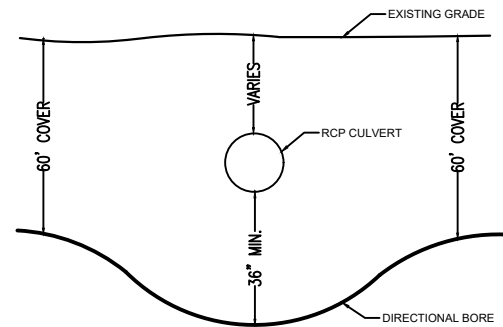
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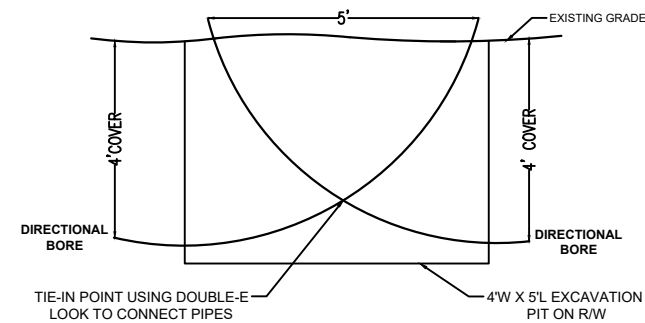
**NOTES:**  
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# BURIED CABLE CONSTRUCTION DETAILS (CONTINUED)

**TYPICAL DETAIL "E"  
CULVERT CROSSING DETAIL**



**TYPICAL DETAIL "F"  
DIRECTIONAL BORE TIE-IN DETAIL**

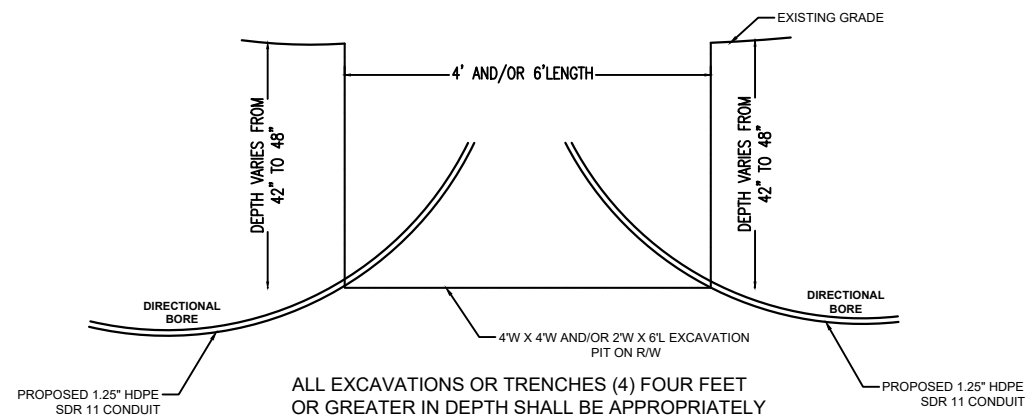


-BORE FROM EACH DIRECTION IS RUN AT DESIGN DEPTH TO SOME POINT PAST THE INTENDED TIE-IN, THEN TURN UP TO DAYLIGHT.

-TIE-IN POINT IS EXCAVATED, PIPE CUT OFF WHERE THEY CROSS EACH OTHER AT DESIGN DEPTH AND A HIGH PRESSURE COUPLER IS INSTALLED TO CONNECT THE TWO PIPES AT DESIGN DEPTH.

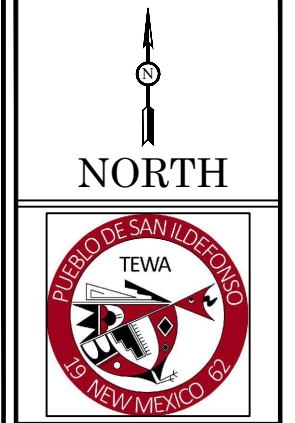
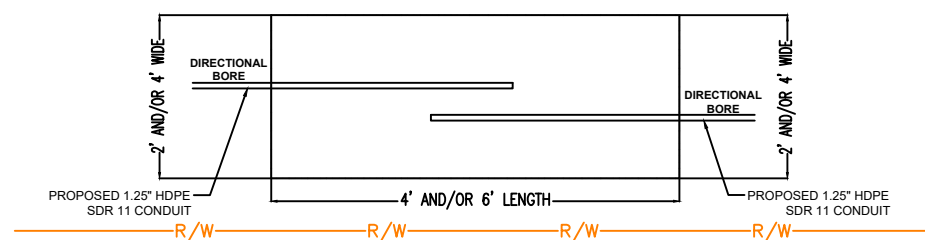
-ALL EXCAVATIONS OR TRENCHES (4) FOUR FEET OR GREATER IN DEPTH SHALL BE APPROPRIATELY BENCHED, SHORED, OR SLOPED ACCORDING TO THE PROCEDURES AND REQUIREMENTS SET FORTH IN OSHA'S EXCAVATION STANDARDS; 29 CFR 1926.650, 1926.651 AND 1926.652

**TYPICAL DETAIL "G"  
STANDARD BORE PIT DETAIL 1  
SIDE VIEW**



ALL EXCAVATIONS OR TRENCHES (4) FOUR FEET OR GREATER IN DEPTH SHALL BE APPROPRIATELY BENCHED, SHORED, OR SLOPED ACCORDING TO THE PROCEDURES AND REQUIREMENTS SET FORTH IN OSHA'S EXCAVATION STANDARDS; 29 CFR 1926.650, 1926.651 AND 1926.652

**TYPICAL DETAIL "H"  
STANDARD BORE PIT DETAIL 2  
TOP VIEW**



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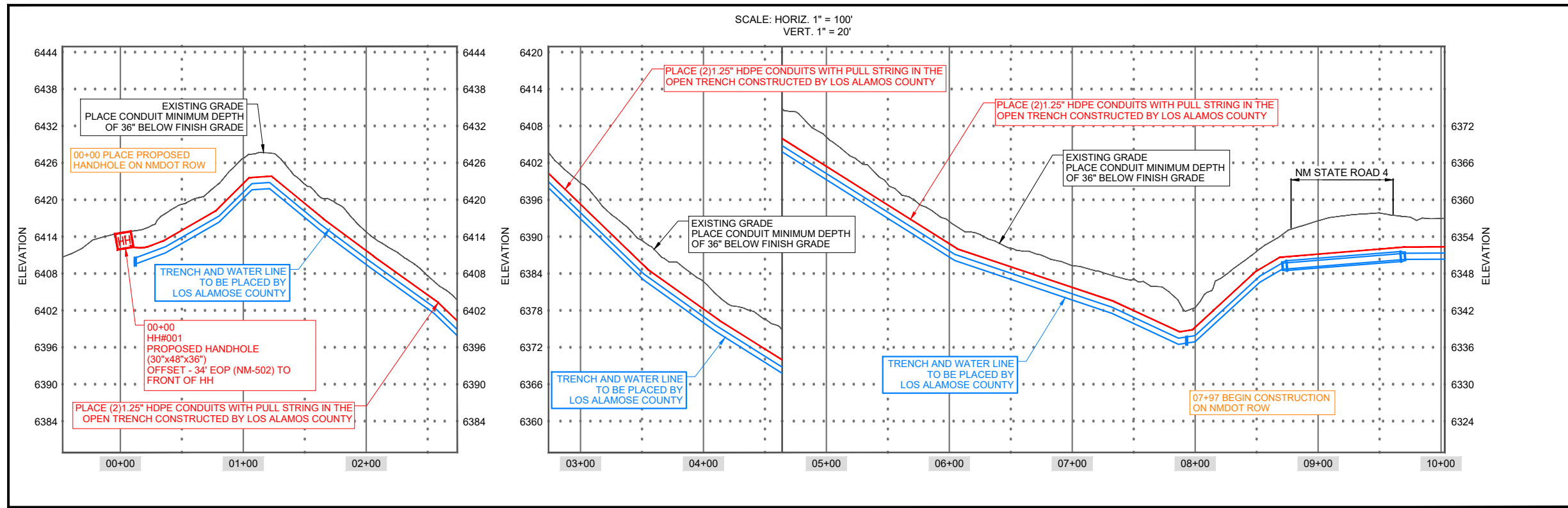
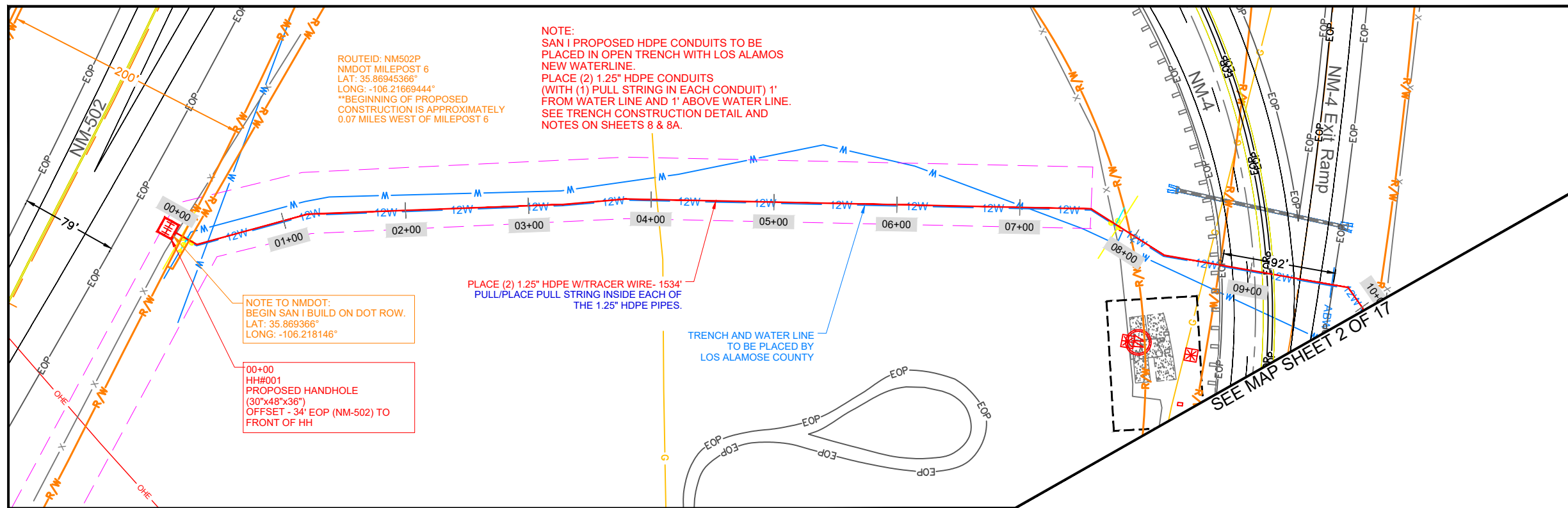
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PAGE  
10 OF 29



# MAP SHEET 1

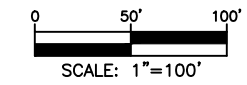


**\*\*CONSTRUCTION NOTE: PLEASE MARK UP ALL UTILITY LINES CROSSED DURING CONSTRUCTION**

Construction Start date:  
 Construction End date:

Pull Fiber Start date:  
 Pull Fiber End date:

Splice/Test Est Start Date:  
 Splice/Test Actual Date:



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PROJECT NAME:  
SAN I\_NMDOT 4 PARALLEL

PROJECT LOCATION:  
4 ELK TRAIL RD SANTA FE, NM

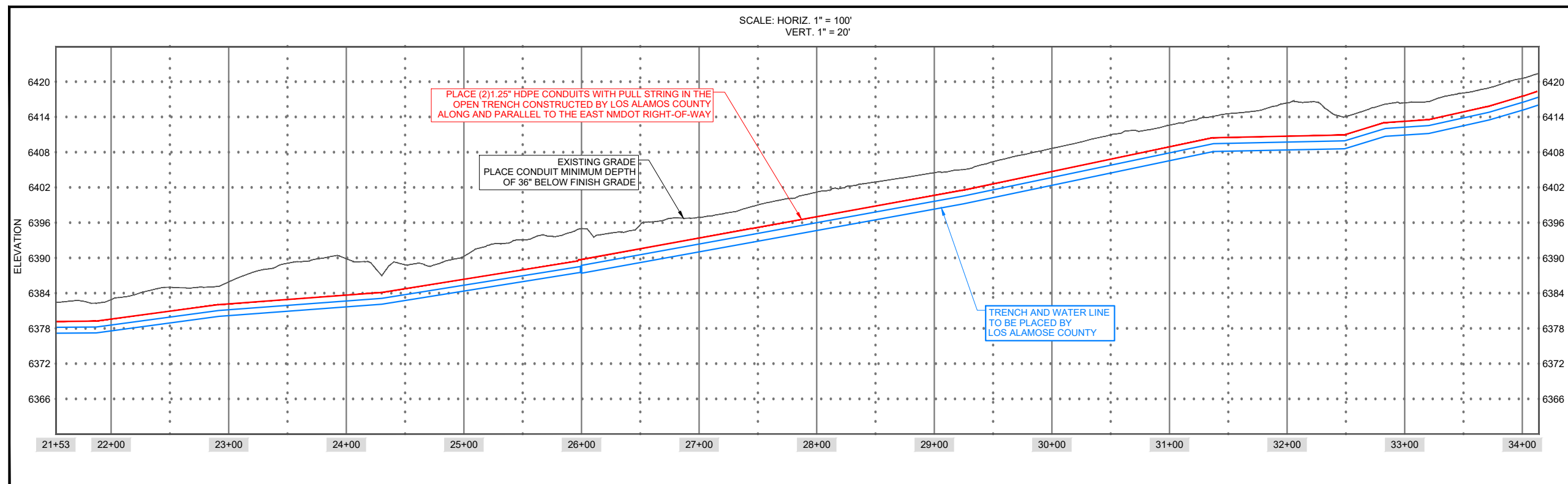
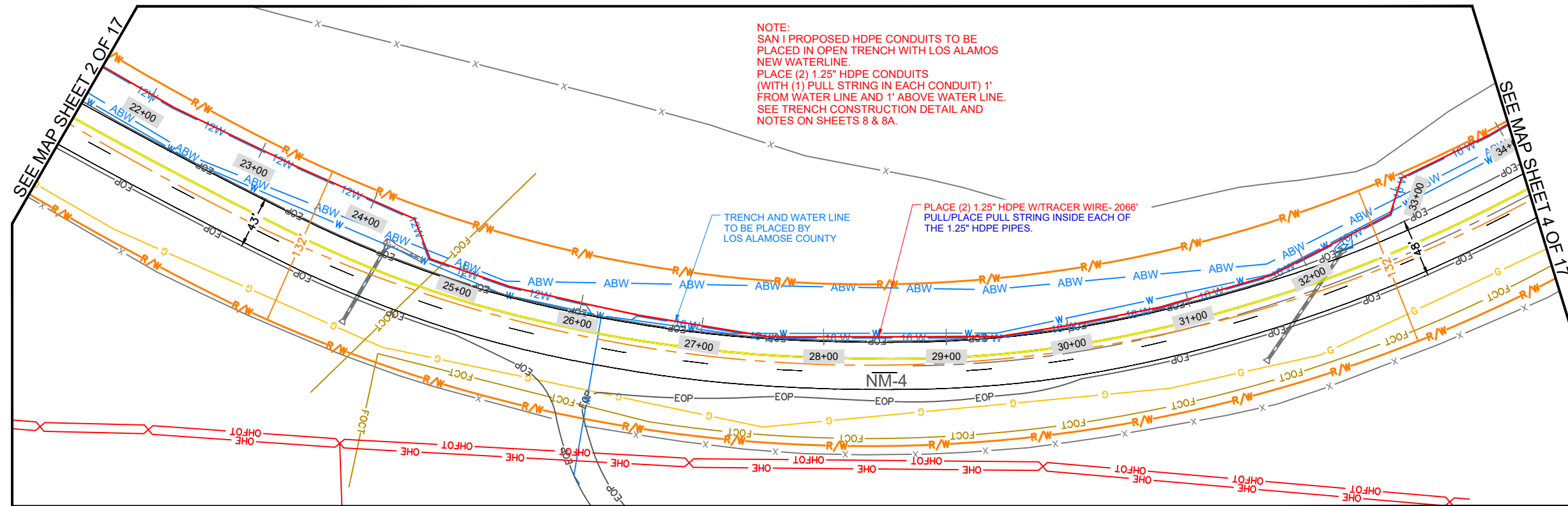
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# MAP SHEET 3

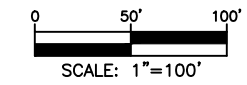


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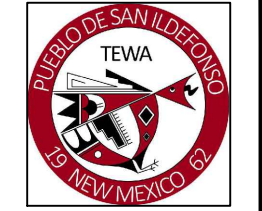
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Construction End date:

Pull Fiber Start date:  
Pull Fiber End date:

Splice/Test Est Start Date:  
Splice/Test Actual Date:



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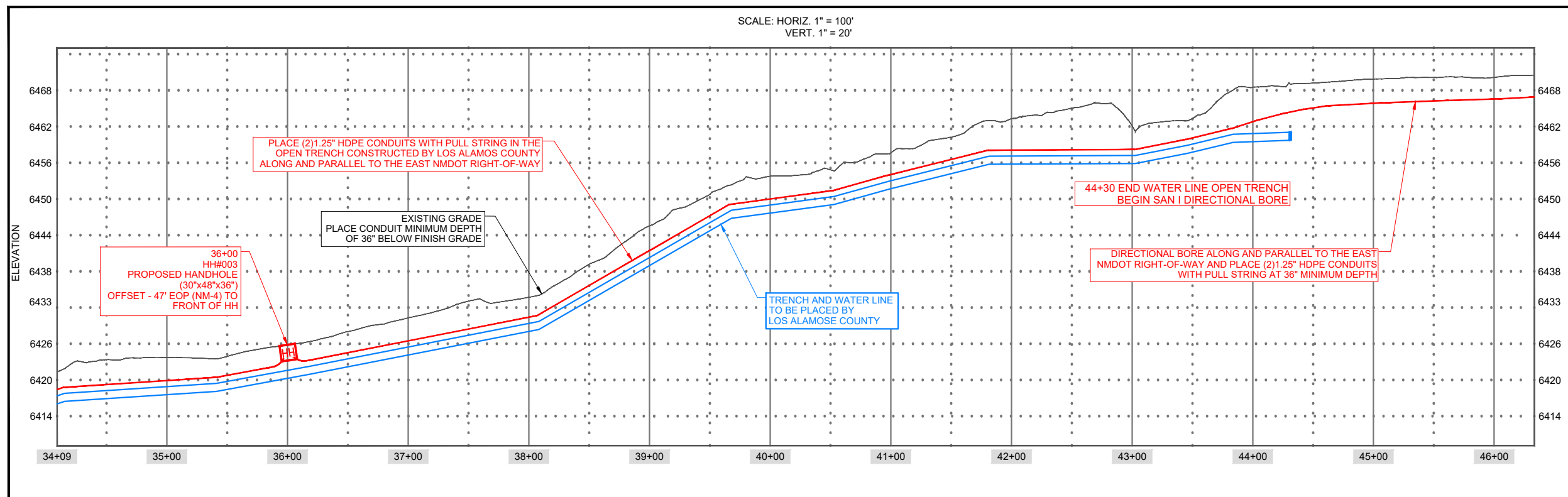
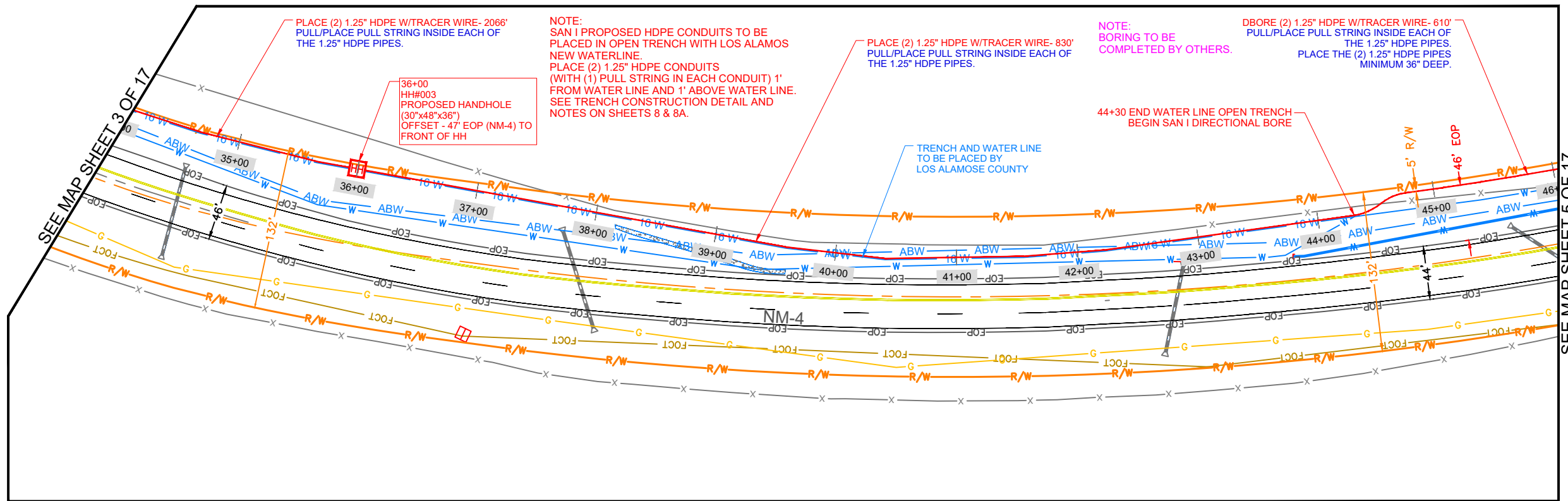
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PROJECT NAME:  
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PROJECT LOCATION:  
**4 ELK TRAIL RD SANTA FE, NM**

PAGE  
**13** OF **29**





# MAP SHEET 4



**\*\*CONSTRUCTION NOTE: PLEASE MARK UP ALL UTILITY LINES CROSSED DURING CONSTRUCTION**

Construction Start date:  
Construction End date:

Pull Fiber Start date:  
Pull Fiber End date:

Splice/Test Est Start Date:  
Splice/Test Actual Date:



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03	5/24/24	REVISED

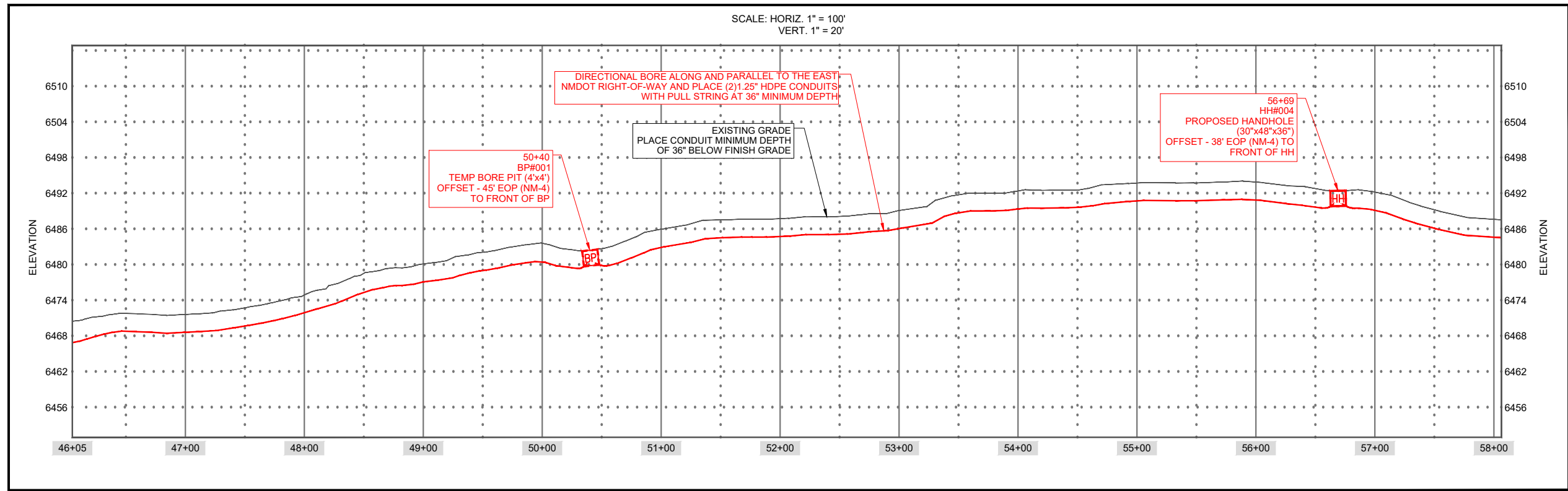
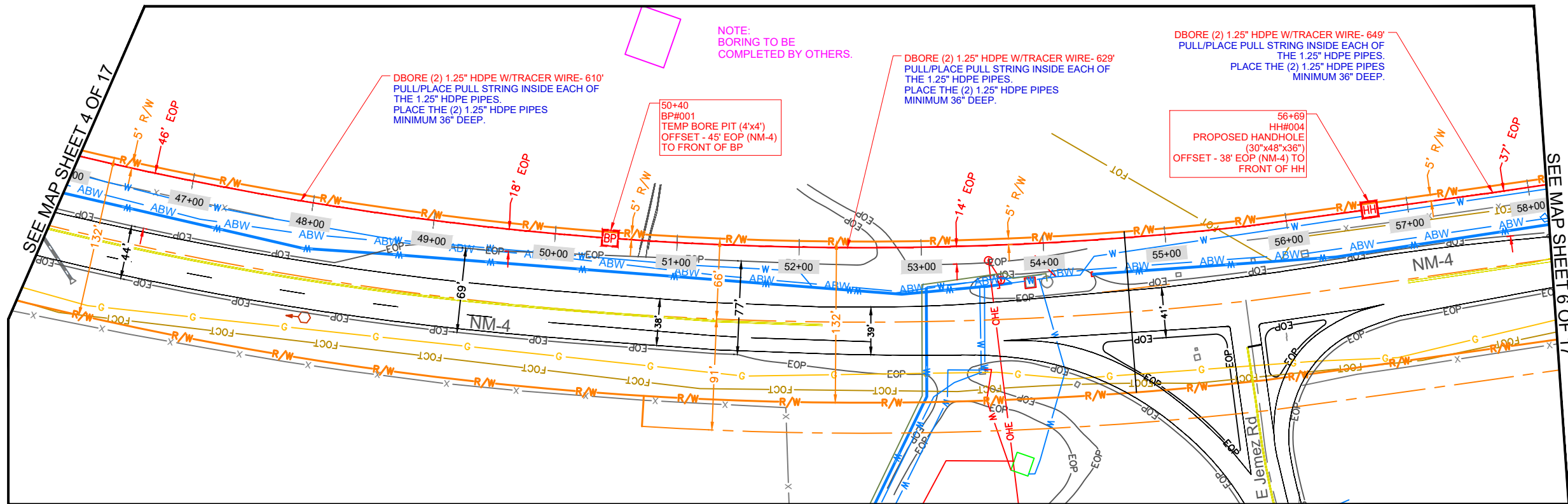
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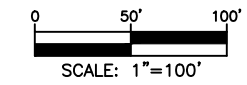


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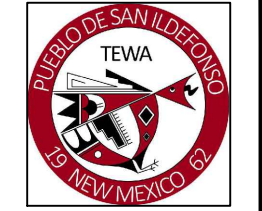
Construction Start date:  
Construction End date:

Pull Fiber Start date:  
Pull Fiber End date:

Splice/Test Est Start Date:  
Splice/Test Actual Date:



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APPROVED BY:  
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**MAP SHEET 5**

PROJECT NAME:  
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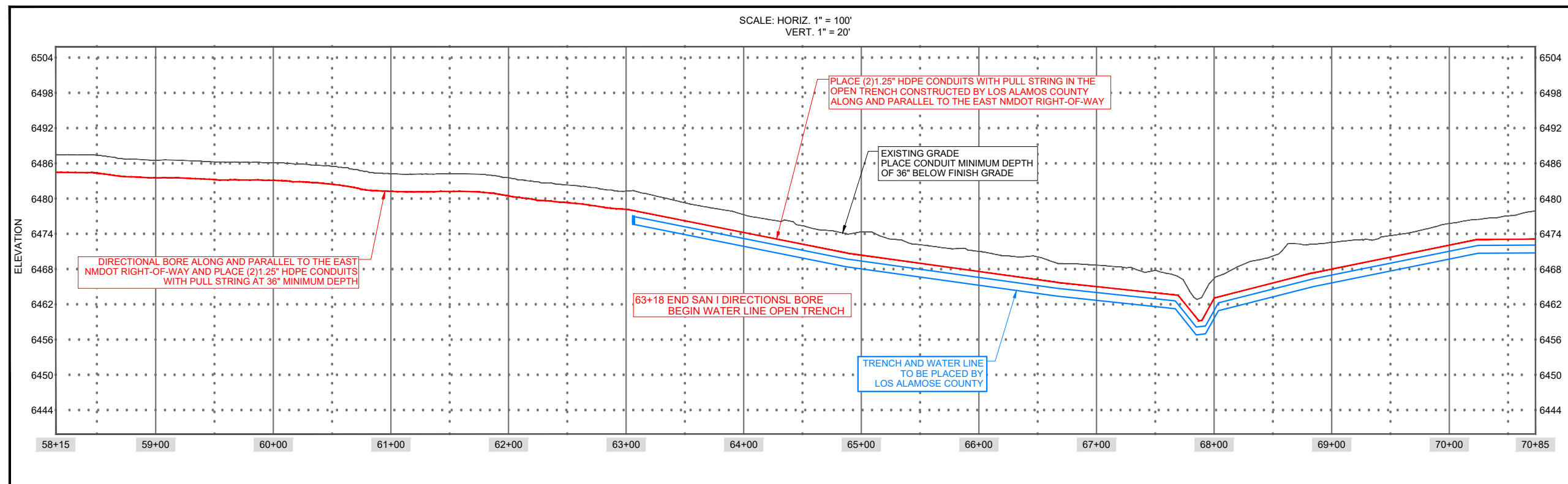
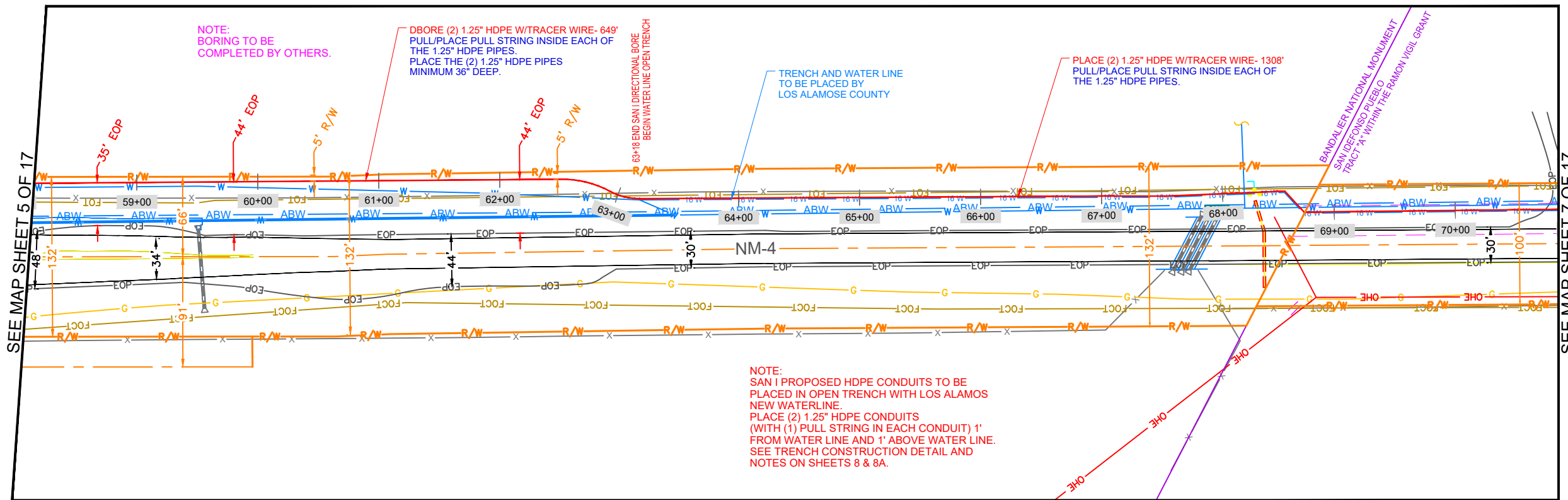
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15 OF 29





# MAP SHEET 6

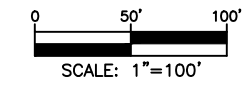


**\*\*CONSTRUCTION NOTE: PLEASE MARK UP ALL UTILITY LINES CROSSED DURING CONSTRUCTION**

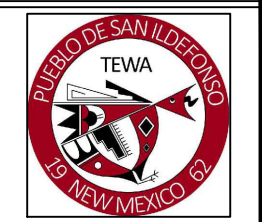
Construction Start date:  
Construction End date:

Pull Fiber Start date:  
Pull Fiber End date:

Splice/Test Est Start Date:  
Splice/Test Actual Date:



SCALE IS FOR REFERENCE PURPOSES ONLY.



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**Robert Paroski**  
APPROVED BY:  
**Bryan Schalipp**  
DATE:  
12/03/2023

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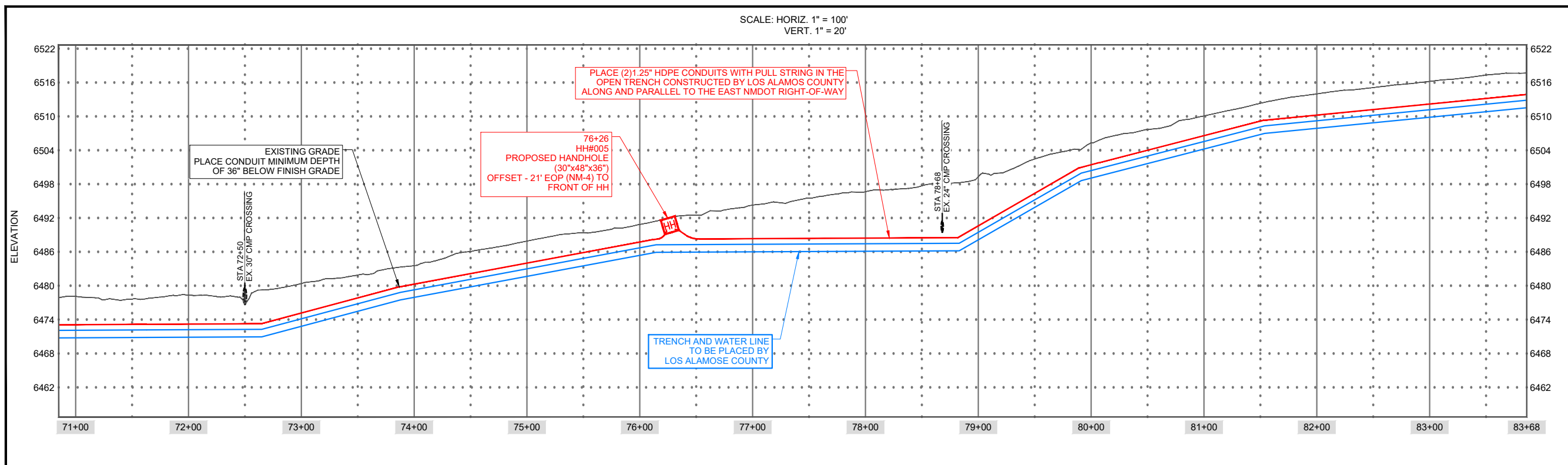
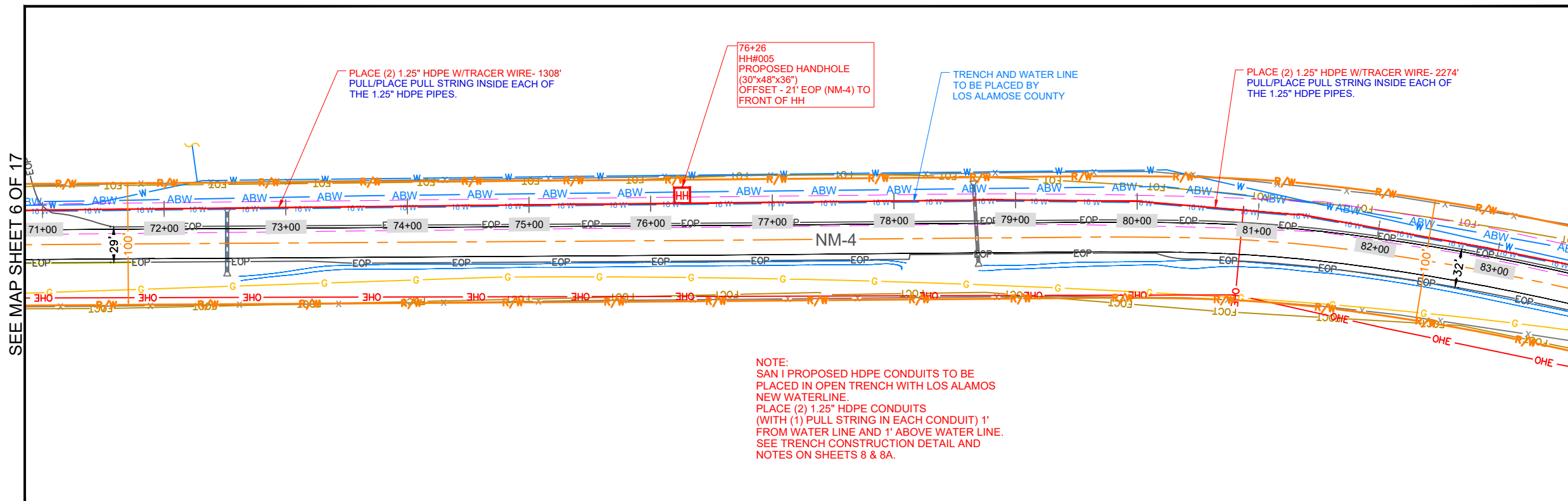
REV NO.	DATE	DESCRIPTION
01	1/18/24	REVISED
02	2/29/24	REVISED
03	5/24/24	REVISED

SHEET TITLE:  
**MAP SHEET 6**  
PROJECT NAME:  
**SAN I\_NMDOT 4 PARALLEL**  
PROJECT LOCATION:  
**4 ELK TRAIL RD SANTA FE, NM**

PAGE  
16 OF 29



# MAP SHEET 7



**\*\*CONSTRUCTION NOTE: PLEASE MARK UP ALL UTILITY LINES CROSSED DURING CONSTRUCTION**

Construction Start date:  
Construction End date:

Pull Fiber Start date:  
Pull Fiber End date:

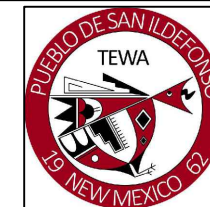
Splice/Test Est Start Date:  
Splice/Test Actual Date:



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Bryan Schalipp

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REV NO.	DATE	DESCRIPTION
01	1/18/24	REVISED
02	2/29/24	REVISED
03	5/24/24	REVISED

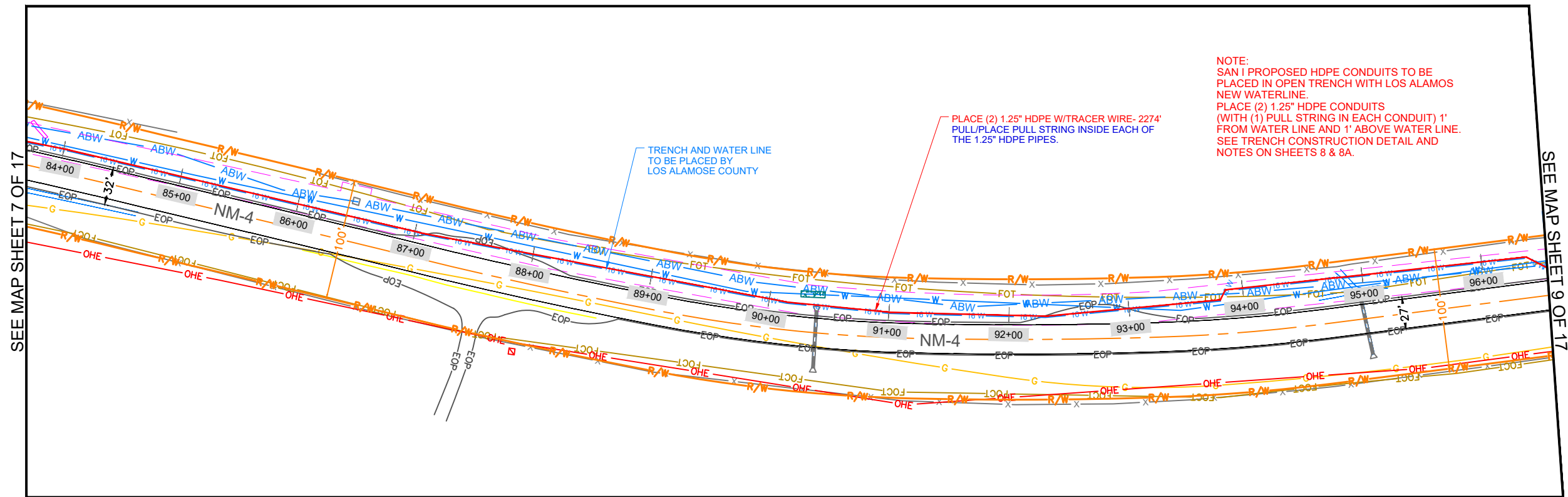
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MAP SHEET 7  
PROJECT NAME:  
SAN I\_NMDOT 4 PARALLEL  
PROJECT LOCATION:  
4 ELK TRAIL RD SANTA FE, NM

PAGE  
17 OF 29

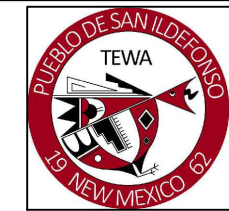


Know what's below.  
Call before you dig.

# MAP SHEET 8



NORTH



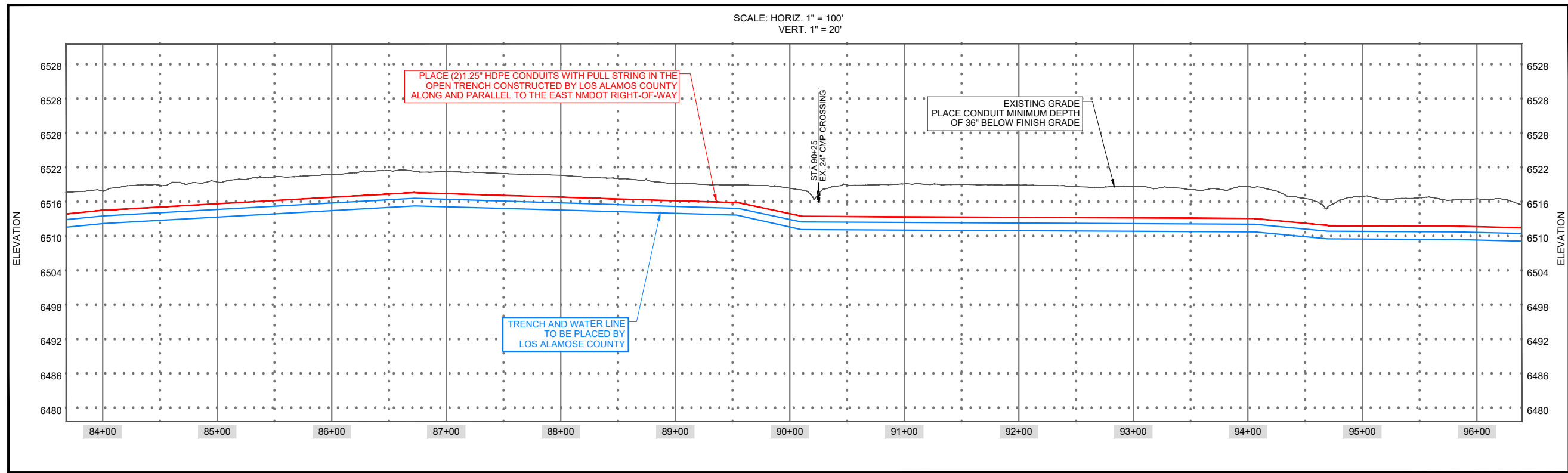
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REV NO.	DATE	DESCRIPTION
01	1/18/24	REVISED
02	2/29/24	REVISED
03	5/24/24	REVISED

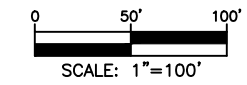


**\*\*CONSTRUCTION NOTE: PLEASE MARK UP ALL UTILITY LINES CROSSED DURING CONSTRUCTION**

Construction Start date:  
Construction End date:

Pull Fiber Start date:  
Pull Fiber End date:

Splice/Test Est Start Date:  
Splice/Test Actual Date:



SCALE IS FOR REFERENCE PURPOSES ONLY.

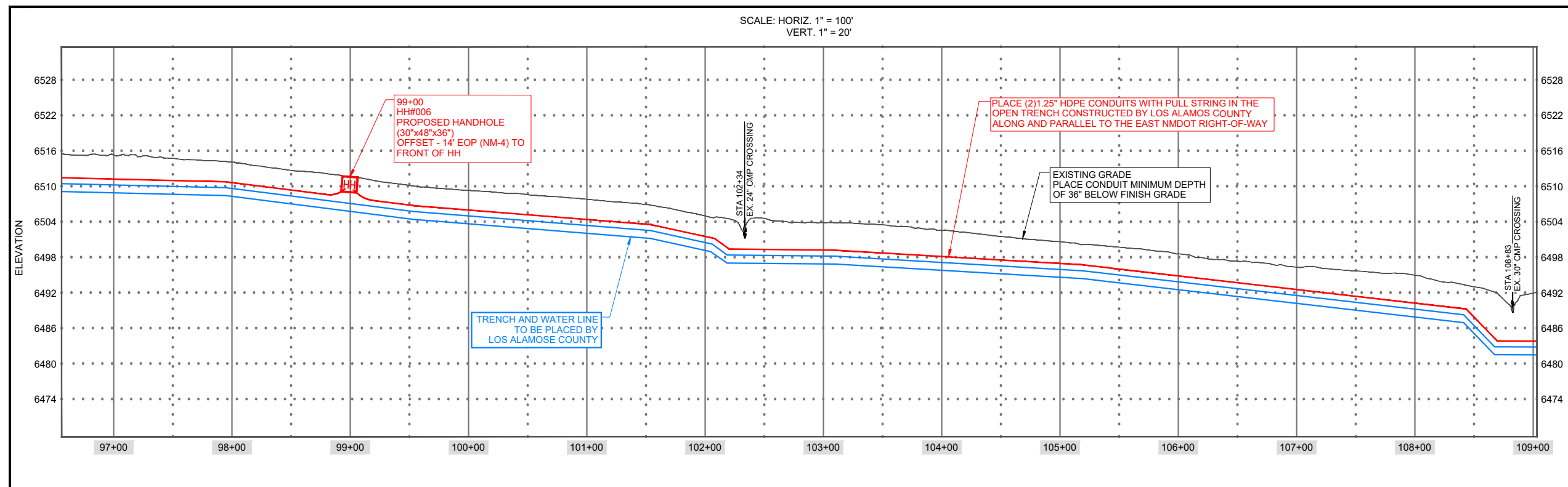
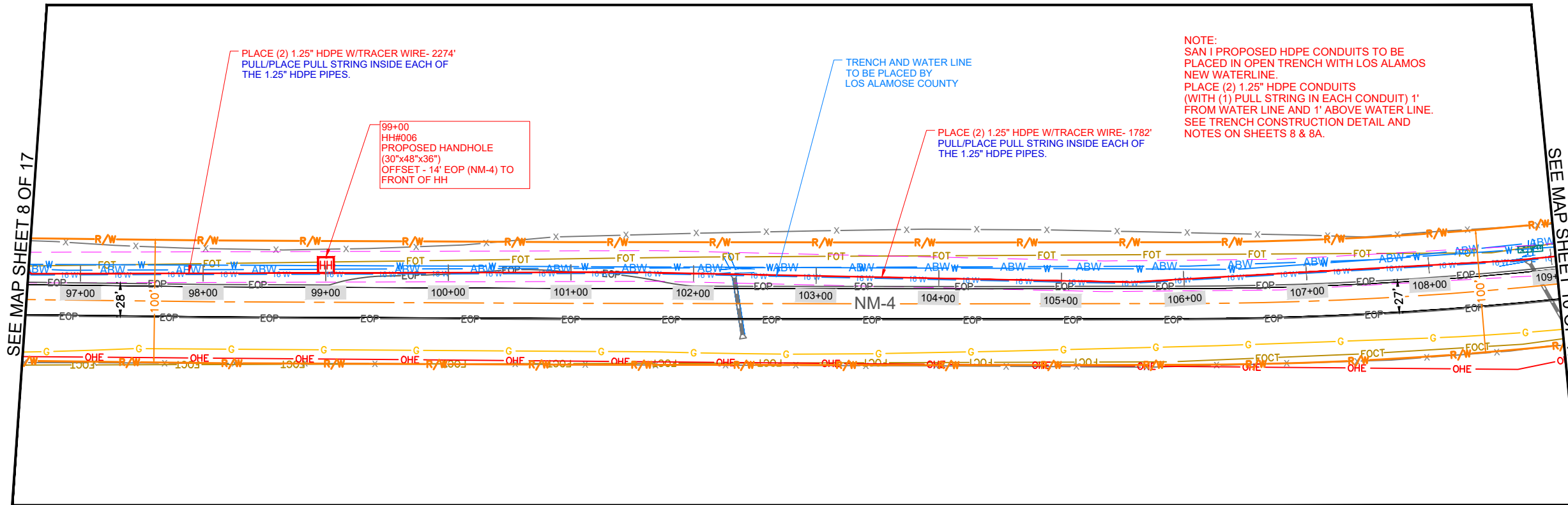
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MAP SHEET 8  
 PROJECT NAME:  
SAN\_L\_NMDOT 4 PARALLEL  
 PROJECT LOCATION:  
4 ELK TRAIL RD SANTA FE, NM

PAGE  
18 OF 29





# MAP SHEET 9

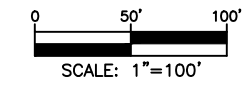


**\*\*CONSTRUCTION NOTE: PLEASE MARK UP ALL UTILITY LINES CROSSED DURING CONSTRUCTION**

Construction Start date:  
Construction End date:

Pull Fiber Start date:  
Pull Fiber End date:

Splice/Test Est Start Date:  
Splice/Test Actual Date:



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PUEBLO DE SAN ILDEFONSO  
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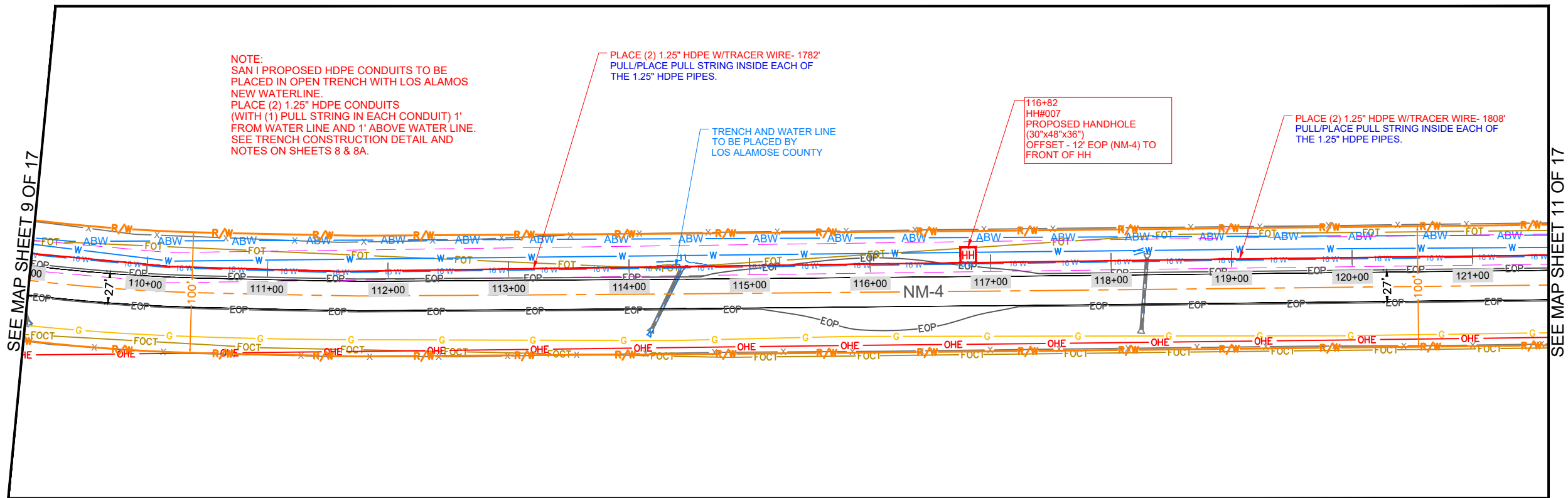
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02	2/29/24	REVISED
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SHEET TITLE:  
MAP SHEET 9  
PROJECT NAME:  
SAN I NMDOT 4 PARALLEL  
PROJECT LOCATION:  
4 ELK TRAIL RD SANTA FE, NM

PAGE  
19 OF 29

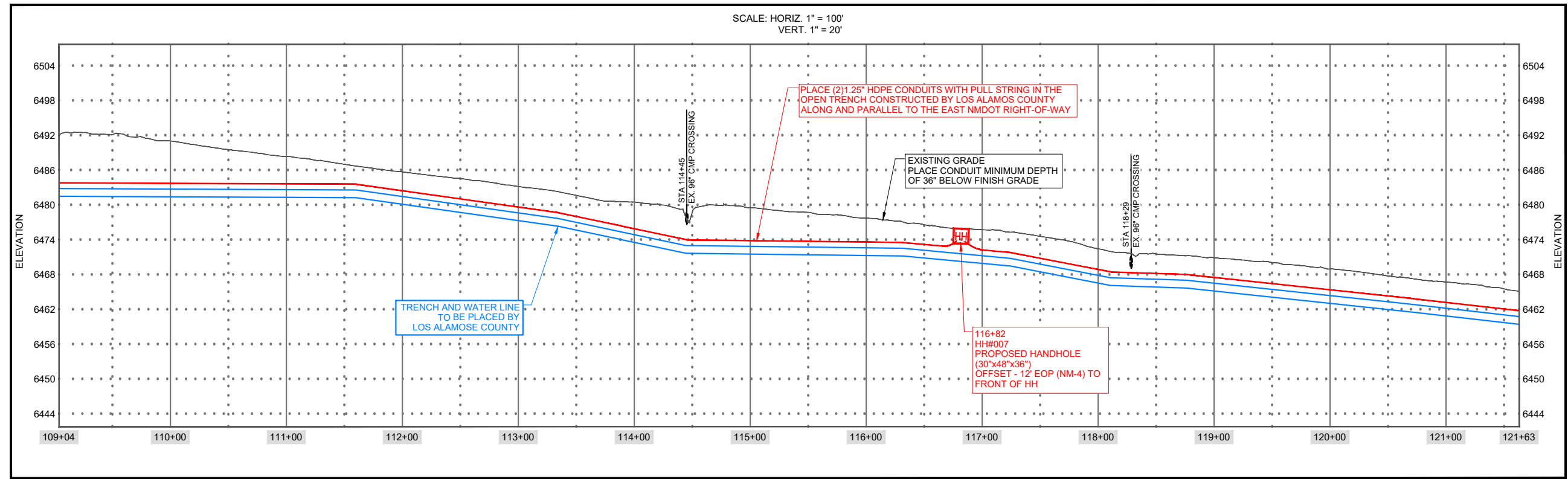
811  
Know what's below.  
Call before you dig.

# MAP SHEET 10



SEE MAP SHEET 9 OF 17

SEE MAP SHEET 11 OF 17

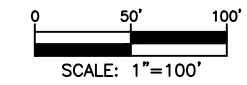


**\*\*CONSTRUCTION NOTE: PLEASE MARK UP ALL UTILITY LINES CROSSED DURING CONSTRUCTION**

Construction Start date:  
 Construction End date:

Pull Fiber Start date:  
 Pull Fiber End date:

Splice/Test Est Start Date:  
 Splice/Test Actual Date:



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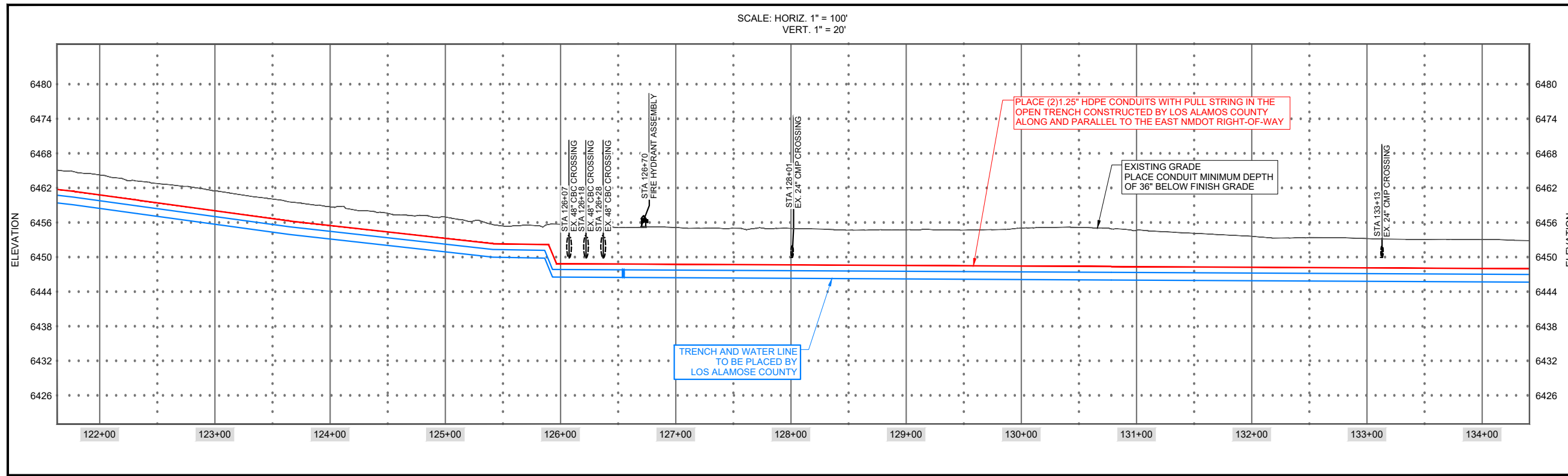
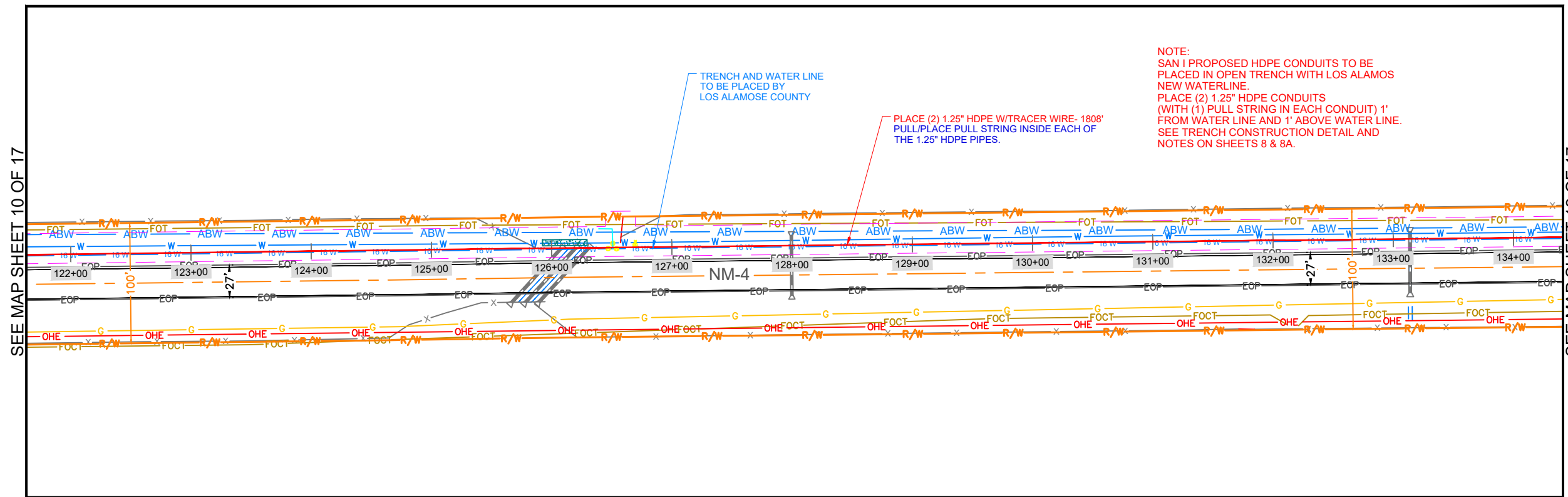
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SHEET TITLE:  
 MAP SHEET 10  
 PROJECT NAME:  
 SAN\_L\_NMDOT 4 PARALLEL  
 PROJECT LOCATION:  
 4 ELK TRAIL RD SANTA FE, NM

PAGE  
 20 OF 29

# MAP SHEET 11

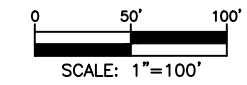


**\*\*CONSTRUCTION NOTE: PLEASE MARK UP ALL UTILITY LINES CROSSED DURING CONSTRUCTION**

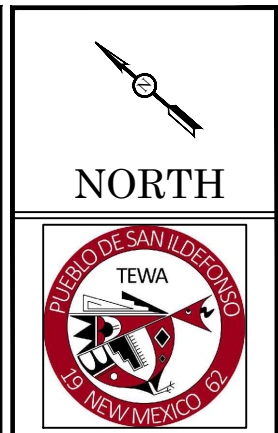
Construction Start date:  
 Construction End date:

Pull Fiber Start date:  
 Pull Fiber End date:

Splice/Test Est Start Date:  
 Splice/Test Actual Date:



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REV NO.	DATE	DESCRIPTION
01	1/18/24	REVISED
02	2/29/24	REVISED
03	5/24/24	REVISED

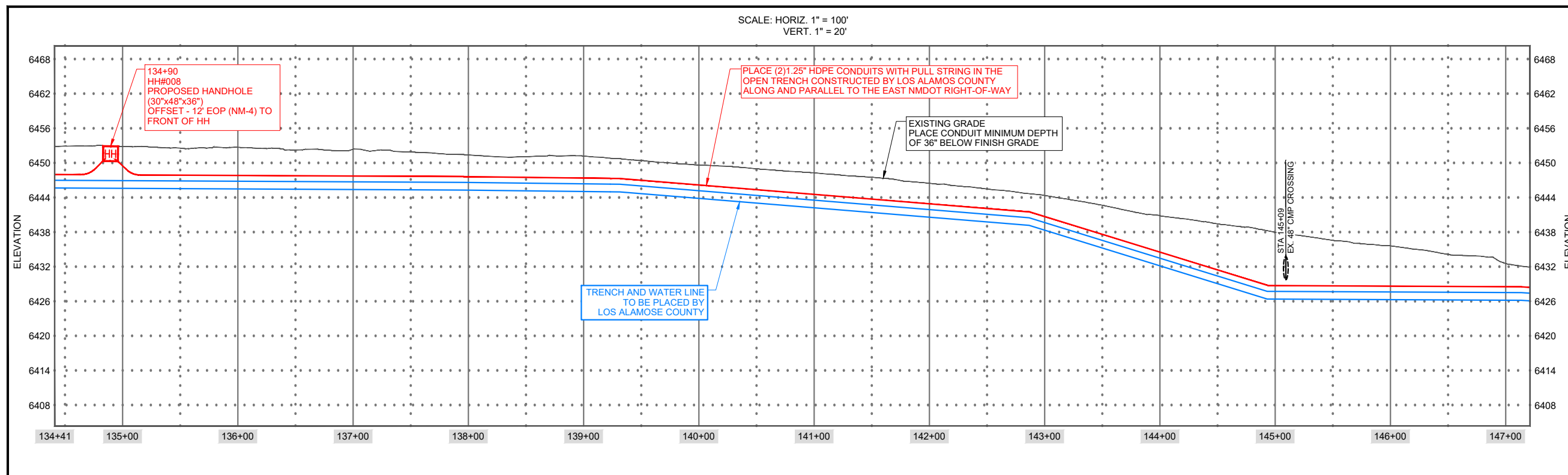
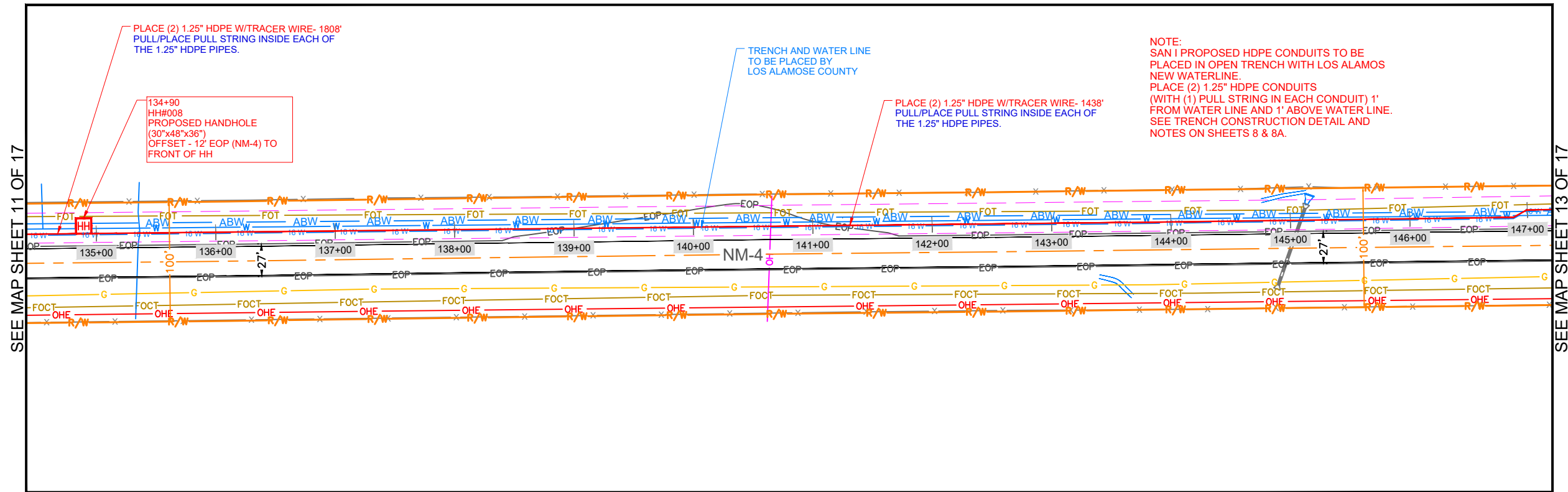
SHEET TITLE:  
 MAP SHEET 11  
 PROJECT NAME:  
 SAN I\_NMDOT 4 PARALLEL  
 PROJECT LOCATION:  
 4 ELK TRAIL RD SANTA FE, NM

PAGE  
 21 OF 29





# MAP SHEET 12



**\*\*CONSTRUCTION NOTE: PLEASE MARK UP ALL UTILITY LINES CROSSED DURING CONSTRUCTION**

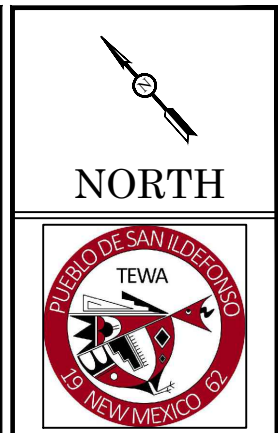
Construction Start date:  
Construction End date:

Pull Fiber Start date:  
Pull Fiber End date:

Splice/Test Est Start Date:  
Splice/Test Actual Date:



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REV NO.	DATE	DESCRIPTION
01	1/18/24	REVISED
02	2/29/24	REVISED
03	5/24/24	REVISED

SHEET TITLE:  
MAP SHEET 12

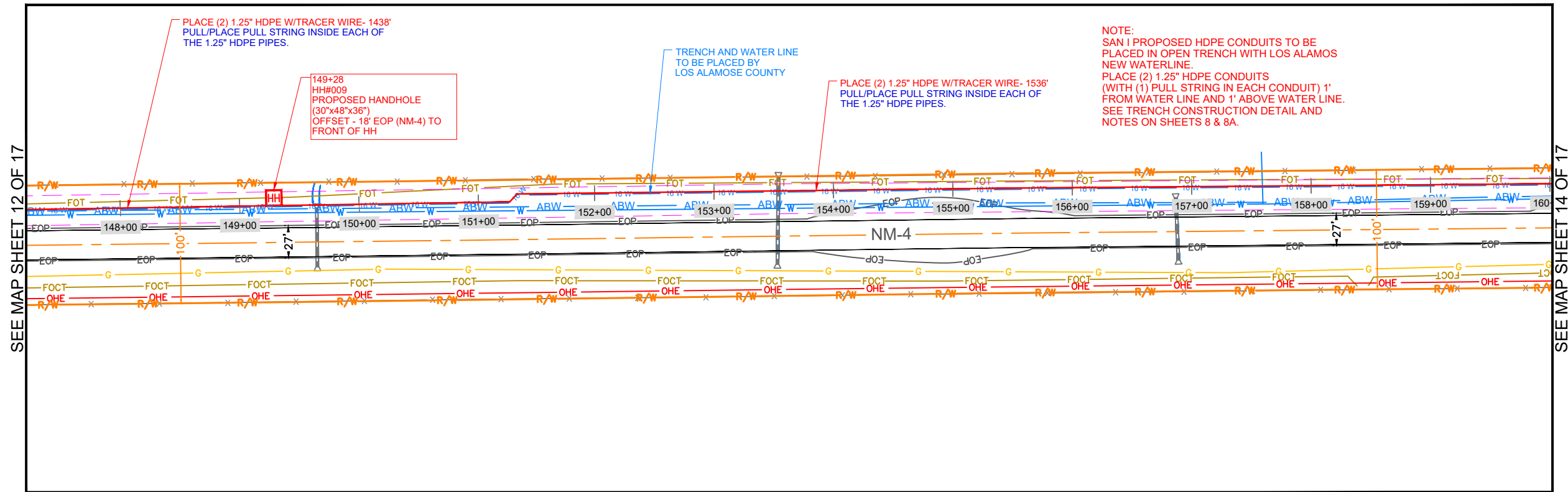
PROJECT NAME:  
SAN I\_NMDOT 4 PARALLEL

PROJECT LOCATION:  
4 ELK TRAIL RD SANTA FE, NM

PAGE  
22 OF 29

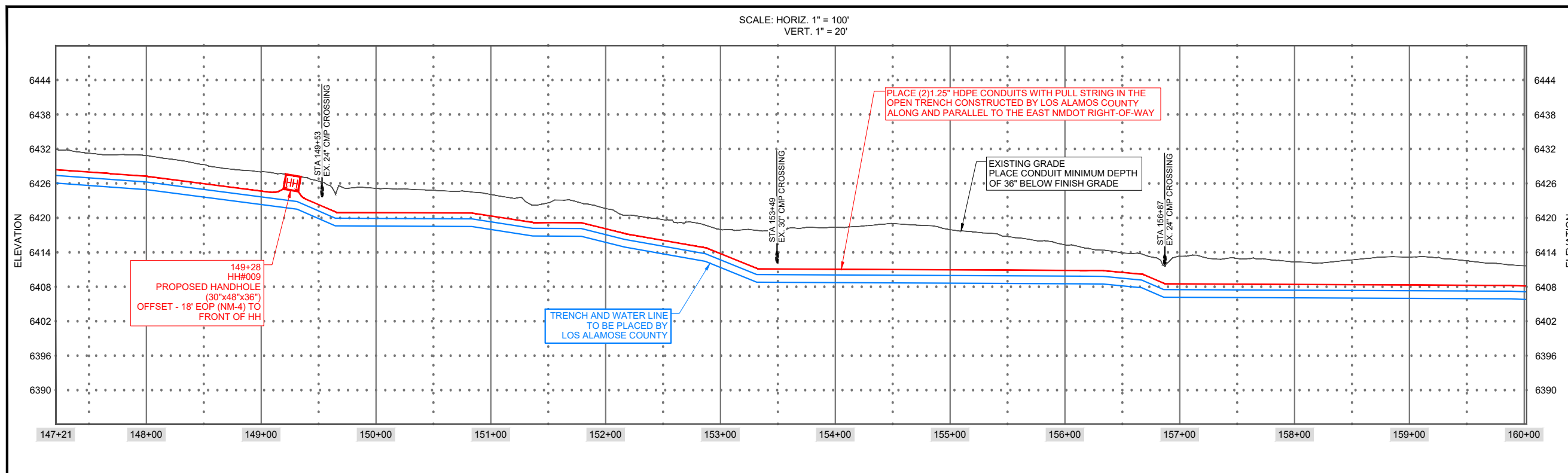


# MAP SHEET 13



SEE MAP SHEET 12 OF 17

SEE MAP SHEET 14 OF 17



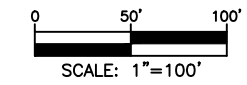
SCALE: HORIZ. 1" = 100'  
VERT. 1" = 20'

**\*\*CONSTRUCTION NOTE: PLEASE MARK UP ALL UTILITY LINES CROSSED DURING CONSTRUCTION**

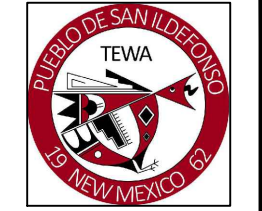
Construction Start date:  
Construction End date:

Pull Fiber Start date:  
Pull Fiber End date:

Splice/Test Est Start Date:  
Splice/Test Actual Date:



SCALE IS FOR REFERENCE PURPOSES ONLY.



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**Robert Paroski**  
APPROVED BY:  
**Bryan Schalipp**  
DATE:  
**12/03/2023**

DRAWING REVISIONS:

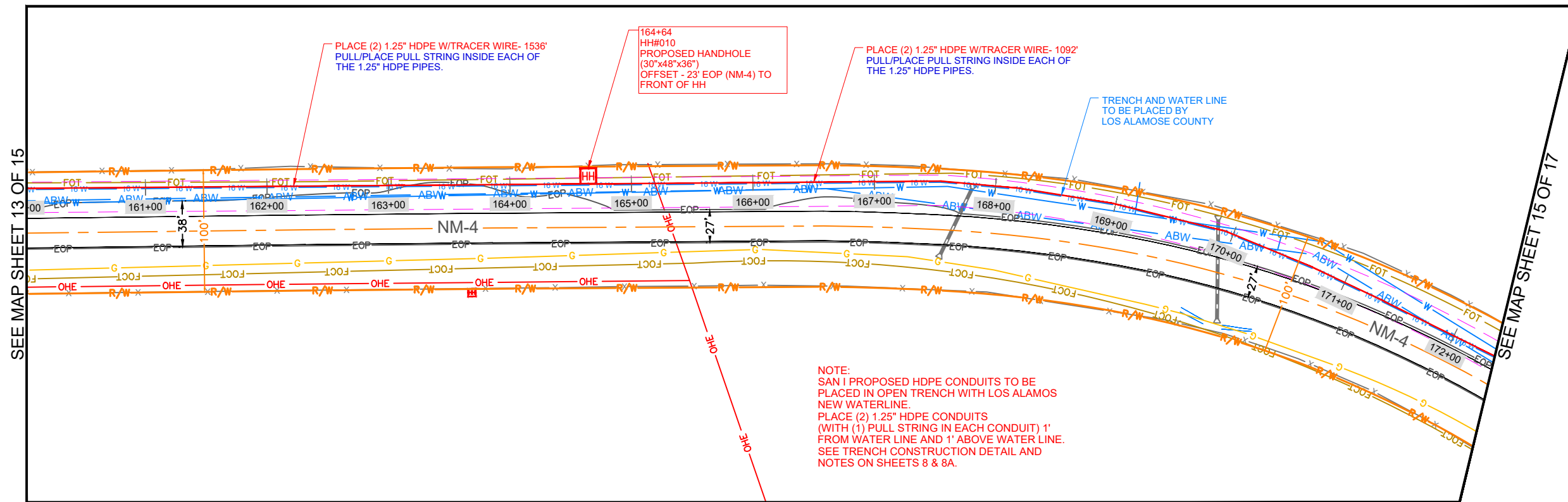
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02	2/29/24	REVISED
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SHEET TITLE:  
**MAP SHEET 13**  
PROJECT NAME:  
**SAN L\_NMDOT 4 PARALLEL**  
PROJECT LOCATION:  
**4 ELK TRAIL RD SANTA FE, NM**

PAGE  
**23** OF **29**



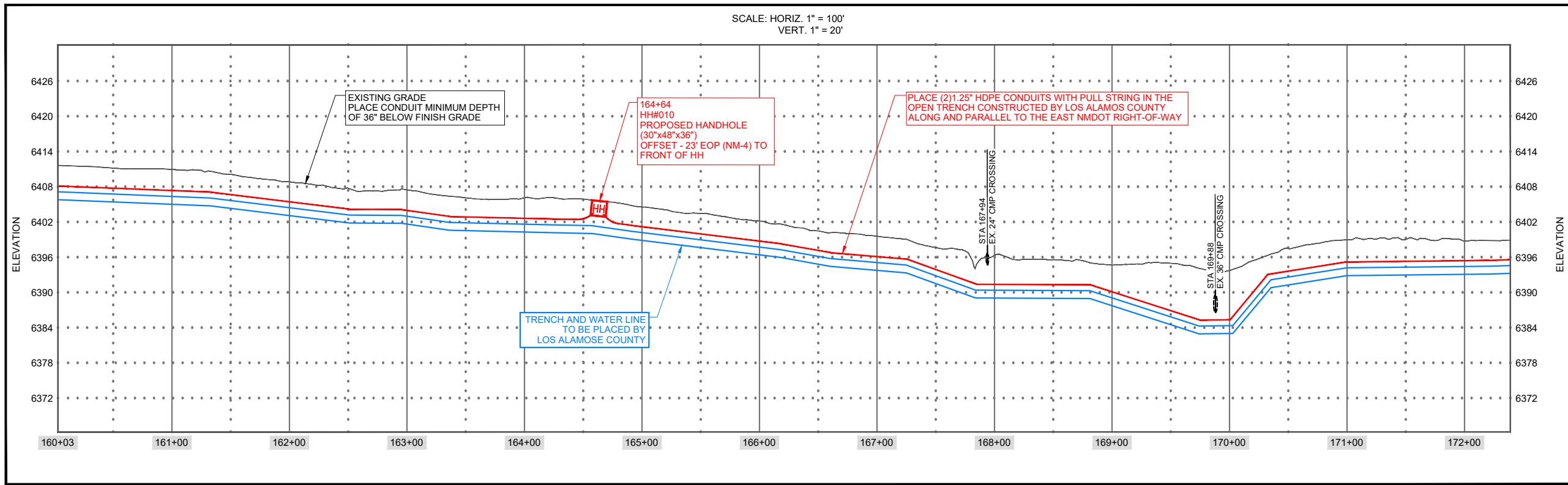
# MAP SHEET 14



NOTE:  
 SAN I PROPOSED HDPE CONDUITS TO BE PLACED IN OPEN TRENCH WITH LOS ALAMOS NEW WATERLINE.  
 PLACE (2) 1.25" HDPE CONDUITS (WITH (1) PULL STRING IN EACH CONDUIT) 1' FROM WATER LINE AND 1' ABOVE WATER LINE. SEE TRENCH CONSTRUCTION DETAIL AND NOTES ON SHEETS 8 & 8A.

SEE MAP SHEET 13 OF 15

SEE MAP SHEET 15 OF 17



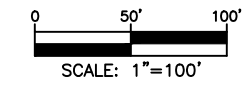
SCALE: HORIZ. 1" = 100'  
 VERT. 1" = 20'

**\*\*CONSTRUCTION NOTE: PLEASE MARK UP ALL UTILITY LINES CROSSED DURING CONSTRUCTION**

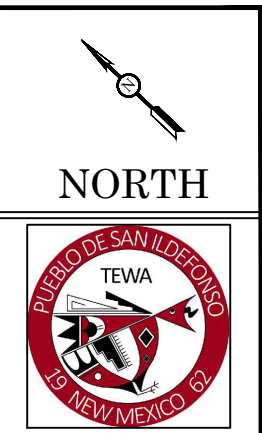
Construction Start date:  
 Construction End date:

Pull Fiber Start date:  
 Pull Fiber End date:

Splice/Test Est Start Date:  
 Splice/Test Actual Date:



SCALE IS FOR REFERENCE PURPOSES ONLY.



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DRAWING REVISIONS:

REV NO.	DATE	DESCRIPTION
01	1/18/24	REVISED
02	2/29/24	REVISED
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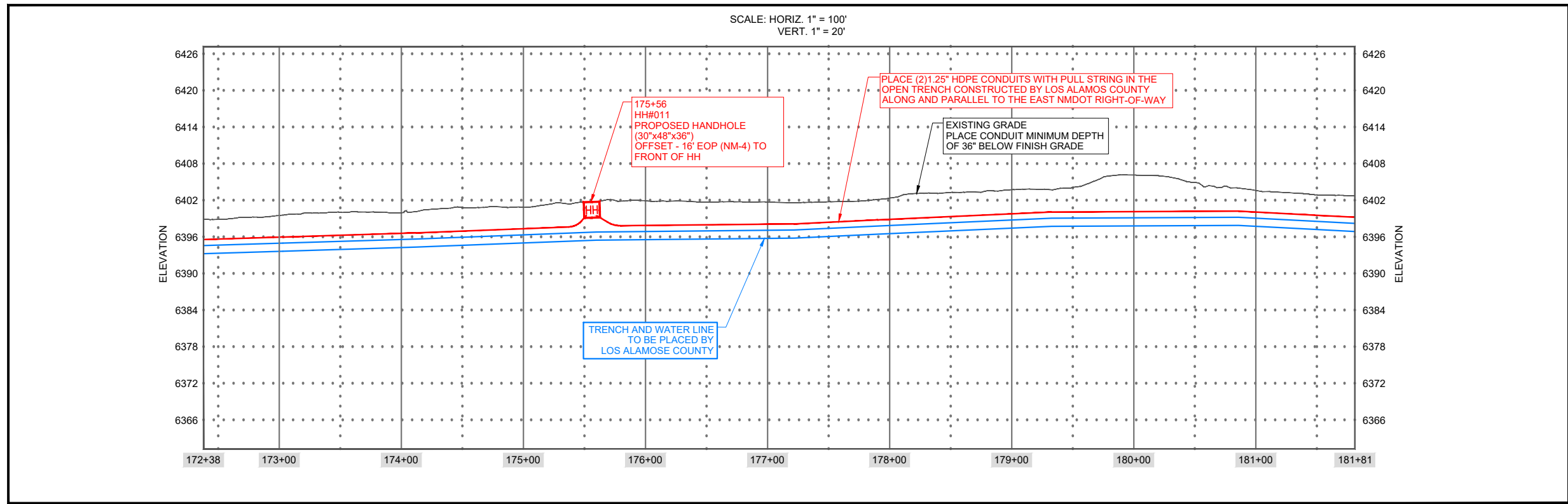
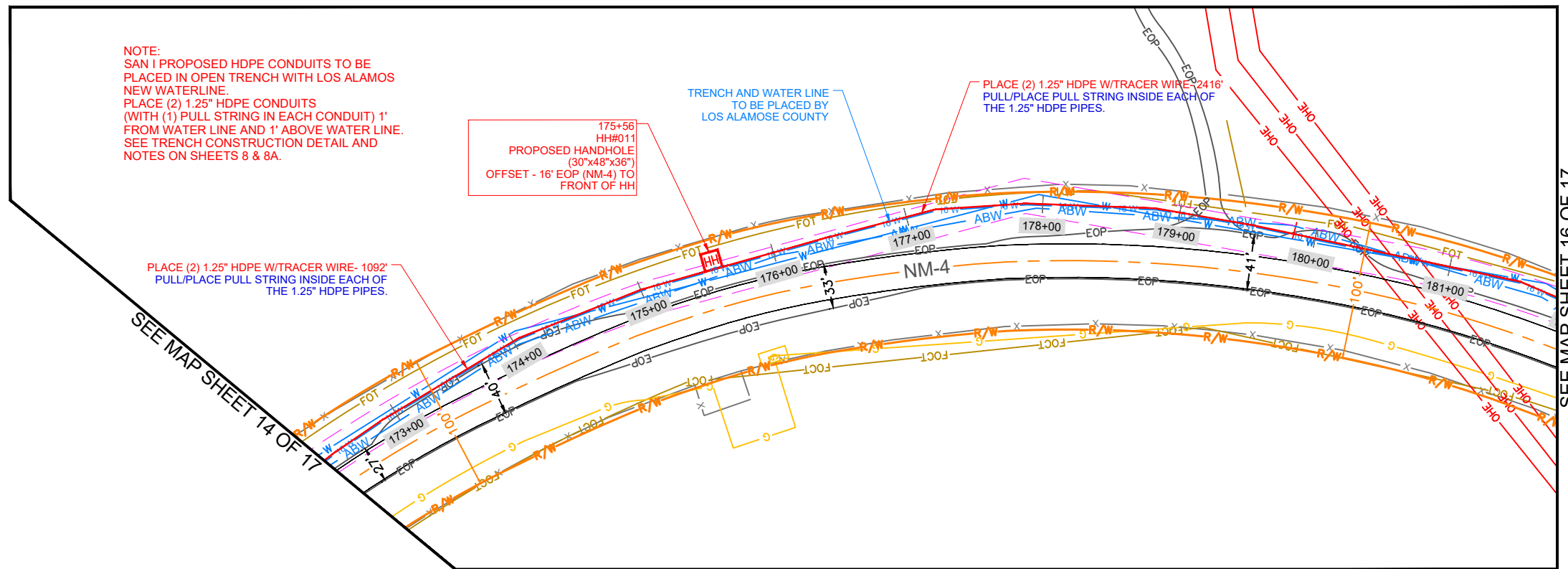
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 MAP SHEET 14  
 PROJECT NAME:  
 SAN I\_NMDOT 4 PARALLEL  
 PROJECT LOCATION:  
 4 ELK TRAIL RD SANTA FE, NM

PAGE  
 24 OF 29





# MAP SHEET 15

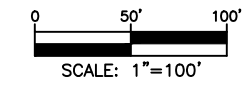


**\*\*CONSTRUCTION NOTE: PLEASE MARK UP ALL UTILITY LINES CROSSED DURING CONSTRUCTION**

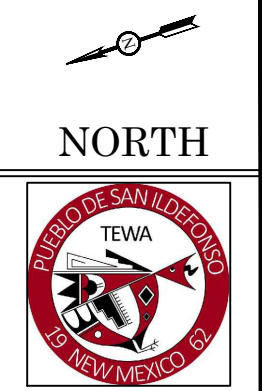
Construction Start date:  
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Pull Fiber Start date:  
 Pull Fiber End date:

Splice/Test Est Start Date:  
 Splice/Test Actual Date:



SCALE IS FOR REFERENCE PURPOSES ONLY.



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DRAWING REVISIONS:

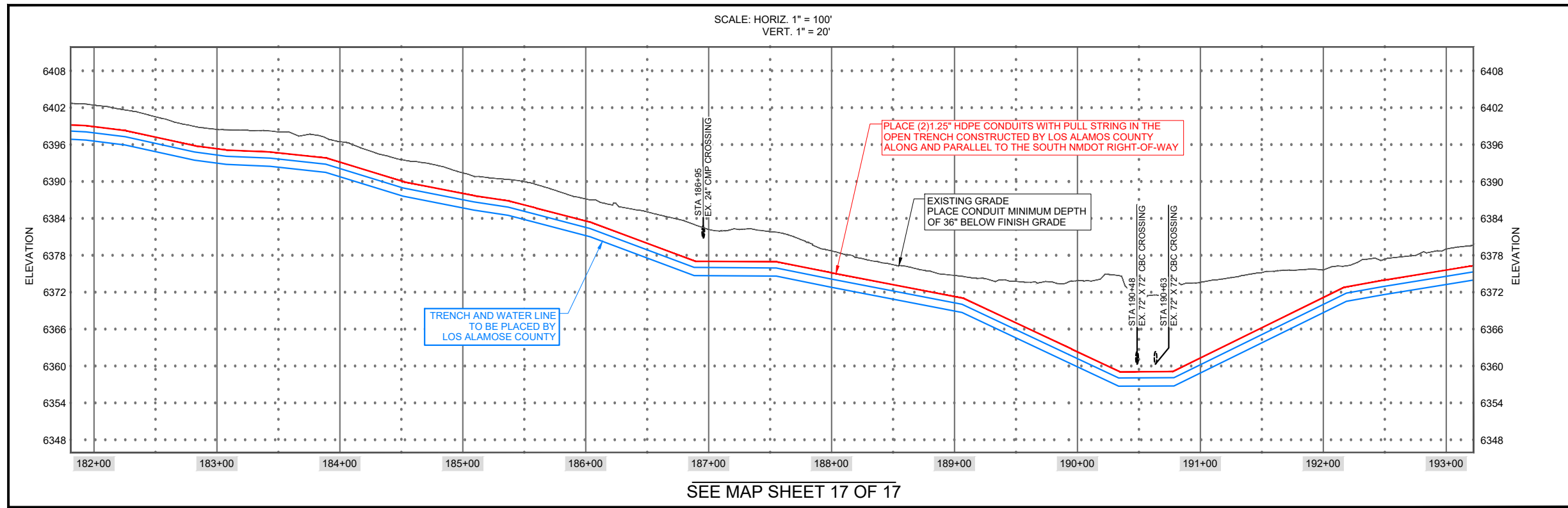
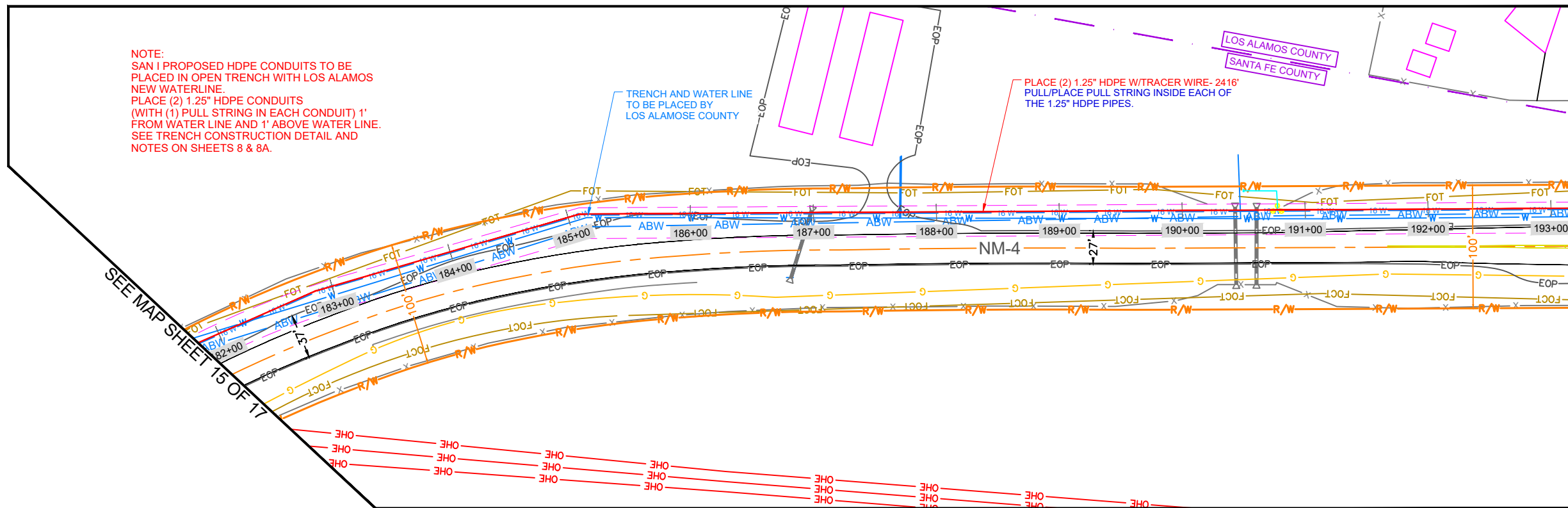
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SHEET TITLE:  
 MAP SHEET 15  
 PROJECT NAME:  
 SAN I NMDOT 4 PARALLEL  
 PROJECT LOCATION:  
 4 ELK TRAIL RD SANTA FE, NM

PAGE  
 25 OF 29



# MAP SHEET 16

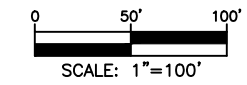


**\*\*CONSTRUCTION NOTE: PLEASE MARK UP ALL UTILITY LINES CROSSED DURING CONSTRUCTION**

Construction Start date:  
 Construction End date:

Pull Fiber Start date:  
 Pull Fiber End date:

Splice/Test Est Start Date:  
 Splice/Test Actual Date:



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 Bryan Schalipp  
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DRAWING REVISIONS:

REV NO.	DATE	DESCRIPTION
01	1/18/24	REVISED
02	2/29/24	REVISED
03	5/24/24	REVISED

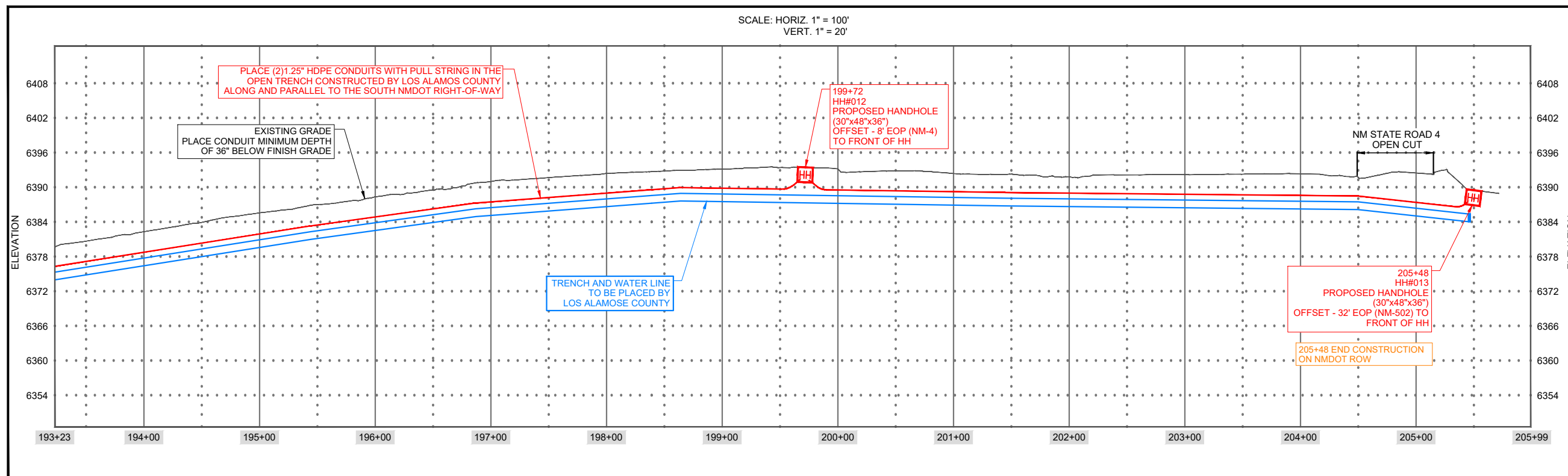
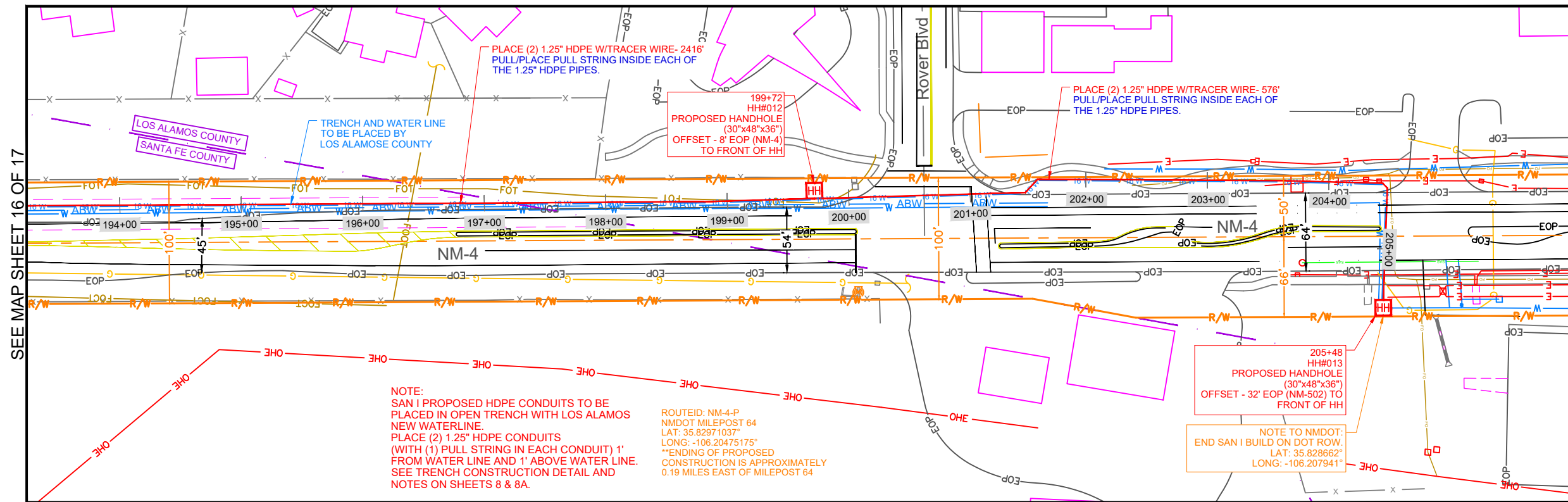
SHEET TITLE:  
 MAP SHEET 16  
 PROJECT NAME:  
 SAN I\_NMDOT 4 PARALLEL  
 PROJECT LOCATION:  
 4 ELK TRAIL RD SANTA FE, NM

PAGE  
 26 OF 29



THIS STAKING SHEET IS INTENDED & PREPARED FOR ROUTE DESIGNATION ONLY & THE ENGINEER HEREBY & EXPRESSLY SET OUT IN THE CONTRACT, DISCLAIMS ANY & ALL RESPONSIBILITY AND/OR OBLIGATION FOR MARKING, NOTING AND/OR REFLECTING THE EXISTENCE AND/OR LOCATION OF ANY & ALL UNDERGROUND FACILITIES, INCLUDING BUT NOT LIMITED TO PETROLEUM BEARING PIPELINES, NOT WITHSTANDING SAID FACILITIES MAY BE VISIBLE AND/OR MARKED ON THE SURFACE ALONG, OVER OR ACROSS SAID PROPOSED ROUTE AS SHOWN ON THIS STAKING SHEET. SCALE IS APPROXIMATE AND SHOWN NOT TO BE USED TO DETERMINE ACTUAL LOCATIONS.

# MAP SHEET 17



**\*\*CONSTRUCTION NOTE: PLEASE MARK UP ALL UTILITY LINES CROSSED DURING CONSTRUCTION**

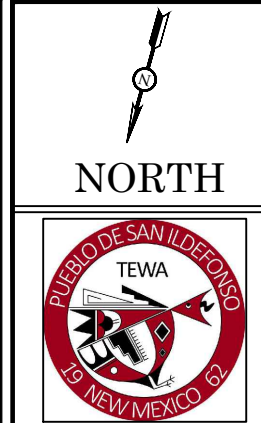
Construction Start date:  
Construction End date:

Pull Fiber Start date:  
Pull Fiber End date:

Splice/Test Est Start Date:  
Splice/Test Actual Date:



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APPROVED BY:  
Bryan Schalipp  
DATE:  
12/03/2023

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REV NO.	DATE	DESCRIPTION
01	1/18/24	REVISED
02	2/29/24	REVISED
03	5/24/24	REVISED

SHEET TITLE:  
MAP SHEET 17  
PROJECT NAME:  
SAN I\_NMDOT 4 PARALLEL  
PROJECT LOCATION:  
4 ELK TRAIL RD SANTA FE, NM

PAGE  
27 OF 29





# NMDOT ROW MAP

### SURVEY NOTES:

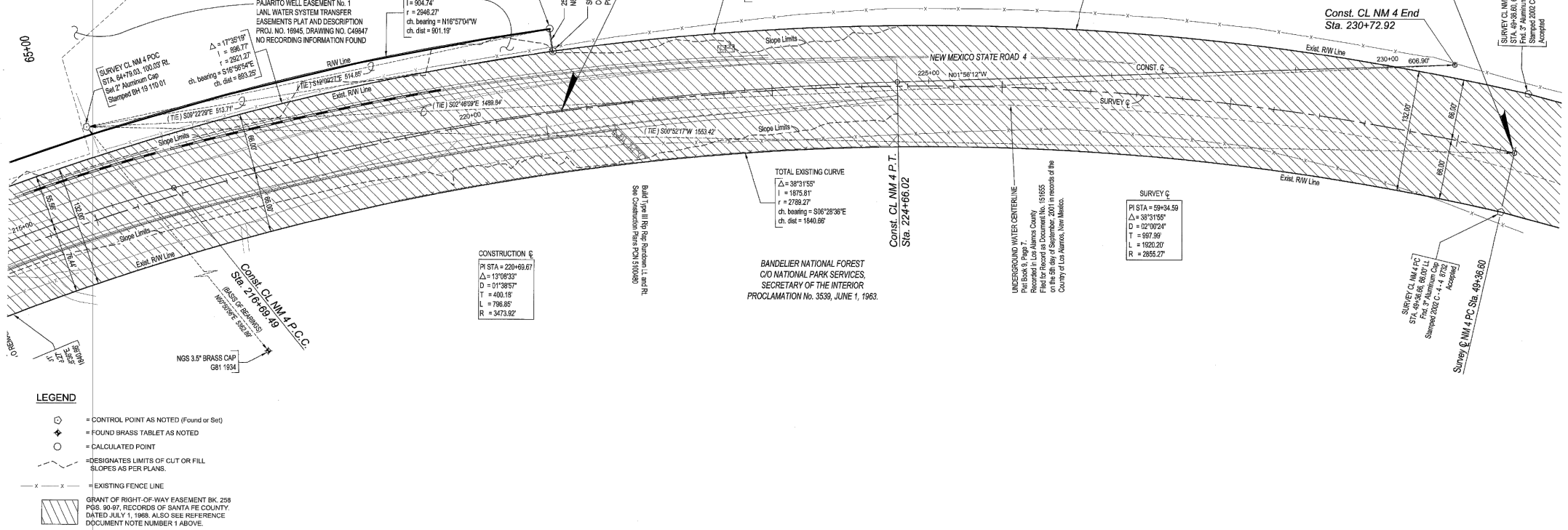
- ALL DISTANCES ARE HORIZONTAL GROUND DISTANCES.
- BEARINGS ARE NEW MEXICO STATE PLANE GRID CENTRAL ZONE (NAD83)
- BASIS OF BEARINGS IS BETWEEN CONTROL POINTS "B" 19 110 01' AND "G81 1934" (N 57°05'59" E) AS SHOWN ON NMDOT CONTROL MAP NMP 5100480 N.M.S.R. 4.
- DATE OF FIELD SURVEY: APRIL TO MAY 2019.
- ALL STATIONS SHOWN FOR EXISTING MONUMENTS ARE TO SURVEY CENTERLINE UNLESS OTHERWISE NOTED. ALL STATIONS SHOWN FOR NEW RIGHT-OF-WAY FEATURES ARE TO SURVEY CENTERLINE UNLESS OTHERWISE NOTED.
- RIGHT OF WAY DIMENSIONS ARE FROM THE SURVEY CENTERLINE.
- BASIS OF STATIONING FOR SURVEY CENTERLINE DERIVED FROM EXISTING MAP: LAND SURVEY PLAT LOS ALAMOS NATIONAL LABORATORY TRACT C-4 DOC. NO. 1380271 BK. 588, PG. 009, AND USFRA BRASS CAP FOUND (STA. 70+55, 83.22 LT.) USED FOR STATIONING ONLY.
- BASIS OF STATIONING FOR CONSTRUCTION CENTERLINE DERIVED FROM CONSTRUCTION PLANS ENTITLED PCN 5100480.
- ALL PROPOSED CONSTRUCTION FEATURES ARE BASED ON 90% COMPLETE DESIGN PLANS PCN 5100480.

### REFERENCE DOCUMENTS CONTINUED:

- SEE ATOMIC ENERGY COMMISSION RIGHT-OF-WAY EASEMENT, NEW MEXICO SR 4, LOS ALAMOS & SANTA FE COUNTIES, FILE NO. 2-4317, SIGNED BY LESLIE H. BEATY (REG. NO. 1085, NEW MEXICO) AND DATED JAN. 4th, 1968 ALSO APPROVED BY THE NEW MEXICO STATE HIGHWAY COMMISSION (SIGNED BY THE STATE ENGINEER) DATED FEB. 15th, 1968. THE BRASS CAP MONUMENTS (U.S.P.R.A./2549) FOUND AS SHOWN HEREON NOT USED TO DEFINE THE CENTERLINE ALIGNMENT OF THE EASEMENT AND DO NOT REPRESENT THE RIGHT OF WAY LINES AS DEFINED BY THIS SURVEY BASED ON A PLAT OF RECORD AS SHOWN HEREON. THIS SECTION OF THE NM SR 4 WAS DONE UNDER THE FEDERAL WORKS AGENCY - PUBLIC ROADS ADMINISTRATION AS A.E.C. ACCESS ROAD SEC. "A" AS SHOWN ON SHEETS NO. 4 AND NO. 5 OF THESE PLANS. NO RECORDS OF THE PLANS USED, NOR ANY MONUMENTATION MAPS WERE FOUND TO BASE THIS SURVEY ON. FEDERAL HIGHWAY MONUMENTATION MAPS MAY EXIST WHICH MAY RESULT IN NM SR 4 ROW EASEMENT LOCATION AS SHOWN HEREON BEING IN A DIFFERENT LOCATION.

### REFERENCE DOCUMENTS:

- REFERENCE MAP USED FOR PCN 5100480: LAND SURVEY PLAT LOS ALAMOS NATIONAL LABORATORY TRACT C-4 DOC. NO. 1380271 BK. 588, PG. 009.
- ADDITIONAL RIGHT-OF-WAY INFORMATION IS AVAILABLE IN THE NMDOT, GENERAL OFFICE, LANDS ENGINEERING SECTION, SANTA FE, NEW MEXICO.
- FOR BUILD NOTES INFORMATION; REFER TO CONSTRUCTION PLANS FOR PCN 5100480.
- GRANT OF RIGHT-OF-WAY BK 258, PGS. 90-97, RECORDS OF SANTA FE COUNTY, DATED JULY 1, 1988.



**LEGEND**

- CONTROL POINT AS NOTED (Found or Set)
- ⊙ FOUND BRASS TABLET AS NOTED
- ⊙ CALCULATED POINT
- - - DESIGNATES LIMITS OF CUT OR FILL SLOPES AS PER PLANS.
- - - EXISTING FENCE LINE
- - - GRANT OF RIGHT-OF-WAY EASEMENT BK. 258 PGS. 90-97, RECORDS OF SANTA FE COUNTY, DATED JULY 1, 1988, ALSO SEE REFERENCE DOCUMENT NOTE NUMBER 1 ABOVE.

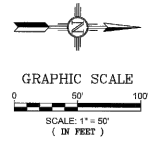
RIGHT-OF-WAY MAP PREPARED BY:  
**Bohannan & Huston**  
 www.bhinc.com 800.877.5332  
 7500 JEFFERSON NE  
 ALBUQUERQUE, NEW MEXICO 87109  
 PHONE: 505-823-1000

NO.	DESCRIPTION	DATE	BY
4			
3			
2			
1			

REVISIONS (OR CHANGE NOTICES)

FOR OWNERS NAME AND AREAS FOR PARCEL NUMBERS SHOWN SEE SHEET 1A OF 3.

SANTA FE COUNTY, NEW MEXICO  
 SECTIONS 20 & 29  
 TOWNSHIP 19 NORTH, RANGE 7 EAST N.M.P.M.

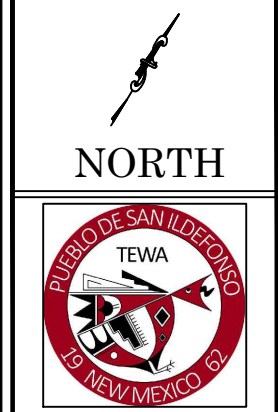


**SURVEYORS CERTIFICATION**  
 I CERTIFY THAT I AM A REGISTERED PROFESSIONAL SURVEYOR AND THAT THESE RIGHT-OF-WAY MAPS ARE AN INTERIM PRODUCT OF PROJECT DESIGN DEVELOPMENT AND WERE PREPARED BY ME OR UNDER MY DIRECTION AND ARE BASED ON ACTUAL FIELD SURVEYS PERFORMED UNDER MY DIRECTION FROM APRIL TO MAY 2019. CONFORMANCE WITH THE STATE OF NEW MEXICO'S MINIMUM STANDARDS FOR RIGHT-OF-WAY SURVEYING WILL OCCUR FOLLOWING ACTUAL ACQUISITION OF THE RIGHT-OF-WAY REQUIRED BY PROJECT NUMBER 5100480.

Barry S. Phillips, N.M.P.S. 15517 DATE: 3-30-2022  
 BOHANNAN HUSTON, INC.  
 7500 JEFFERSON STREET NE  
 ALBUQUERQUE, NM 87109

**FINAL MAP**  
 DATE: 4/6/2022

PCN 5100480  
 NEW MEXICO DEPARTMENT OF TRANSPORTATION  
**RIGHT OF WAY MAP**  
 NEW MEXICO PROJECT NO.  
**5100480**  
 SANTA FE COUNTY  
 SCALE 1" = 50' SHEET 2 OF 3



DRAWN BY:  
**Robert Paroski**  
 APPROVED BY:  
**Bryan Schalipp**  
 DATE:  
 12/03/2023

**DRAWING REVISIONS:**

REV. NO.	DATE	DESCRIPTION
01	1/18/24	REVISED
02	2/29/24	REVISED
03	5/24/24	REVISED

SHEET TITLE:  
**NMDOT ROW MAP**  
 PROJECT NAME:  
**SAN L NMDOT 4 PARALLEL**  
 PROJECT LOCATION:  
**4 ELK TRAIL RD SANTA FE, NM**

PAGE  
 28 OF 29



NOT TO SCALE

# NMDOT ROW MAP

### SURVEY NOTES:

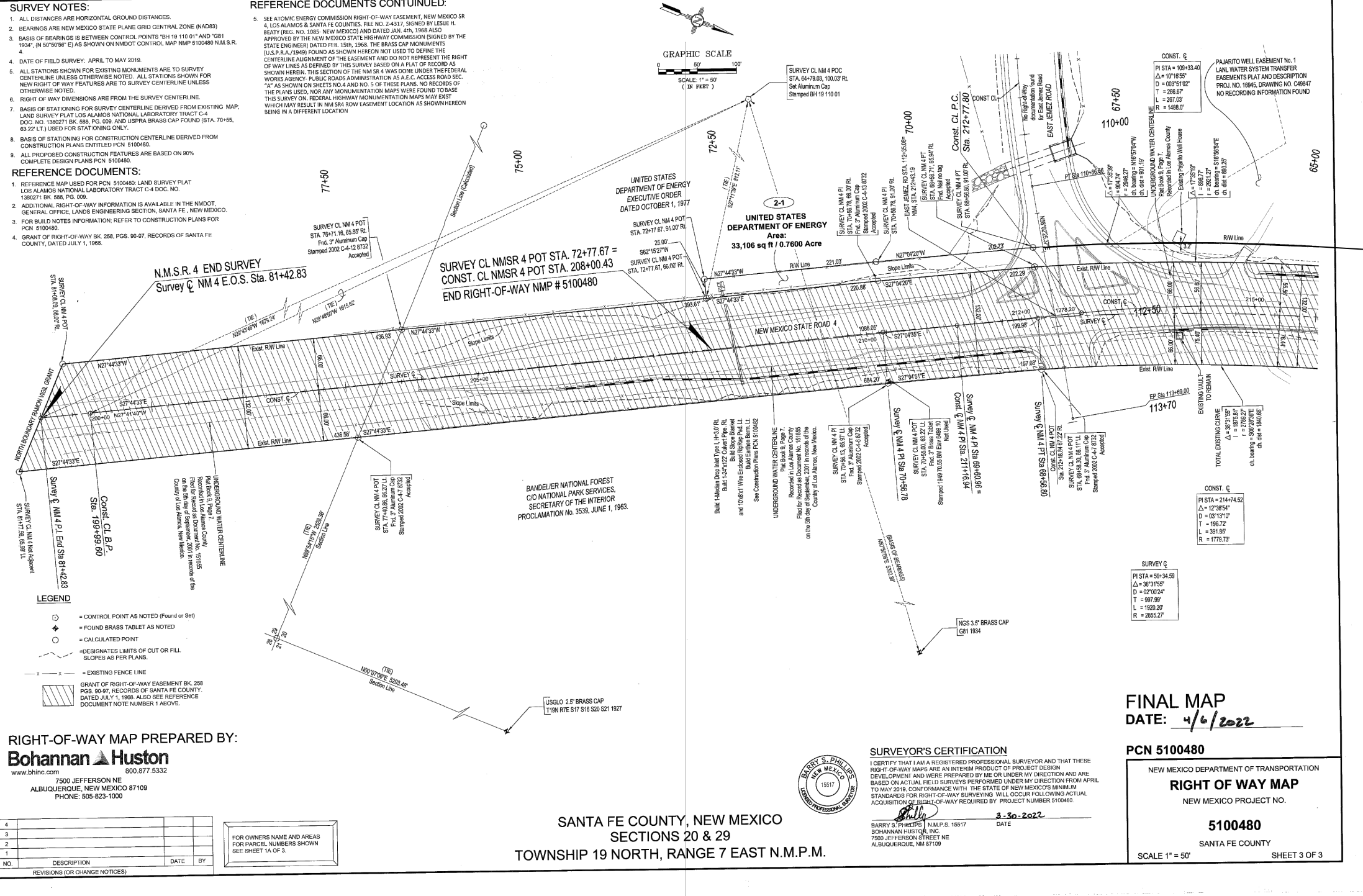
- ALL DISTANCES ARE HORIZONTAL GROUND DISTANCES.
- BEARINGS ARE NEW MEXICO STATE PLANE GRID CENTRAL ZONE (NAD83)
- BASIS OF BEARINGS IS BETWEEN CONTROL POINTS 'B' 19 110 01" AND 'G' 19 104" (N 50°50'56" E) AS SHOWN ON NMDOT CONTROL MAP NMP 5100480 N.M.S.R. DATE OF FIELD SURVEY: APRIL TO MAY 2019.
- ALL STATIONS SHOWN FOR EXISTING MONUMENTS ARE TO SURVEY CENTERLINE UNLESS OTHERWISE NOTED. ALL STATIONS SHOWN FOR NEW RIGHT OF WAY FEATURES ARE TO SURVEY CENTERLINE UNLESS OTHERWISE NOTED.
- RIGHT OF WAY DIMENSIONS ARE FROM THE SURVEY CENTERLINE.
- BASIS OF STATIONING FOR SURVEY CENTERLINE DERIVED FROM EXISTING MAP: LAND SURVEY PLAT LOS ALAMOS NATIONAL LABORATORY TRACT C-4 DOC. NO. 1380271 BK. 988, PGS. 1009, AND USRA BRASS CAP FOUND (STA. 70+55, 63.22 LT.) USED FOR STATIONING ONLY.
- BASIS OF STATIONING FOR CONSTRUCTION CENTERLINE DERIVED FROM CONSTRUCTION PLANS ENTITLED PCN 5100480.
- ALL PROPOSED CONSTRUCTION FEATURES ARE BASED ON 90% COMPLETE DESIGN PLANS PCN 5100480.

### REFERENCE DOCUMENTS:

- REFERENCE MAP USED FOR PCN 5100480: LAND SURVEY PLAT LOS ALAMOS NATIONAL LABORATORY TRACT C-4 DOC. NO. 1380271 BK. 988, PGS. 1009.
- ADDITIONAL RIGHT-OF-WAY INFORMATION IS AVAILABLE IN THE NMDOT, GENERAL OFFICE, LANDS ENGINEERING SECTION, SANTA FE, NEW MEXICO.
- FOR BUILD NOTES INFORMATION, REFER TO CONSTRUCTION PLANS FOR PCN 5100480.
- GRANT OF RIGHT-OF-WAY BK. 258, PGS. 90-97, RECORDS OF SANTA FE COUNTY, DATED JULY 1, 1968.

### REFERENCE DOCUMENTS CONTINUED:

- SEE ATOMIC ENERGY COMMISSION RIGHT-OF-WAY EASEMENT, NEW MEXICO SR 4, LOS ALAMOS & SANTA FE COUNTIES, FILE NO. Z-4317, SIGNED BY LESLIE H. BEATY (REG. NO. 1085-NEW MEXICO) AND DATED JAN. 4TH, 1968 ALSO APPROVED BY THE NEW MEXICO STATE HIGHWAY COMMISSION (SIGNED BY THE STATE ENGINEER) DATED FEB. 15TH, 1968. THE BRASS CAP MONUMENTS (U.S.P.A. 1949) FOUND AS SHOWN HEREON NOT USED TO DEFINE THE CENTERLINE ALIGNMENT OF THE EASEMENT AND DO NOT REPRESENT THE RIGHT OF WAY LINES AS DERIVED BY THIS SURVEY BASED ON A PLAT OF RECORD AS SHOWN HEREIN. THIS SECTION OF THE N.M.S.R. 4 WAS DONE UNDER THE FEDERAL WORKS AGENCY - PUBLIC ROADS ADMINISTRATION AS A.E.C. ACCESS ROAD SEC. "A" AS SHOWN ON SHEETS NO. 4 AND NO. 5 OF THESE PLANS. NO RECORDS OF THE PLANS USED, NOR ANY MONUMENTATION MAPS WERE FOUND TO MAKE THIS SURVEY ON. FEDERAL HIGHWAY MONUMENTATION MAPS MAY EXIST WHICH MAY RESULT IN N.M.S.R. 4 ROW EASEMENT LOCATION AS SHOWN HEREON BEING IN A DIFFERENT LOCATION.



- ### LEGEND
- = CONTROL POINT AS NOTED (Found or Set)
  - ◆ = FOUND BRASS TABLET AS NOTED
  - = CALCULATED POINT
  - = DESIGNATES LIMITS OF CUT OR FILL SLOPES AS PER PLANS.
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RIGHT-OF-WAY MAP PREPARED BY:  
**Bohannon & Huston**  
 www.bhinc.com 800.877.5332  
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NO.	DESCRIPTION	DATE	BY
4			
3			
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1			

FOR OWNERS NAME AND AREAS FOR PARCEL NUMBERS SHOWN SEE SHEET 1A OF 3.

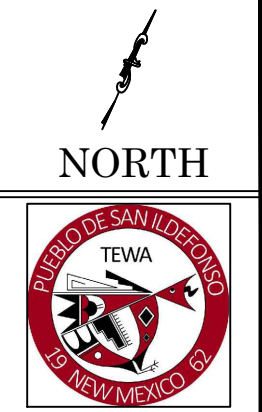
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 Barry S. Phillips  
 BARRY S. PHILLIPS N.M.P.S. 15517 DATE 3-30-2022  
 BOHANNAN HUSTON, INC.  
 7500 JEFFERSON STREET NE  
 ALBUQUERQUE, NM 87109

**FINAL MAP**  
 DATE: 4/6/2022

**PCN 5100480**  
 NEW MEXICO DEPARTMENT OF TRANSPORTATION  
**RIGHT OF WAY MAP**  
 NEW MEXICO PROJECT NO.  
**5100480**  
 SANTA FE COUNTY  
 SCALE 1" = 50' SHEET 3 OF 3



10g  
 10g-Consulting



DRAWN BY:  
**Robert Paroski**  
 APPROVED BY:  
**Bryan Schalipp**  
 DATE:  
 12/03/2023

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**NMDOT ROW MAP**  
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 PROJECT LOCATION:  
**4 ELK TRAIL RD SANTA FE, NM**

PAGE  
 29 OF 29



NOT TO SCALE